

Development Control B Committee Agenda



Date: Wednesday, 13 March 2019

Time: 2.00 pm

Venue: The Council Chamber - City Hall, College Green, Bristol, BS1 5TR

Distribution:

Councillors: Sultan Khan (Chair), Richard Eddy (Vice-Chair), Carla Denyer, Harriet Clough, Lesley Alexander, Tom Brook, Harriet Bradley, Mike Davies, Fi Hance, Olly Mead, Jo Sergeant and Jude English

Copies to: Zoe Willcox (Director: Development of Place), Gary Collins, Laurence Fallon, Jon Fellingham, Norman Cornthwaite, Allison Taylor (Democratic Services Officer), Jeremy Livitt, Rachael Dando, David Fowler (Members' Office Manager (Conservative)), Stephen Fulham, Zarah Jama and Paul Shanks

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Date: Tuesday, 5 March 2019

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Agenda

1. Welcome, Introduction and Safety Information

2.00 pm

(Pages 4 - 5)

2. Apologies for Absence

3. Declarations of Interest

To note any interests relevant to the consideration of items on the agenda.

Any declarations of interest made at the meeting which are not on the register of interests should be notified to the Monitoring Officer for inclusion.

4. Minutes of the last Meeting

To agree the Minutes of the last Meeting as a correct record for signature by the Chair.

(Pages 6 - 12)

5. Appeals

To note appeals lodged, imminent public inquiries and appeals awaiting decision.

(Pages 13 - 21)

6. Enforcement

To note enforcement notices.

(Pages 22 - 23)



7. Public forum

Any member of the public or councillor may participate in public forum. The detailed arrangements for so doing are set out in the Public Information Sheet at the back of this agenda. Please note that the following deadlines will apply in relation to this meeting:

Questions:

Written questions must be received three clear working days prior to the meeting. For this meeting, this means that your question(s) must be received at the latest by 5pm on Thursday 7th March 2019.

Petitions and statements:

Petitions and statements must be received by noon on the working day prior to the meeting. For this meeting, this means that your submission must be received at the latest by 12.00 noon on Tuesday 12th March 2019.

The statement should be addressed to the Service Director, Legal Services, c/o The Democratic Services Team, City Hall, 3rd Floor Deanery Wing, College Green,
P O Box 3176, Bristol, BS3 9FS or email - democratic.services@bristol.gov.uk

8. Planning and Development

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| To consider the following applications for Development Control Committee B - | (Pages 24 - 25) |
| a) 18/04977/P - Trust Headquarters, Marlborough Street | (Pages 26 - 65) |
| b) 18/05184/P - Broadwalk Shopping Centre | (Pages 66 - 93) |
| c) 18/06358/F - Alexandra Park Public Car Park | (Pages 94 - 123) |
| d) 18/04727/F - Public Conveniences, Circular Road (The Downs) | (Pages 124 - 172) |
| e) 18/04108/F - St Gabriels Court, St Gabriels Road | (Pages 173 - 223) |
| f) 19/00253/H - 6 Alford Road | (Pages 224 - 234) |

9. Date of Next Meeting

24th April 2019 at 6.00 pm.



Public Information Sheet

Inspection of Papers - Local Government
(Access to Information) Act 1985

You can find papers for all our meetings on our website at www.bristol.gov.uk.

You can also inspect papers at the City Hall Reception, College Green, Bristol, BS1 5TR.

Other formats and languages and assistance
For those with hearing impairment

You can get committee papers in other formats (e.g. large print, audio tape, braille etc) or in community languages by contacting the Democratic Services Officer. Please give as much notice as possible. We cannot guarantee re-formatting or translation of papers before the date of a particular meeting.

Committee rooms are fitted with induction loops to assist people with hearing impairment. If you require any assistance with this please speak to the Democratic Services Officer.

Public Forum

Members of the public may make a written statement ask a question or present a petition to most meetings. Your statement or question will be sent to the Committee and be available in the meeting room one hour before the meeting. Please submit it to democratic.services@bristol.gov.uk or Democratic Services Section, City Hall, College Green, Bristol BS1 5UY. The following requirements apply:

- The statement is received no later than **12.00 noon on the working day before the meeting** and is about a matter which is the responsibility of the committee concerned.
- The question is received no later than **three clear working days before the meeting**.

Statements will not be accepted after **12.00 noon on the working day before the meeting** unless they have been submitted in advance to Bristol City Council but were not received by the Democratic Services Section. Anyone submitting multiple statements for an application should note that they will only be allowed to speak once at the meeting.

Any statement submitted should be no longer than one side of A4 paper. If the statement is longer than this, then for reasons of cost, only the first sheet will be copied and made available at the meeting. For copyright reasons, we are unable to reproduce or publish newspaper or magazine articles that may be attached to statements.

By participating in public forum business, we will assume that you have consented to your name and the details of your submission being recorded and circulated to the committee. This information will



also be made available at the meeting to which it relates and placed in the official minute book as a public record (available from Democratic Services).

We will try to remove personal information such as contact details. However, because of time constraints we cannot guarantee this, and you may therefore wish to consider if your statement contains information that you would prefer not to be in the public domain. Public Forum statements will not be posted on the council's website. Other committee papers may be placed on the council's website and information in them may be searchable on the internet.

Process during the meeting:

- Public Forum is normally one of the first items on the agenda, although statements and petitions that relate to specific items on the agenda may be taken just before the item concerned.
- There will be no debate on statements or petitions.
- The Chair will call each submission in turn. When you are invited to speak, please make sure that your presentation focuses on the key issues that you would like Members to consider. This will have the greatest impact.
- Your time allocation may have to be strictly limited if there are a lot of submissions. **This may be as short as one minute.**
- If there are a large number of submissions on one matter a representative may be requested to speak on the groups behalf.
- If you do not attend or speak at the meeting at which your public forum submission is being taken your statement will be noted by Members.

Webcasting/ Recording of meetings

Members of the public attending meetings or taking part in Public forum are advised that all Full Council and Cabinet meetings and some other committee meetings are now filmed for live or subsequent broadcast via the council's [webcasting pages](#). The whole of the meeting is filmed (except where there are confidential or exempt items) and the footage will be available for two years. If you ask a question or make a representation, then you are likely to be filmed and will be deemed to have given your consent to this. If you do not wish to be filmed you need to make yourself known to the webcasting staff. However, the Openness of Local Government Bodies Regulations 2014 now means that persons attending meetings may take photographs, film and audio record the proceedings and report on the meeting (Oral commentary is not permitted during the meeting as it would be disruptive). Members of the public should therefore be aware that they may be filmed by others attending and that is not within the council's control.

Bristol City Council
Minutes of the Development Control B Committee

30 January 2019 at 6.00 pm



Members Present:-

Councillors: Sultan Khan (Chair), Richard Eddy (Vice-Chair), Carla Denyer, Harriet Clough, Lesley Alexander, Tom Brook, Harriet Bradley, Mike Davies, Fi Hance, Olly Mead and Jo Sergeant

Officers in Attendance:-

Gary Collins, Laurence Fallon, Norman Cornthwaite, Tom Watson, Anna Schroeder, Kayna Tregay and Natalie Queffurus

1. Welcome, Introduction and Safety Information

Councillor Sultan Khan welcomed all parties to the meeting and explained arrangements for emergency access in the event of a fire.

2. Apologies for Absence

None were received.

3. Declarations of Interest

None were received.

4. Minutes of the previous meeting

The Committee noted a Public Forum Statement from Councillor Stevens concerning the application relating to Stoke Lodge which was considered by the Committee.

Resolved – that the Minutes be agreed as a correct record and signed by the Chair.

Councillor Eddy raised a Point of Order (CMR 11.12 refers) concerning the fencing being erected on the Stoke Lodge site and TPO Legislation.



The Chair stated that as this was not an item on the Agenda he could not permit a debate on it, but would ask the Head of Development Management to update the Committee on Stoke Lodge.

The Head of Development Management stated that BCC had taken advice from Counsel before agreeing that the erection of the fencing was permitted development. BCC Officers have visited the site to ensure that the trees are not being harmed and that what is going on at the site is acceptable, and that arboricultural good practice is being carried out. There is ongoing dialogue with the School and BCC Officers have provided advice to the School; the School has responded positively to the advice given. A TPO application is not required and he (the Head of Development Management) has responded to Local Members concerning the issue.

5. Appeals

The Head of Development Management gave an overview of the Appeals in progress drawing attention to the following appeals concerning digital adverts:

Item 12 – City Point, Temple Gate. This Appeal has been dismissed on public safety grounds.

Item 45 – Central Reservation Temple Way. This Appeal has been dismissed.

Item 46 – Public Footpath, West side of Bond Street. This Appeal has been allowed.

6. Enforcement

The Head of Development Management stated that whilst there had not been any notices served since the last meeting (due to the Christmas break) there are likely to be Enforcement issues to be reported at the next Meeting.

7. Public forum

Members of the Committee received Public Forum Statements in advance of the meeting.

The statements were heard before the application they related to and were taken fully into consideration by the Committee prior to reaching a decision.

8. Planning and Development

The Committee considered the following Planning Applications

9. Planning Application Number 18/02968/X - Avon Crescent, Bristol BS1 6XQ



This item had been considered by the Committee at its Meeting on 6th November 2018, but a decision had been deferred pending a site visit. The site visit took place on 19th December 2018 and Members were reminded that only those who attended the site visit could participate in the item at this Meeting. The Members who attended the site visit were as follows: Lesley Alexander, Mike Davies, Carla Denyer, Richard Eddy, Fi Hance, Sultan Khan, Olly Mead and Jo Sergeant.

The Head of Development Management and his representative presented this item and made the following comments:

- This is an application to vary a Condition (No. 15) on a previously granted permission
- It is to improve highway safety
- Drivers will be slowed down through various measures
- Members attention was also drawn to Condition 9
- The application was recommended for approval

Answers to questions

- The Committee is acting as the Planning Authority and the decision on the application has to be made on planning grounds
- The scheme was originally approved in 2014 and revisions have been made since then
- A Condition concerning access to Underfall Yard is included
- The footway width is a minimum of 1.1 m but varies and is considered acceptable; it would be difficult to get a wider footway
- The trees are not detailed on the plan as there is a Condition to require the submission of details of landscaping
- Having a cycleway all along the street would impinge on parking
- Although there is No Entry at one end it is effectively a two way street; it will be narrowed and the proposed table will reduce speeds
- Any diversionary route would cut off access to businesses
- There will be a small increase in the number of proposed on-street parking spaces
- Residents and businesses were consulted as part of the scheme evolution
- A Zebra Crossing was not considered to be deliverable
- The scheme has to be assessed against Policy and although this scheme is not optimum it is considered to be acceptable

Comments

- It was not considered that assurances to residents had been met and there were concerns about safety
- There was disappointment with the scheme
- There were concerns that the quality of the safety measures had been reduced to save money



- The scheme was not considered acceptable on highway safety grounds
- In response the Head of Development Management advised that an unintended consequence could be that no scheme is delivered. The revised scheme had been submitted because the project did not have the budget to deliver the original scheme and if permission was refused for this scheme it was possible that Avon Crescent could remain unchanged. There was no effective legal powers to require the original scheme to be implemented.

Members were reminded that they could refuse, approve or defer a decision on the application. They could also strengthen Condition 9.

Councillor Eddy moved refusal of the application on highway and pedestrian safety grounds. This was seconded by Councillor Mead and on being put to the vote, it was

Resolved (5 for, 1 against, 2 abstention) – that the application be refused on highway and pedestrian safety grounds.

10 Planning Application Number 18/04911/F - 21 St Thomas Street, Bristol BS1 6JS

The Head of Development Management and his representative presented this item and made the following comments:

- This is a full application for the demolition of existing structure and construction of a new 13 storey development
- The details of the development and the Key Issues were described along with the consultation
- The application was recommended for refusal and the reasons for refusal were summarised

Answers to questions

- The bulk and massing of the proposed development is considered to be a problem and out of keeping with the Conservation Area and immediate neighbours
- 18 affordable units are proposed although these are proposed as key worker housing and therefore do not meet the BCC requirements for 77% social rented affordable housing and 23% intermediate affordable housing.
- University staff are not considered to be Key Workers, although school staff are
- There is no BCC Policy on Key Workers
- A clause in a S106 agreement could be added concerning affordable housing but it might not be possible to deliver this as this had not been offered by the applicant

Comments

- Concerns about there being no affordable housing
- Concerns about the proximity to low level buildings and space standards



- Concerns about harm to existing residents' amenity

Councillor Eddy moved refusal of the application for the reasons contained in the report. This was seconded by Councillor Mead and on being put to the vote, it was

Resolved (8 for, 2 against, 1 abstention) – that the application be refused for the reasons contained in the report.

11 Planning Application Number 17/04072/F - 66 Church Road, Redfield Bristol BS5 9UY

The Head of Development Management and his representative presented this item and made the following comments:

- This is a full application for the demolition of existing structure and construction of a new 13 storey development
- The details of the development and the Key Issues were described along with the consultation
- The application was recommended for refusal and the reasons for refusal were summarised

Answers to questions

- The bulk and massing of the proposed development is considered to be a problem and out of keeping with the Conservation Area and immediate neighbours
- 18 affordable units are proposed although these are proposed as key worker housing and therefore do not meet the BCC requirements for 77% social rented affordable housing and 23% intermediate affordable housing.
- University staff are not considered to be Key Workers, although school staff are
- There is no BCC Policy on Key Workers
- A clause in a S106 agreement could be added concerning affordable housing but it might not be possible to deliver this as this had not been offered by the applicant

Comments

- Concerns about there being no affordable housing
- Concerns about the proximity to low level buildings and space standards
- Concerns about harm to existing residents' amenity

Councillor Eddy moved refusal of the application for the reasons contained in the report. This was seconded by Councillor Mead and on being put to the vote, it was



Resolved (8 for, 2 against, 1 abstention) – that the application be refused for the reasons contained in the report.

12 Planning Application Number 17/04071/F - 68 to 70 Church Road, Redfield, Bristol BS5 9JY

The Head of Development Management and his representative presented this item and made the following comments:

- This is a full application for the demolition of the existing building and replacement with a mixture of residential units and retail/business unit
- The details of the application and Key Issues were described
- Summary of the objections received was also provided
- It was noted that this and the adjacent proposal – 66 Church Road – do not provide a coherent development
- The recommendation for the application is refusal and the reasons for the recommendation were summarised

Answers to questions

- Although the issue of access to the electricity sub station would be a private matter, if the application were granted a Condition would be included concerning access and waste, etc.

Councillor Eddy moved refusal of the application for the reasons contained in the report. This was seconded by Councillor Denyer and on being put to the vote, it was

Resolved (6 for, 4 against, 1 abstention) – that the application be refused for the reasons contained in the report.

13 Planning Application Number 18/05677/F - Merchants House, Wapping Road, Bristol BS1 4RW

The Head of Development Management and his representative presented this item and made the following comments:

- This is a full application for external and internal works including installation of a roof terrace, recladding the elevations, new pedestrian access and altered parking provision
- The details of the application and Key Issues were described
- Summary of the objections received was also provided
- The recommendation for the application is refusal and the reasons for the recommendation were summarised



Comments

- Some Members liked the proposed appearance of the building and considered the application to be acceptable
- Other Members did not like the proposals and thought the appearance was not appropriate in this conservation area and within the setting of the listed buildings, and that the cladding would soon date

Councillor Mead moved refusal of the application for the reasons contained in the report. This was seconded by Councillor Brooks and on being put to the vote, it was

Resolved (7 for, 4 against, 0 abstention) – that the application be refused for the reasons contained in the report.

14 Date of Next Meeting

The Committee noted that the next meeting was scheduled for **2.00pm on Wednesday 13th March 2019** in the Council Chamber, City Hall.

Meeting ended at 9.10 pm

CHAIR _____



DEVELOPMENT CONTROL COMMITTEE B**13th March 2019****REPORT OF THE SERVICE DIRECTOR - PLANNING****LIST OF CURRENT APPEALS****Householder appeal**

Item	Ward	Address, description and appeal type	Date lodged
1	Lockleaze	16 Baily Place Cheswick Village Bristol BS16 1BG Erection of a first floor, side extension. Appeal against refusal Delegated decision	10/12/2018
2	Lawrence Hill	30 Eve Road Bristol BS5 0JX Proposed roof extension and single storey rear extension. Appeal against refusal Delegated decision	24/01/2019
3	Redland	13 Kingsley Road Cotham Bristol BS6 6AF New rear dormer, new roof windows, one and two storey rear extensions, external insulation to side and rear elevations, replacement windows, alterations to front garden to incorporate car parking. Appeal against refusal Delegated decision	11/02/2019
4	Hartcliffe & Withywood	23 Hollisters Drive Bristol BS13 0EY Raise roof and add dormers roof extensions front and rear , remove chimney. Appeal against refusal Delegated decision	11/02/2019
5	Lawrence Hill	100 Goodhind Street Bristol BS5 0TB Part double/Part single storey rear extension. Appeal against refusal Delegated decision	11/02/2019
6	Stockwood	100 Stockwood Road Stockwood Bristol BS14 8JF Proposed two storey side extension and single storey front extension. Appeal against refusal Delegated decision	12/02/2019

7	Lockleaze	635 Muller Road Bristol BS5 6XP Proposed two/single storey extension to side and single storey infill extension to rear. Appeal against refusal Delegated decision	13/02/2019
8	Easton	90 Avonvale Road Bristol BS5 9RU Single storey extension to the side of the property, excavation works to create basement. Vehicle access. Appeal against refusal Delegated decision	18/02/2019

Informal hearing

Item	Ward	Address, description and appeal type	Date of hearing
9	Filwood	PX Centre Bedminster Road Bristol BS3 5NR Outline planning application (with access, layout, scale and appearance to be considered) for redevelopment of the site to provide 32no. self-contained flats (Use Class C3) with associated access, parking, drainage and hard/soft landscape works. Appeal against refusal Delegated decision	TBA

Public inquiry

Item	Ward	Address, description and appeal type	Date of inquiry
10	Ashley	15-16 Brunswick Square Bristol BS2 8NX Proposed change of use from Private Members' Club (Sui Generis) at ground floor and lower ground floor with ancillary office use on the upper floors to office floorspace (B1a) on all floors with associated provision of waste storage and bicycle parking facilities and external alterations. Appeal against refusal Delegated decision	19/03/2019
11	Ashley	15-16 Brunswick Square Bristol BS2 8NX Internal and external works associated with the proposed change of use from Private Members' Club (Sui Generis) at ground floor and lower ground floor with ancillary office use on the upper floors to office floorspace (B1a) on all floors with associated provision of waste storage and bicycle parking facilities. Appeal against refusal Delegated decision	19/03/2019

Written representation

Item	Ward	Address, description and appeal type	Date lodged
12	Hillfields	16 Woodcote Road Bristol BS16 4DE Proposed new 1no. bedroom house, on land adjacent to 16 Woodcote road and a 2 storey extension to the existing house. Appeal against refusal Delegated decision	17/10/2018
13	Southmead	21 Shetland Road Bristol BS10 5JT Erection of a detached dwellinghouse. Appeal against refusal Delegated decision	18/10/2018
14	Avonmouth & Lawrence Weston	16 Grove Leaze Bristol BS11 9QN Erection of a single storey rear extension. Appeal against conditions imposed Delegated decision	26/10/2018
15	Easton	28 York Road Easton Bristol BS5 6BJ Application for a Certificate of Proposed Development - proposed porch. Appeal against refusal Delegated decision	15/11/2018
16	Clifton Down	40 - 44 St Pauls Road Clifton Bristol BS8 1LR Retrospective application for the erection of a timber structures over the rear patio area. Appeal against refusal Delegated decision	27/11/2018
17	Clifton Down	40 - 44 St Pauls Road Clifton Bristol BS8 1LR Retrospective application for the erection of a timber structure over the rear patio area. Appeal against refusal Delegated decision	27/11/2018
18	Clifton Down	40 - 44 St Pauls Road Clifton Bristol BS8 1LR Enforcement notice appeal for the erection of timber structures to rear without planning permission. Appeal against an enforcement notice	27/11/2018

19	Eastville	Land And Buildings On The South Side Sandy Lane Bristol BS5 6SP Enforcement notice appeal for use of garage/store for commercial car repairs (COU). Appeal against an enforcement notice	10/12/2018
20	Cotham	12E Alfred Place Kingsdown Bristol BS2 8HD Enforcement notice appeal for the erection of dormer without planning permission. Appeal against an enforcement notice	10/12/2018
21	Lawrence Hill	Site ND6 Temple Quay Land Bounded By Providence Place, Old Bread Street & Avon Street Bristol BS2 0ZZ Erection of a 6- to 11-storey building comprising 120 no. (PRS - privately rented sector), residential units (1-, 2- and 3-bed), 524 sqm of flexible commercial floorspace (Use Classes A1, A2, A3, A4, B1a, D1 or D2) at ground floor level and associated development, including landscaping, public realm, bin storage, plant areas and cycle parking (Major application). Appeal against non-determination Committee	10/12/2018
22	Hengrove & Whitchurch Park	12 Valentine Close Bristol BS14 9NB Enforcement appeal for use of detached garage as self-contained unit of residential accommodation including subdivision of garden area. Appeal against an enforcement notice	10/12/2018
23	Brislington West	239 Bloomfield Road Bristol BS4 3QT Two storey side extension to create new dwelling, with raised rear terrace and associated works. Appeal against refusal Delegated decision	11/12/2018
24	Knowle	41 Imperial Walk Bristol BS14 9AD Erection of a new dwelling with access, screening, parking, landscaping and associated works. Appeal against refusal Delegated decision	21/01/2019
25	Hartcliffe & Withywood	30 Honey Garston Road Bristol BS13 9LT Erection of 2 no 2 bedroomed flats to side of existing house Appeal against refusal Delegated decision	21/01/2019

26	Horfield	1 Bishopthorpe Road Bristol BS10 5AA Proposed two storey single dwelling. Appeal against refusal Delegated decision	24/01/2019
27	Stoke Bishop	Stoke Lodge Playing Fields Shirehampton Road Bristol BS9 1BN Erection of new changing room building and associated works to replace existing building. Appeal against refusal Committee	24/01/2019
28	Bishopston & Ashley Down	91 Ashley Down Road Bristol BS7 9JT Retention of outbuilding. Appeal against refusal Delegated decision	04/02/2019
29	Bedminster	Bridge And Land To The North Of South Liberty Lane Bristol Demolition of existing bridge and construction of 17 no. houses and 3 no. flats (Use Class C3) with associated new access road, car parking, landscaping and ground works. Major Application Appeal against refusal Delegated decision	05/02/2019
30	Eastville	1 Park Road Stapleton Bristol BS16 1AZ Application for variation of condition No. 5 following grant of planning permission 15/06140/F - proposed change to hours of operation condition to allow opening from 18:30 until 22:30 on two Friday evenings of every month. Appeal against refusal Delegated decision	06/02/2019
31	Avonmouth & Lawrence Weston	Land Adjoining 104 Avonmouth Road Bristol BS11 9ND One 48-Sheet digital advertisement display. Appeal against refusal Delegated decision	12/02/2019
32	Westbury-on-Trym & Henleaze	29 Hobhouse Close Bristol BS9 4LZ Erection of new 2 storey dwelling attached to side of 29 Hobhouse Close and associated structures. Appeal against refusal Delegated decision	18/02/2019
33	Bedminster	Units 3 To 4 Charnwood House Marsh Road Bristol BS3 2NA Notification of Prior Approval for a proposed change of use of a building from office use (Class B1(a)) to 8 flats (Class C3). Appeal against refusal Delegated decision	20/02/2019

34	Central	Unit 4, 12 Broad Quay Bristol BS1 4DH Change of use of the highway to an external seating area consisting of loose furniture and barrier screening and alterations to the shop front including retractable awnings with heating and signage. Appeal against refusal Delegated decision	20/02/2019
35	Central	26 Baldwin Street Bristol BS1 1SE Erection of two additional storeys above the existing building to accommodate 4no. flats. Appeal against refusal Delegated decision	22/02/2019
36	Hartcliffe & Withywood	2 Honey Garston Road Bristol BS13 9LY Construction of a new 2 storey 2 bedroom dwelling (end of terrace) on land adjacent 2 Honey Garston Road. Appeal against refusal Delegated decision	22/02/2019
37	Central	Land Adjacent To The Quays Cumberland Road Bristol Proposed 6 storey building (plus a basement), to accommodate 13 units operated as serviced apartments (Use Class C1), (major application). Appeal against refusal Committee	25/02/2019
38	Stoke Bishop	88 Shirehampton Road Bristol BS9 2DR Installation fencing on perimeter of flat roof and formation of roof terrace [outdoor amenity space]. Appeal against refusal Delegated decision	25/02/2019
39	Stoke Bishop	Second Floor Flat 88 Shirehampton Road Bristol BS9 2DR Installation of a dormer window to side of main roof. Appeal against conditions imposed Delegated decision	25/02/2019
40	Horfield	Land At Inn On The Green 2 Filton Road Bristol BS7 0BH Discontinuance notice appeal in respect of advertisement hoarding on the land.	25/02/2019
41	Brislington East	Land At St Anne's Road St Annes Road St Annes Bristol Discontinuance notice appeal in respect of advertisement hoarding.	25/02/2019

List of appeal decisions

Item	Ward	Address, description and appeal type	Decision and date decided
42	Cotham	16 Clyde Road Redland Bristol BS6 6RP Partial demolition of existing garage/store structure and erection of a single storey, 1 bedroom dwelling with revised access. Appeal against refusal Delegated decision	Appeal allowed 31/01/2019
43	Clifton Down	36 Hampton Park Bristol BS6 6LH Construction of a one bed house, sunken into existing rear garden. Appeal against refusal Delegated decision	Appeal dismissed 05/02/2019
44	Central	City Point Temple Gate Bristol BS1 6PL LED Digital Smartscreen. Appeal against refusal Delegated decision	Appeal dismissed 23/01/2019
45	Ashley	Land Next To 75 City Road Bristol BS2 8UQ New three storey end of terrace building containing 2no. residential units. Appeal against refusal Delegated decision	Appeal allowed 29/01/2019
46	Ashley	Wadham Mansions Balmoral Road Bristol BS7 9AU Erection of a 4 storey building comprising 2 No (1 bed-space) studio apartments and 1 No (4 bed-space) 2 bedroom duplex, with cycle store, attached to existing 4 storey block of apartments. Appeal against refusal Delegated decision	Appeal dismissed 06/02/2019
47	Westbury-on-Trym & Henleaze	14 Southfield Road Westbury Bristol BS9 3BH Replacement of existing wooden single glazed sash windows with uPVC double glazed units (3no. at the front of the house facing the street, 3no. at the rear). Appeal against refusal Delegated decision	Appeal dismissed 07/02/2019
48	Stockwood	18 Burfoote Gardens Bristol BS14 8TE Conversion of existing garage and erection of a first floor extension to provide a two storey dwelling house. Appeal against refusal Delegated decision	Appeal allowed 13/02/2019

49	Stoke Bishop	19 Druid Hill Bristol BS9 1EW Single storey side extension to extend existing hair salon. Appeal against refusal Delegated decision	Appeal dismissed 27/02/2019
50	Bedminster	Advertising Next To 267 West Street Bedminster Bristol BS3 3PZ Replacement of existing internally illuminated 48-sheet backlight advertising display with an internally illuminated 48-sheet digital advertising display. Appeal against refusal Delegated decision	Appeal dismissed 30/01/2019
51	Ashley	62 North Road Bishopston Bristol BS6 5AF Excavation and construction of a new driveway. Appeal against refusal Delegated decision	Appeal allowed 30/01/2019 Costs not awarded
52	Hillfields	62 Hillfields Avenue Bristol BS16 4JP New dwelling. Appeal against refusal Delegated decision	Appeal dismissed 13/02/2019
53	Ashley	The Full Moon Backpacker Hotel And Attic Bar North Street City Centre Bristol BS1 3PR The retention of an automated teller machine and associated signage. Appeal against refusal Delegated decision	Appeal dismissed 18/02/2019
54	Ashley	The Full Moon Backpacker Hotel And Attic Bar North Street City Centre Bristol BS1 3PR The retention of an automated teller machine and associated signage. Appeal against refusal Delegated decision	Appeal dismissed 18/02/2019
55	Ashley	The Full Moon Backpacker Hotel And Attic Bar North Street City Centre Bristol BS1 3PR The retention of 1no illuminated top sign and 1no illuminated logo panel. Appeal against refusal Delegated decision	Appeal dismissed 18/02/2019

56	Stoke Bishop	Home Farm Barn Parrys Lane Bristol BS9 1AF Rebuild the existing part of the house in the rear garden with a lower floor level and to make it wider than at present and create annex accommodation. To lower the level of a part of the rear garden. Appeal against refusal Delegated decision	Appeal allowed 05/02/2019
57	Stoke Bishop	49 The Crescent Sea Mills Bristol BS9 2JT Proposed first floor rear extension. Appeal against refusal Delegated decision	Appeal dismissed 05/02/2019
58	Hengrove & Whitchurch Park	2 Arrowfield Close Bristol BS14 0UQ Proposed two storey side extension. Appeal against refusal Delegated decision	Appeal dismissed 05/02/2019
59	Hillfields	Land At Dominion Road And To Rear Of 135 Ridgeway Road Bristol BS16 3EF Proposed demolition of the garage building and the development of a new single storey 1 bedroom bungalow with associated parking and garden space. Appeal against refusal Delegated decision	Appeal dismissed 27/02/2019
60	Easton	76 Robertson Road Bristol BS5 6JT Adaptation and retention of garage. Appeal against refusal Delegated decision	Appeal dismissed 27/02/2019

DEVELOPMENT CONTROL COMMITTEE B

13th March 2019

REPORT OF THE SERVICE DIRECTOR - PLANNING

LIST OF ENFORCEMENT NOTICES SERVED

Item	Ward	Address, description and enforcement type	Date issued
1	Bedminster	Land Adj 5 Winterstoke Road Bristol BS3 2NN To take discontinuance action in respect of advertisement hoarding. Discontinuance notice	06/02/2019
2	Bedminster	Advertising Next To 267 West Street Bedminster Bristol BS3 3PZ To take discontinuance action in respect of advertisement hoarding. Discontinuance notice	12/02/2019
3	Brislington West	77 Talbot Road Bristol BS4 2NP Erection of fencing/structure/enclosure over front garden area Enforcement notice	30/01/2019
4	Central	35 - 37 Stokes Croft Bristol BS1 3PY Installation of external roller shutters and associated shutter housing structures to the front of the property. Enforcement notice	30/01/2019
5	Clifton Down	92 Queens Road Clifton Bristol BS8 1RT Installation of plant and equipment without planning permission. Enforcement notice	12/02/2019

6	St George West	150 Church Road Redfield Bristol BS5 9HN	30/01/2019
<p>Change of use from an Class A3 (sale of cold food and drink to be consumed on the premises) to a mixed A3/A5 use including the cooking of food and takaway.</p> <p>Enforcement notice</p>			
7	Stoke Bishop	88 Shirehampton Road Bristol BS9 2DR	18/02/2019
<p>Erection of fencing on roof of single storey rear element without planning permission.</p> <p>Enforcement notice</p>			

Development Control Committee B 13 March 2019

Report of the Service Director - Planning

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Planning Applications

Item	Ward	Officer Recommendation	Application No/Address/Description
1	Central	Refuse	18/04977/P - Trust Headquarters Marlborough Street City Centre Bristol BS2 8CC Outline planning application to consider Access and Scale (with Appearance, Landscaping and Layout reserved) for the demolition of all existing structures and the erection of a hospital transport hub, comprising a 400-space Cycle Centre, 820-space hospital-only car park, hospital bus drop-off point and associated works (Major).
2	Knowle	Grant subject to Legal Agreement	18/05184/P - Broadwalk Shopping Centre Broad Walk Bristol BS4 2QU Outline planning application with all matters reserved other than access for the partial demolition, refurbishment and redevelopment of Broadwalk Shopping Centre and adjacent land to provide a mixed use scheme comprising residential apartments (C3), retail floorspace (A1/A2), cafes, bars and restaurants (A3/A4/A5), offices (B1), community (D1) and leisure uses (D2). Provision of vehicular and pedestrian .cycle accesses, parking, servicing, landscaping, public realm and associated works.
3	Eastville	Grant subject to Legal Agreement	18/06358/F - Alexandra Park Public Car Park Alexandra Park Fishponds Bristol BS16 2BG Proposed sui-generis residential development of 34 studio units, a communal space, an external communal deck, a refuse store and a cycle store in prefabricated container units located an existing surface car park at the end of Alexandra Park, Fishponds, Bristol.
4	Stoke Bishop	Grant	18/04727/F - Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ Demolition of existing WCs. and construction of cafe, replacement WCs and education booth.

Item	Ward	Officer Recommendation	Application No/Address/Description
5	Lawrence Hill	Grant	18/04108/F - St Gabriels Court St Gabriels Road Bristol BS5 0RT Demolition of all buildings, altered access and construction of a mixed use development comprising 'flexible' commercial uses (Use Classes: A1, A2 and/or B1(a-c) along with 10 dwellinghouses with associated car parking, refuse and cycle storage.
6	Brislington West	Grant	19/00253/H - 6 Alford Road Bristol BS4 3HS Single storey side/rear extension; replacement garage; and associated works.

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Development Control Committee B – 13 March 2019

ITEM NO. 1

WARD: Central **CONTACT OFFICER:** Susannah Pettit
SITE ADDRESS: Trust Headquarters Marlborough Street City Centre Bristol BS2 8CC

APPLICATION NO: 18/04977/P Outline Planning

DETERMINATION DEADLINE: 14 January 2019

Outline planning application to consider Access and Scale (with Appearance, Landscaping and Layout reserved) for the demolition of all existing structures and the erection of a hospital transport hub, comprising a 400-space Cycle Centre, 820-space hospital-only car park, hospital bus drop-off point and associated works (Major).

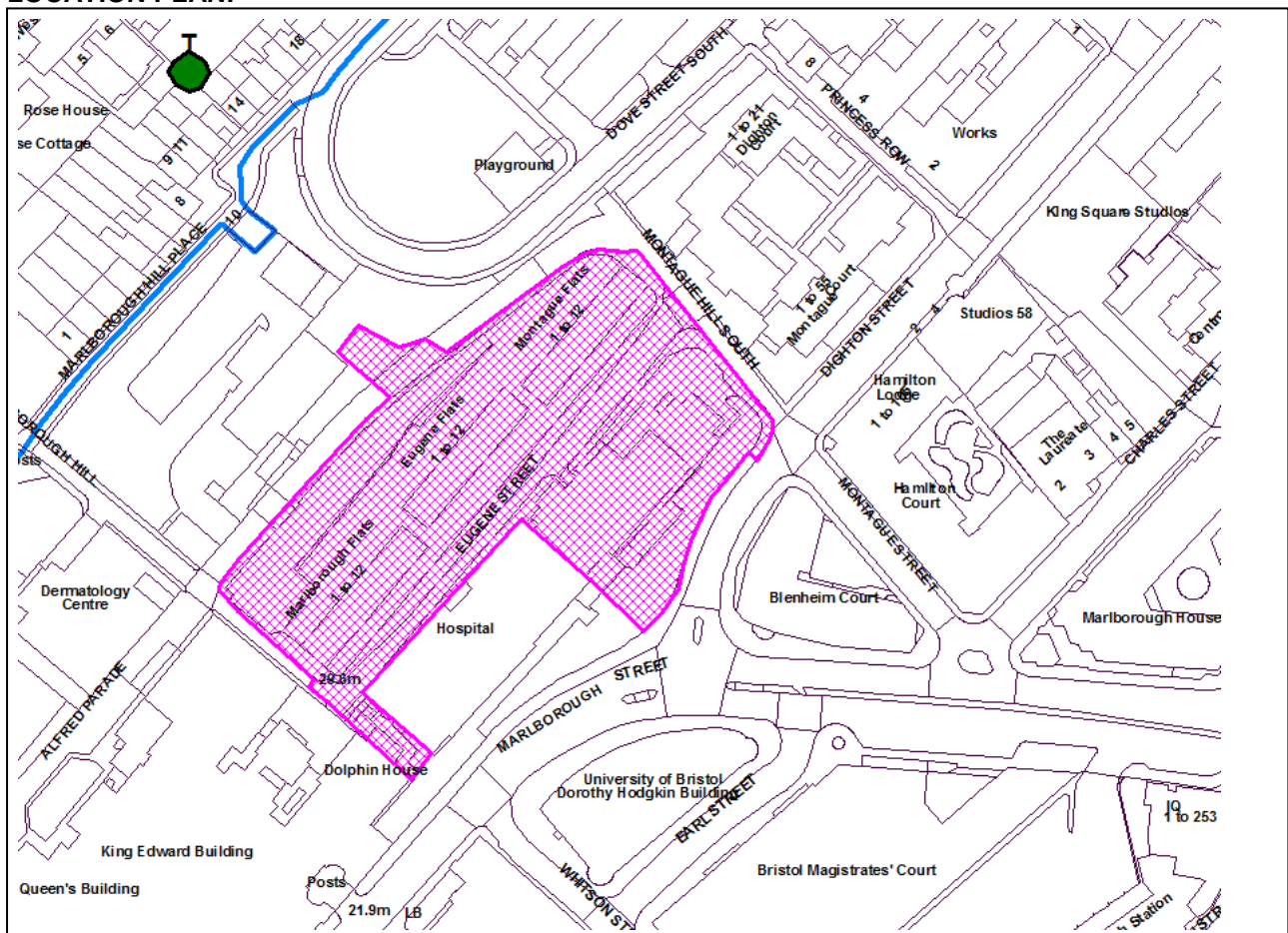
RECOMMENDATION: Refuse

AGENT: WYG
 90 Victoria Street
 Bristol
 BS1 6DP

APPLICANT: UBH NHS Foundation Trust

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



Development Control Committee B – 13 March 2019**Application No. 18/04977/P : Trust Headquarters Marlborough Street City Centre Bristol BS2 8CC****SUMMARY**

This application is made by the University Hospital Bristol Trust. It is an Outline application, seeking approval for Scale and Access only, leaving Appearance, Layout and Landscaping to Reserved Matters.

The application seeks approval for an eight storey car park ('Transport Hub') on the block bounded by Eugene Street, Marlborough Hill South and Marlborough Hill. The application would include the demolition of 3 blocks of flats on Eugene Street, which are on the Local List.

The Hospital Trust have set out that there is a need for the transport hub since many of the patients travel a significant distance, with the hospital being the regional centre for paediatric burns and brain injury, cancer and adult cardiac services and the Children's Hospital. In addition, a number of patients and visitors are unable to use public transport, and require nearby car parking, frequently finding it difficult to find parking spaces which results in stress and missed appointments.

In assessing the supporting documents for an Outline application, officers are required to reach a view on the overall principle of development as well as those matters that have been applied for - in this case Scale and Access. In reaching this view, matters of principle for this application include Transport, Amenity, Sustainability, Design, Heritage and Trees, and officers have concluded that the application contains insufficient justification or mitigation for the type of development proposed, such that it would bring about unwarranted harm.

A significant number of responses have been received to public consultation, with strong letters of both support and objection. There have also been objections from Councillor Smith, Councillor Negus and Councillor English.

Members will see from the report that officers have considered all the issues and have come to a recommendation of refusal, recognising that as with any major scheme there are a number of competing demands.

The recommendation is to refuse the application on the grounds of Transport, Air Quality, Loss of Housing, Heritage/Design, Amenity and an unmitigated loss of trees.

SITE DESCRIPTION

The application relates to a piece of land owned by the University Hospital Bristol Trust, ("The Trust"), bounded by Montague Hill South, Marlborough Hill, Marlborough Street and Dighton Street. The site also encompasses Eugene Street.

The application site plan encompasses an existing 3 and 4 storey multi-storey car park, which is accessed from Montague Hill South, as well as the residences in Marlborough Flats, Eugene Flats and Montague Flats - 36 flats in total. These buildings are three storey, red brick inter-war residential properties. According to the application supporting documents, 8 of these are uninhabitable. The properties are Locally Listed, undesignated heritage assets.

The hospital precinct is predominantly arranged to the south and south-west, with student accommodation and the Magistrates Court to the south-east. Prevailing building heights within this area are between 6-8 storeys. To the north and east the buildings drop down to a smaller scale with more residential uses as the land rises up Dighton Street. There are relatively steep level

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changes across the site, going from south-east to north-west.

In terms of policy designations, the site is within the City Centre, the Hospital Precinct, and the St Michael's neighbourhood as defined by the Local Plan. The site is not within a conservation area, although it is close to the Kingsdown, Stokes Croft and St James Parade Conservation Areas which are all roughly equidistant from the site. The nearest listed buildings are Grade II listed Montague Court, Montague Hill South (2 Dighton Street), and nos. 1-4 (consec) Marlborough Hill Place to the north west. There are a number of trees on and around the site, most notably, a row of category B trees along the northern edge of the site to the rear of the Eugene Street flats.

RELEVANT HISTORY

A pre-application enquiry (BCC ref: 16/00820/PREAPP), seeking advice on the same development as proposed here, was responded to on 11 April 2016.

Advice was that the proposal would appear to run counter to Bristol City Council's commitment to reduce car trips. It was advised that a robust case would need to be put forward before the principle could be accepted, with the applicant being advised to focus on the following aspects: traffic modelling, assessment of key junctions, strategy for decommissioning existing car parks, review of Travel Plan, air quality management, mitigation for the loss of affordable housing units on site, design quality, overshadowing, sustainability and community involvement.

Application at The Glen Hospital, Redland Hill (BCC ref: 17/00799/F)(which the applicant uses as a comparison).

Planning permission was granted on 19.04.18 for the erection of a 3 tier (four storey) decked car park above the ground floor level with associated landscaping works to provide 263 car parking spaces (including car sharing and car pool facilities), 14 disabled parking spaces and 4 electric car spaces along with further 22 cycle parking spaces.

APPLICATION

Outline planning permission is sought, to consider Access and Scale only (with Appearance, Layout and Landscaping reserved for future applications) for a multi storey transport hub containing 820 car parking spaces and 400 cycle parking spaces. The transport hub would be for hospital use only.

The transport hub would be 8 storeys high (when seen from the south) would be positioned on the block between Eugene Street and Montague Hill South. The vehicular access to the transport hub would be from Eugene Street at ground floor level, with the exit onto Montague Hill South from level one.

Access to the car park would be restricted to hospital patients, staff and visitors. The application indicates that technology such as barcodes on letters would be used to restrict public access, and that there would be charges in place at the same rate as existing car parks on the site. 34 car parking spaces would be for disabled users, the remaining 786 spaces would be standard spaces, 16 of which would be for EV. The disabled parking facilities would be at each end of the building at the internal circulation hubs. A staffed information centre would be available.

The proposal also includes a new shuttle bus stop, cycle and changing facilities, reception area and upgrades to the surrounding public realm, to include a landscaped area and improvements to Marlborough Hill steps.

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The residential buildings on Eugene Street would be demolished to make way for the proposal, with Eugene Street becoming the access road into the car park and remaining as public highway. Montague Hill South (which is currently a private road) would continue to be open for through traffic between Kingsdown and Dighton Street.

The application documents set out that the need for the car park stems from a lack of parking currently on the hospital site, and that the UHB's Estate Strategy 2015-2020 indicated a proposal to create around 1,300 additional parking spaces. Patient feedback surveys have indicated that the lack of parking is the biggest cause of distress for patients, and that trying to find a car parking space often means patients are late for appointments or miss them entirely. These delays and missed appointments impacts on the care that can be provided for the patients concerned, as well as appointment times for other patients and generally the efficient running of the hospital.

The Hospital currently has parking for 349 spaces in 22 car parks surrounding the hospital campus. The application sets out that 4 car parks (containing 192 spaces, of which 31 are disabled bays) would be closed upon delivery of the proposed transport hub. With the closure of these car parks the total uplift in spaces would be 628. These car parks are at MEMO (on Eugene Street); the existing Trust multi-storey car park within the site; (which is proposed to be demolished upon delivery of the Transport Hub); Estates Road and SWEB (on the north side of Montague Hill South). The application contains no information on what the intended uses would be for the decommissioned car park sites, including the one within the application site.

The application supporting documents state that the proposal would not result in any more vehicles on the network than currently exist as there would be no change in activity levels. The application is made on the basis that the proposal would remove the need for people to drive around the local area looking for a parking space as they would be assured of a space within the new car park.

The application is supported by the following drawings and supporting documents:

- Air Quality Assessment
- Heritage Statement
- Statement of Community Involvement
- Sustainability Statement
- Transport Statement
- Travel Plan
- Tree Survey
- Planning Statement
- Design and Access Statement
- Drawings showing Access and Scale
- Indicative drawings showing layout and elevations

EQUALITIES ASSESSMENT

During the determination of this application, due regard has been given to the impact of the scheme in relation to the Equalities Act 2010 in terms of impact upon key equalities protected characteristics. These characteristics are age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation. This application could impact upon some groups with protected characteristics. Whilst there is potential for benefits for groups with protected characteristics, these benefits are weighed against the harm, and the issues are given significant weight. This balance is explored in the Key Issues section of this report.

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PRE APPLICATION COMMUNITY INVOLVEMENT

Due to its size, the application is required to be accompanied by a Statement of Community Involvement. Guidance and good practice examples exist to inform the choice of appropriate methods in order to help ensure effective, efficient, transparent and accountable community involvement. Those responsible for undertaking community involvement are expected to reflect such good practice to ensure inclusive, fair and effective initiatives. Failure to do so may limit the validity and relative credibility of the involvement undertaken.

The application is supported by a Statement of Community Involvement (dated September 2018) which has been assessed. The Statement of Community Involvement shows that the applicant undertook a comprehensive exercise, using a number of outreach processes. The process and outcomes of the SCI are summarised below:

i) Process

- The applicant undertook community engagement in 2017/2018. Firstly, they identified groups for engagement. These were, local elected members, patients' groups, community stakeholders (First West, Bristol Civic Society, Kingsdown Residents Association, Christmas Steps Arts Quarter and Hartcliffe and Withywood Community Partnership) and residents from the surrounding area.
- Members were also taken on tours of the University Hospital site and the BRI.
- A Patient Transport Survey was carried out, and the results showed that 57% of respondents visited the hospital by car.
- A public exhibition was held on 19 July 2018, and 2,772 letters were sent to residents living locally, and a further 8,000 letters were sent to Trust members, charities, community health groups and councillors. A press release was sent to the local media to advertise the exhibition. Feedback forms were available at the exhibition.

ii) Fundamental Outcomes

- Number of existing car park spaces is insufficient
UHB response: Planning policy would allow 2,000 spaces, but the Trust are committed to the Travel Plan which encourages sustainable modes of transport
- Uncertainty over the need for more hospital car parking - is this facility for convenience or medical necessity?
UHB response: There is a need for parking availability close to the hospital for patients with a medical need, such as patients with Leukaemia for whom the threat of infection is high and cannot use public transport. An analysis of numbers of such patients has informed the planning application.
- Concern that the Transport Hub would encourage car use and discourage cycling and public transport use;
UHB response: UH aims for at least 53% of patients and visitors to continue arriving to hospital via public transport. The car park would not result in any more vehicles on the network than currently exist. The Trust's Travel Plan is being updated to reflect a greater emphasis on recommending sustainable forms of transport.
- Concerns over air quality impact
UHB response: People would not have to search for spaces. An air quality assessment forms part of the planning application.
- Concern over access from the Hub to the main hospital
UHB response: Marlborough Steps would be improved although we appreciate that this presents

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challenges to more vulnerable patients. If planning permission were to be granted, we would explore moving vulnerable patients around the site using electric buggies.

- Concern over patient and visitor access to the Hub;
- Uncertainty over the need for more hospital car parking;
- Concern that the Transport Hub would encourage car use and discourage cycling and public transport use;

- Concern over loss of UH Bristol-owned housing on Eugene Street.

UHB response: The Trust purchased the flats from BCC in 2009 without conditions, for the purpose of hospital related development. The Trust is not a housing developer and money needs to be spent on providing healthcare services. Tenants of these flats have only ever agreed 6 month leases since the sale. To mitigate this impact, full packages of support would be provided for tenants who would be required to move away from the Eugene Street flats, including a three month notice period.

Aside from the negative concerns raised above, the SCI reports that 90% of the respondents were either in favour of the proposals or in favour in principle with some issues.

RESPONSE TO PUBLICITY AND CONSULTATION

Letters were sent to 597 nearby neighbours and occupiers on 05.11.2018. A site notice and a press advert were also both posted on 14.11.18. The 21 day period given to comment expired on 05.12.2018, although a large number of representations were received after this date and are considered within this report.

In total, 239 objections were received, and 169 letters of support. 9 neutral comments were also received.

OBJECTIONS ARE SUMMARISED:

- Kingsdown Conservation Group: Objection (summarised)

The proposal conflicts with the Bristol Core Strategy and the joint transport plan. The desire to demolish 36 fundamentally sound, purpose built flats is repugnant, and the desire to build another multi-storey car park on the site is offensive. The Trust should integrate its thinking and decision making with the existing parking provision and Bristol's emerging transport strategies. The mass, height and length of the proposed car park would assault Kingsdown's surviving historic townscape by being built to an entirely different scale and at a low level of the escarpment. Views in and out of the conservation area would be obstructed. Both the Grade II Listed Montague Court and the locally listed King Edward VII Memorial Hospital building are immediately adjacent to the proposed car park and the setting of other heritage assets that are in the Stokes Croft and St James's Parade conservation areas would be substantially harmed by the brash new building.

There are no views from the west end of Dove Street or Dove Street South. There is no section drawing showing the car park in relation to the topography of Kingsdown.

- Conservation Advisory Panel: Objection

The Panel considered that the supporting documents had been written at the end of the design process and written on behalf of the client rather than as an objective analysis. The statements failed to note the fact that the site was a part of a landscape of garden houses from the 17th Century onwards. Reference should have been made to the recent English Heritage study of Bristol Town Houses and the desktop study submitted for alterations to no.6 Kingsdown Parade on the hillside above, and appropriate strategies suggested for the recording of any such remains that

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might be revealed in the proposed development.

The proposals would have adverse effects on the setting of several listed buildings, including Montague Court and Charles Holden's locally listed Edward VII Memorial Building and would be seen from at least three conservation areas. This enormous building at the foot of the escarpment would be an offence to the topography. The Kingsdown conservation area character appraisal referred to views out of the area and this building would have an adverse impact on these views. The loss of the existing key worker houses was objectionable.

In summary, these proposals were the final coup in destroying the Kingsdown hillside.

The Panel strongly objected to the proposals.

- Bristol Civic Society: Objection (Summarised)

The existing car park would be demolished, and this could continue to serve its purpose. The loss of the 36 flats would be a retrograde if the car park becomes obsolescent. The Trust's comparison of its parking capacity against hospitals of a similar size leads to a false conclusion as most hospitals built in the 20th Century are suburban. A better comparison would be older, city centre hospitals. These have not increased car parking provision, but instead have developed bespoke hospital access park and ride schemes. The increase in on-site parking is certain to attract more private cars into the city centre and add considerably more traffic and pollution in the city centre. The introduction of a filter light on the north of Marlborough Street would harm the character of the area by removing an area of green space. It would be an additional obstacle for pedestrians and would give traffic a priority over them which is contrary to BCS10.

The Trust has degraded the Eugene Street properties through lack of maintenance. Their 2009 purchase from the Council is not a planning consideration.

TRAFFIC AND TRANSPORT RELATED OBJECTIONS:

- As a staff nurse living in Clifton I struggle to get buses to and from work and this proposal will make it harder. The plan has not considered the staff who work hard to keep the hospital running. The proposal is great for patients and visitors, but better transport links are needed for the whole of Bristol.

- The application is in direct opposition to the Council's planning policies and will remove valuable housing and increase traffic in the centre of Bristol. It will also increase already dangerous airborne pollution levels around the hospital. Alternative options should be selected - for example, improving the bus services to the hospital, and getting buses more of them to stop outside the building. The existing shuttle bus service could also be improved.

- There is already heavy traffic on the streets around the site, and within 5 minutes' walk there is an NCP car park and Trenchard Street car park that are nearly never full. The already congested road junction would be exacerbated and prevent access for emergency vehicles.

- The traffic analysis does not consider traffic circulation properly. This proposal would concentrate all private car access traffic onto the Marlborough Street corridor. Adding more traffic onto Marlborough Hill would be unacceptable. The BRI is a city centre hospital - it cannot provide parking to everyone and trying to is very old-fashioned thinking. There are other better ways to help with transport difficulties. We need fewer car journeys in Bristol, not more.

- By making it easier to park the hospital would encourage more people to drive to Bristol.

- The proposal would lead to an increase in traffic due to the proposal - from increased hospital attendances as well as freeing up space in Trenchard Street car park for vehicles associated with local commerce. The traffic and air quality assessments state that the development will not lead to an increase in hospital/patient journeys. If this is the case, the size of the car park is not justified.

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Conversely, if it is justified, then it will lead to increased car journeys

- We are short of living accommodation in Bristol, and we are over-run with cars. If the housing is no longer required for NHS staff, then make it available for the homeless. If access to the BRI is a problem, then provide more and cheaper public transport.

- The proposal is counter to National policy as well as the Joint Local Transport Plan and emerging Bristol Transport Strategy which both aim to reduce the volume of traffic in the centre of Bristol in order to support more sustainable transport choices, particularly with regard to the air-quality considerations for pedestrians and cyclists. Alternative solutions to tackle how patients and visitors access hospitals should be sought by the applicants, including hospital buses and park and ride options. It is disappointing that as a health organisation, major developer and employer in the area, the UHB Trust have not been more pro-active in supporting healthier and more sustainable solutions.

- Despite the inconvenience caused for some members of the public and hospital staff, more physical activity, not car use, needs to be encouraged for the long term improvement of community health.

- To reach my home on Marlborough Hill I can drive up Montague Hill South, and access Marlborough Hill from Eugene Street. It does not appear that this access would be available if this development goes ahead.

- There is no necessity for a car park to resolve patients' transport problems. The existing free bus service should be extended and improved. Free shuttle buses should link not only with the railway station but also with the metro bus network. A taxi service would be feasible in the future with cheaper, driverless taxis. Discussions about improving existing bus services should be had with the operators now, and not just when the proposed car park is open. There are already several services to help including Dial-a-Ride, the Patient Transport Service, Volunteer Services and the Shuttle. It would be preferable to build on these facilities.

- If in the future, a congestion charge is introduced, the car park would become obsolete

- One of the main reasons cited in the application is to provide parking for disabled patients, yet there are fewer than 5% of spaces allocated for disabled patients.

- Whoever wrote and reviewed the phrase in the transport statement "public transport may be an option for some non-medical necessity visitors" needs to be sent on a disability awareness course. Better public transport, walking and cycling to the hospital will help everyone, no matter what their disability is. Driving is a privilege and could be revoked.

- There is no awareness in the application of how unpleasant the immediate area around the BRI is
- so no consideration on how the design could make things better such as looking at how people walk to the hospital and why it takes so long to cross from the bus station. There is no attempt to address these issues.

Bristol Traffic Project: Objection

Cycle links around the hospital should be improved. There are flaws in the transport and air quality modelling. The proposal would endanger the health of citizens and patients. -The proposal would give rise to dangerous levels of NOx within the wards. Were medical staff consulted on this proposal? It would fall foul of the General Medical Council's good medical practice guidance to "make the care of the patient your first concern"

Development Control Committee B – 13 March 2019**Application No. 18/04977/P : Trust Headquarters Marlborough Street City Centre Bristol BS2 8CC****Bristol Walking Alliance (and Living Streets Bristol): Objection**

The hospital needs better pedestrian access to, through and around it, it needs better transport connectivity and a people-focused transport interchange without excessive parking. The Travel Plan has a number of shortcomings. It does not acknowledge the hilly nature of the location, the uphill walk from the centre, limited number of entrances to the main hospital, or the complex pedestrian routes through the buildings. It does not cover the problems of getting to the site from bus stops or railway stations. It undermines any attempt to model the travel behaviours or visitors/patients that would allow evaluation of current patterns to look at alternative measures. Its only aim is to provide information.

The sustainability statement does not consider the impact on climate change, and no account has been taken of the effect of a potential Clean Air Zone or congestion charges that the Council may introduce in the future.

The transport assessment contains inconsistencies; therefore we do not accept that the additional 628 car parking spaces are needed.

Proposed improvements to the pedestrian access to the transport hub are inadequate. It suggests that patients would walk from the transport hub, down Marlborough Hill, along Marlborough Street and Upper Maudlin Street to reach the main entrance of the BRI. This so called "Healthcare Corridor" is at least 200m without shelter, along a pavement next to the noise and air pollution of constant high flows of motor traffic. The planning application does not explore alternatives or how the route could be improved.

OBJECTIONS TO LOSS OF HOUSING:

- It's surprising that the hospital wants to get rid of 36 flats for key workers who do not need to commute to the hospital and replace them with a privately run car park. The application supporting documents state that recruitment and retention levels are very high, therefore there is no requirement to provide key worker accommodation. This shows a disregard for the staff and their families who currently live in the Eugene Street flats. It also doesn't take into account future uncertainty, given Brexit and the general trend of increasing difficulty of recruitment of hospital staff. There are currently 23 staff members in 27 useable flats, and some 8 are uninhabitable. This can only be due to neglect by the Trust itself which is reprehensible, but also means it could chose to make them more habitable. Bad landlord behaviour should not be an excuse for allowing them to demolish the accommodation.

AIR QUALITY OBJECTIONS:

- The air quality assessment acknowledges that air quality around the hospital will get worse.

- This part of the city already has dangerous levels of air pollution and seriously congested roads.

-The function of a hospital is to improve the health of the public which is to a measurable extent, damaged by poor air quality. UHB needs to take short and long term air quality impacts more seriously and develop a plan that provides reassurance on avoiding any increase in traffic, air pollution and associated respiratory disease. More available car parking would generate more car journeys which in turn damage air quality and would be contrary to any clean air strategy. The proposed layout plans suggest that any increase in vehicle numbers would not be matched by improved levels of free-flowing traffic. Any future successful traffic reduction scheme in Bristol would render the development superfluous. Large city centre hospitals cannot realistically be expected to provide copious patient or visitor parking and should concentrate on alternative forms of access.

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- The Council has pledged to be Carbon neutral by 2030, therefore infrastructure that would lead to an increase in CO2 emissions should not be approved.
- It is estimated that 300 people die prematurely in Bristol due to air pollution. These proposals will worsen air quality and increase the risk to people's health.

DESIGN / CONSERVATION OBJECTIONS:

- The hub will impair irredeemably Kingsdown Conservation Area, ruining both the streetscape and historic views. It is hugely out of scale with surrounding structures and covering it in grass will not help it to fit in. It will harm the Kingsdown, St James and Stokes Croft Conservation Areas.
- The application has not properly considered its impact on the setting of nearby listed buildings
- The Eugene Street flats are an interesting architectural heritage of the 1920s.
- The link from Dove Street to Montague Hill South is already blighted by litter and graffiti and frequently dumped waste. Although the proposal for a 'Welcome Area' in Montague Hill South seems positive, I believe that this could become a further site for graffiti, rubbish and on-street drinking.
- The car park would create a physical barrier to walk into the city centre down Montague Hill
- The car park would attract antisocial behaviour

AMENITY OBJECTIONS:

- The application states that the car park would not be the tallest building in the local area, and would not give rise to any significant adverse impacts in terms of overshadowing or loss of light. This is a dismissive attitude, as clearly the development will involve the loss of light and amenity of some local residents
- The proximity of the proposed car park to numerous dwellings and the Carolina House children's playground is scandalous - both due to increased traffic and increased air pollution. It would also block sunlight from the playground

TREES AND WILDLIFE

- The proposal would involve the loss of a row of established trees on the south-east side of Montague Hill South, behind the Eugene Street flats
- The proposal would result in light pollution which could affect local wildlife

SUPPORT LETTERS ARE SUMMARISED:

- I am a local resident and fully support the plans. What is proposed is really a minimum provision. Public transport can never serve many who come to the hospital so should not be considered as a viable alternative to the car park.
- The transport hub is really necessary for patients, visitors and hospital staff on call community staff, visitor and staff bike parking. It is also a good opportunity to reconfigure the road (Montague

Development Control Committee B – 13 March 2019**Application No. 18/04977/P : Trust Headquarters Marlborough Street City Centre Bristol BS2 8CC**

Hill South) leading to the proposed hub.

- It may be regrettable that public transport is not comprehensive enough but for a hospital serving a large area for many specialist services, it is important to provide convenient and stress free access by car to the BRI.
- The proposal will go a long way to solving the problems of patients in rural areas
- Currently the absence of parking means many patients are late for appointments with the inevitable knock on effect on waiting lists and time delays which in turn wastes money
- In patient surveys, the most negative part of their care was recorded as difficulty parking
- I believe that those who oppose this scheme have never had to support someone who is very sick to get to the hospital. Frankly it is inhuman to ask those who are very ill or those supporting the very ill (which can be deeply stressful) to do so via current methods.
- As a volunteer organisation which drives elderly and vulnerable patients to the Oncology Centre and Bristol Heart Centre, the proposal would help, as all we can do is drop patients off and leave the area due to lack of parking facilities. A number of patients require assistance entering the hospital entrance and finding their way to their appointment.
- The current provision for disabled parking at the hospital, and the public car parks is very limited and largely inaccessible to disabled people due to the distance and topography of Bristol. The provision of this car park far outweighs the loss of key worker home provision and the provision of housing in the city resides elsewhere and it should not be for the UH Bristol NHS Foundation Trust to put this in front of the demand for adequate parking provision.
- The proposed cycle hub is welcomed
- I support, but the car park needs to be of a sizeable height to fit a disabled car.

The Christmas Steps Arts Quarter's Traders support this proposal. Hundreds of out-patients now clog up the spaces outside the shops in Colston Street, Perry Road, Upper Maudlin Street, Lower Park Row, St Michael's Hill and Horfield Road as well as Trenchard Street car park. The proposal would enable proper shopper customers to park, patronise the local shops, businesses and restaurants to keep them afloat in these extremely difficult times.

- UHB needs better transport links and facilities - this would aid staff and patients alike and really enhance the service UHB provides.
- As the mother of a child who had weekly appointments at the children's hospital in a wheelchair, this project is vital for those using hospital services who don't qualify for full disability status.

NEUTRAL COMMENTS ARE SUMMARISED:

- The number of disabled parking bays is very small. I support families who need to park at the hospital for emergency admissions. This is the main reason for having more car parking. The bike park is unlikely to suit the needs of staff working at the children's hospital. As a local resident I wouldn't support the development in its current form for these reasons.
- Buses are expensive and take a long time. I live 12 miles away but the bus can take 1.5 hours and cost £40-£50 per week, which means I may no longer be able to work for UHBT. This is the situation for a lot of staff.

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- Citizens Advice Bristol frequently have contact with members of the public trying to get to the hospital from city centre car parks. Dedicated patient car parking and better hospital transport links are desperately needed.

- UCL Institute for Environmental Design and Engineering
(Comments are in relation to comment from Bristol Traffic Team) We have not published the results of our investigations yet but the Bristol Traffic Team have used a statement related to our work, within their objection. The results relating to indoor areas are therefore inconclusive.

Ward Members

COUNCILLOR PAUL SMITH (objection):

It's difficult to know where to start when an application flies in the face of so many planning principles and policies:

- 1) The city centre already has sufficient car parking, this will just attract in more vehicles, increasing congestion and pollution
- 2) There is already a car park on site which is currently used for staff. If staff car parking was reallocated to another city centre car park, e.g. Trenchard Street, the issue of priority patient car parking could be addressed
- 3) The loss of 36 affordable key worker rented flats which are much needed. The trust has been degrading these properties through lack of maintenance and several are already empty and decaying. There is no provision to replace these flats.
- 4) The proposal is contrary to the joint transport plan which is being developed to deal with transport at the sub regional level. It appears that the Hospital have dusted of a 1990s car lead transport strategy when we are moving towards a 21st century transport system. It takes no account of the emerging city centre movement strategy.
- 5) This appears to be finance led not transport led with the desire to generate income for the hospital from parking when there are alternative ways of meeting their parking requirement.
- 6) The basis of the 2009 purchase is not a planning consideration and the current council is not bound by the lack of conditions in a sale made in 2009, nor is planning policy.

I could go on but I'm sure I would stray from purely planning considerations. The hospital has chosen itself to restrict the parking provision it has from patients in general and disabled patients in particular. I do not believe this is the right solution although I'm sure it has the potential to provide significant income to the Trust.

COUNCILLOR ANTHONY NEGUS (objection):

This application is for a commercial multi-storey car park which is a part of proposal that will remove car parking spaces elsewhere in the hospital estate and demolish staff housing in Eugene Street, packaged as a transport hub. It is not in accordance with the Joint Local Transport Plan, or the emerging Bristol Transport Strategy in that it encourages additional cars into the central area of Bristol and thereby worsens air quality in this most sensitive of areas where there are proposals for a clean air zone to specifically address the problems associated with too much vehicle use. In addition, this application also flies in the face of the Joint Spatial Plan with its emphasis on the provision of housing, particularly affordable housing. Instead this proposal requires the demolition of housing which has been made available to key staff. This will put more pressure on the city in general but particularly in areas adjacent to the hospital which, with other pressures from the adjacent university, will have an impact on the availability and affordability of housing for others. The term 'transport hub' suggests a positive, multifaceted solution to an acknowledged problem of patient access to the hospital. Yet in the latest redevelopment very poor provision was made for dropping-off of patients and there appears to be no better provision of a hospital circular bus service. There has also been no improvement to the very poor commercial bus services to the BRI. There is a mis-match between a bus company operating only where it can make a profit and a

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development designed to pay for itself through a private finance process; there is a better solution which neither can achieve without a bigger view.

Finally it is particularly ironic that UBHT should have at its heart all aspects of health when this proposal for more cars coming into central Bristol with an effect on our poor air quality that will further damage health.

I urge you to reject this application and as a result hope that a more considered solution than has been presented may be found.

COUNCILLOR JUDE BREW ENGLISH (objection):

We need homes not car parks. Planning wise loss of local amenity. Pollution levels raised.

OTHER COMMENTS

Nature Conservation Officer has commented as follows:-

In the event of an approval, conditions would be recommended, requiring details of a green roof and bird and bat boxes to be incorporated within the development.

Contaminated Land Environmental Protection has commented as follows:-

The application to develop a new car park has been reviewed in relation to land contamination.

The applicants are referred to the following

' Bristol Core Strategy - BCS23 Pollution

' Local Plan ' DM34 Contaminated Land

' National Planning Policy Framework (2018) Paragraphs 118, 120, 170, 178, 180

' Planning Practice Guidance Note <https://www.gov.uk/guidance/land-affected-by-contamination>

' <https://www.bristol.gov.uk/planning-and-building-regulations-for-business/land-contamination-for-developers>

As this is a major application a minimum of a phase 1 desk study looking into contamination must be submitted to the local planning authority. If any information is already prepared submission prior to determination is encouraged to reduce the burden of pre-commencement conditions. Asbestos containing materials are likely to be present within the fabric of the buildings earmarked for demolition.

If not available it is recommended the standard conditions B11, B12, B13 and C1 are applied to any future planning consent.

Given the proposal is for a high rise building and the site is on a relatively steep hillside, geotechnical considerations should also be considered at this stage and it is recommended a geotechnical specialist is consulted as part of the application for reserved matters.

Energy Services has commented as follows:-

Given its location to the proposed City centre heat network, we would expect the development to connect to this heat network.

However, the Trust already operate a heat network that serves a number of the adjacent buildings so one possible option is to require this building to be connected to the Trust's rather than a BCC operated network although we would probably want to ensure that the Trust heat network is future proofed for connection to the BCC network as theirs is unlikely to become a zero carbon network without this connection

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A full detailed sustainable drainage strategy would be required. This should be designed in accordance with the West of England Sustainable Drainage Developers Guide available at: www.bristol.gov.uk/planning-and-building-regulations/flood-risk-drainage-and-development.

Sustainable Cities Team has commented as follows:-

Given increasing awareness and concern about the public health impacts of local air pollution in Bristol, I am concerned about a transport strategy for staff and patients which is so heavily based on the use of private cars and which is likely to increase their use in the city centre. However, in terms of compliance with sustainability policies in the Local Plan, I am not minded to formally object notwithstanding the concerns outlined above. The following comments are made :

- As indicated in the Energy Statement the numbers and calculations provided are based on high level estimates. Detailed calculations will be required at subsequent stages of the planning process.
- The Domestic Hot Water demand (DHW) for the public WC appears high. I would suggest requesting clarification of this figure at the next stage.
- Further information on the selection and use of materials will be required at the next stage of the planning process.
- Charging points for electric vehicles should be provided in line with current policy. Though not a policy requirement the applicant is strongly encouraged to provide the infrastructure (i.e. ducting and cabling) to enable the number of EV charge points to be increased over time in line with projected demand.
- The initial layouts provided suggested that disabled parking will be distributed across the floors. I recommend that the location of disabled parking bays is reviewed to ensure this meets best practice.

Transport Development Management has commented as follows:-

Objection - please see Key Issue (B).

Urban Design has commented as follows:-

CDG recommends refusal to the application due to loss of unlisted buildings of merit, impact on setting of conservation area, loss of trees about impact on townscape.

Although the design of the building and the public realm is a reserved matter, we remain sceptical whether the ambition for the quality and vibrancy of public space around what's essentially a upgraded car park will be realised.

Please see full comments at Key Issue (D).

Air Quality has commented as follows:-

Objection - please see Key Issue (C)

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RELEVANT POLICIES

Planning (Listed Buildings & Conservation Areas) Act 1990
National Planning Policy Framework – February 2019

Bristol Local Plan comprising Core Strategy (Adopted June 2011), Site Allocation and Development Management Policies (Adopted July 2014) and (as appropriate) the Bristol Central Area Plan (Adopted March 2015) and (as appropriate) the Old Market Quarter Neighbourhood Development Plan 2015.

Planning Obligations - Supplementary Planning Document - Adopted 27 Sept 2012
Kingsdown Conservation Area Character Appraisal
Stokes Croft Conservation Area Character Appraisal
St James Parade Conservation Area Boundary Review

In determining this application, the Local Planning Authority has had regard to all relevant policies of the Bristol Local Plan and relevant guidance.

KEY ISSUES

(A) IS AN OUTLINE APPLICATION CONSIDERED APPROPRIATE?

The application has been submitted in outline form, and seeks to establish the principle of development as well as Access and Scale. Appearance, Landscaping, Layout have all been reserved for future consideration.

Notwithstanding the above, indicative plans were submitted with the application showing the anticipated appearance, layout and landscape proposals. These have not been assessed and have been used for information purposes only.

Whilst in some respects, a full assessment of the principle of the scheme needs to be concerned with Layout and Appearance in addition, officers have accepted the application as valid, and deem that the information submitted is sufficient to determine an outline application.

(B) PRINCIPLE OF DEVELOPMENT

Core Strategy policy BCS2 sets out the regional focus of Bristol City Centre's role, which, the policy states, will be strengthened by the development of office space, new homes and improved transport systems and connectivity including new public transport, pedestrian and cycle routes and transport hubs. The same policy specifically highlights that there will be continued expansion of the University of Bristol and the Bristol Royal Infirmary sites.

Central Area Plan policy BCAP11 recognises that the University Bristol and the Bristol Royal Infirmary are major institutions that make a considerable contribution to the economy and mix of uses within the city centre. It sets out that the Hospital Precinct will be developed for healthcare and ancillary uses associated with the University Hospitals Bristol Trust.

There is a critical need to substantially boost housing supply in Bristol, and this strategic priority is already supported by the existing suite of local plan policies. Core Strategy policy BCS5 sets out the need for 30,600 new homes to be provided in Bristol by 2026. In reference to existing homes, this policy requires that in order to maintain the existing housing stock, existing homes will be

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retained unless they are unsuitable for residential uses, would be used for essential local community facilities, or would be replaced.

The main land use issue for this application is therefore to consider whether the provision of the proposed transport hub, which the application supporting documents describe as being an ancillary use associated with the UHB Trust ("The Trust") would outweigh the loss of 36 homes.

For the use to be considered 'ancillary' it would need to be demonstrated that its operation relied upon a degree of interdependence with the Hospital, such that it would not be deemed a separate planning unit in its own right.

The Planning Statement supporting the application states that the car park would only be used by patients, visitors and a proportion of staff, and that technology would be used (such as barcodes on appointment letters) to prevent access by non-hospital related cars. Officers are therefore satisfied that the proposed development would be seen as ancillary to the hospital.

The applicant sets out their argument that the wording of policy BCAP11 should be interpreted to mean that development by the Hospital within the Hospital Precinct is acceptable, in view of the way it is worded - that the "Hospital Precinct WILL [officer emphasis] be developed for healthcare and ancillary uses".

Officers are in disagreement with the applicant on this matter. Policy BCAP11 states that the Hospital Precinct will be developed for healthcare and ancillary uses associated with the University Hospitals Bristol Trust and encourages the development of new facilities or the redevelopment and renewal of existing facilities. However, this policy operates within the context of other local plan policies which are also relevant to the consideration of these proposals. This policy is therefore generally supportive of hospital development but says nothing about parking proposals and does not, in the view of officers, override other policy considerations.

Furthermore, the site in question forms part of a "precinct designation" rather than a Site Allocation. Site Allocations are identified in the Central Area plan as:

"Sites to be allocated for development for particular land uses."

The same definition does not apply to the precinct designation as it is not an allocated site and is not supported by specified land uses or development considerations. It is therefore true to say that the designation is designed to encourage and facilitate development by the hospital, but not exclusively, and not when that development would be of detriment to the delivery of other local plan policies.

Notwithstanding this consideration, the only discussion within the application documents surrounding the loss of homes, is that the applicant bought the properties from the Council in 2009, and there was no condition of sale that the properties had to remain within residential use. The application documents state that the properties are in a poor state of repair, and that residents have been on short-term tenancies since the hospital purchased the properties.

It should be noted that ten of the objections received in response to public consultation are from residents of these flats who work in the hospital and object to having to find new accommodation.

The Trust states that, as a hospital, they are not required to provide key worker accommodation. It is accepted that the applicant must assess their assets and balance them against their needs, however, planning policy (including the NPPF) is clear regarding the LPA's responsibility in ensuring an appropriate supply of housing. Exceptional justification would therefore be needed to

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allow development that would lead to the loss of viable housing stock, yet the application does not demonstrate that advice offered at pre-application stage has been followed, as there has been no evidence of the Trust having engaged with this issue. The application does not, for example, give details of any maintenance that has taken place since the Trust acquired the flats, which could demonstrate that reasonable efforts had been made as to the upkeep of the properties.

In view of the above considerations, it is not considered that the need for the car park outweighs the loss of 36 residential units on this site. There is insufficient evidence to demonstrate that the residences are unsuitable for residential uses, and this is supported by the representations from residents of these flats.

Policy BCS 5 projects continued high levels of in-migration to the City, leading to negative consequences such as increased commuting into the city, increased overcrowding and an increased demand for housing to support the workforce or population pressures which would exceed supply. The loss of 36 residential units would therefore be contrary to this policy.

(C) WOULD THE PROPOSAL IMPACT ON HIGHWAY SAFETY, OR INTRODUCE A SEVERE IMPACT ON THE ROAD NETWORK?

Section 9 of the NPPF requires all developments that generate significant amounts of movement to be required to provide a Travel Plan, and the application should be supported by a Transport Statement/Transport Assessment. It also states that in assessing applications, it should be ensured that:

- appropriate opportunities to promote sustainable transport modes can be - or have been - taken up, given the type of development and its location;
- safe and suitable access to the site can be achieved for all users; and
- any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree.

It prioritises pedestrian and cycle movements and requires improvements to the local public transport infrastructure, requires development to address the needs of those with mobility impairment, create places which are safe and secure, allow efficient deliveries and provide for low emission vehicles. Furthermore, developments generating significant amounts of traffic require a Travel Plan. It also states that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

BCS10 sets out development principles, and places pedestrians and cyclists at the top of the hierarchy of road user priorities. It states that proposals should minimise the need to travel, especially by private car, and maximise opportunities for the use of walking, cycling and public transport. The same policy also states that developments should be designed and located to ensure the provision of safe streets and reduce as far as possible the negative impacts of vehicles such as excessive volumes, fumes and noise. Proposals should create places and streets where traffic and other activities are integrated and where buildings, spaces and the needs of people shape the area.

Central Area Plan policy BCAP29 states that proposals for long-stay public car parking will only be acceptable where it would replace existing provision and would be appropriately located within the hierarchy of vehicular routes in the city centre. Proposals for short-stay car parking will be considered on a case by case basis having regard to the nature and requirements of the proposed development, the position of the site in the hierarchy of vehicular routes and the desirability of reducing car use in favour of more sustainable modes of transport.

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The site is located within the City Centre. It is therefore sustainable and easily accessible by a choice of transport modes. Despite this, existing congestion occurring during the day, and particularly in peak periods, results in the highway network operating beyond its available capacity to the detriment of road safety and air quality. This negatively impacts on the health and wellbeing of Bristol's citizens.

Detailed comments have been made by the council's Transport Development Management Officer, and these are available to view on the Council's website. The comments were provided to the applicant, and they were given the opportunity to respond. The Trust subsequently submitted a rebuttal to the comments by way of an additional Technical Note (TN01) and final TDM comments on this are also both online. Officers' assessment of both submissions is summarised below.

(i) Trip Generation / Traffic Impact

Trips have been modelled using patient data and surveys of visitors and staff. Staff data has not been included in additional trip generation, as it has been assumed that a similar number of staff parking spaces will be retained, so no additional trips would be generated.

Modelling of various junctions has been undertaken which indicates that there are currently no capacity issues with the highway network and that there are none proposed into the future, with the development and the background traffic growth. This has not taken into any consideration any alterations arising from the emerging City Centre Framework, and doesn't appear to have modelled the committed development at Callowhill Court. The latter includes a car park of 380 parking spaces, the size of which was reduced significantly and access redesigned from the original proposal due to a lack of capacity on the highway network.

Anyone familiar with the area would be confused by the outcome of the modelling, as this link experiences significant congestion throughout the daytime.

The proposal is for a net parking increase of 628 parking spaces. The applicant sets out that this figure is still below that allowed by adopted parking standards, however, the Council's parking standards are maximum standards, with the stipulation that where appropriate in Bristol City Centre, a significantly lower level of car parking provision will be expected.

It is clear without any modelling work that this will have a significant impact on the network, particularly concentrated on the Marlborough Street / Dighton Street node. The applicants suggest that the increase in traffic arising from this will be partly offset by the reduction in movements cruising searching for a parking space.

It is highly unlikely that this will be offset by such an extent, and the benefits of the reduction in these movements will be minimal, and will not offset the impacts of the development proposal. It is apparent from the modelling compared to site visits and knowledge and experience of the area, that the modelling is not reflective of the existing circumstances. Most significantly, the model has not been validated alongside the existing queue lengths. The models used make the assumption that the route downstream of each junction is clear and with unobstructed space for the junctions to discharge traffic into. This is not the case, as traffic can build up from the end of the M32. Capacity at all of the junctions is therefore not reflected fully in the models.

The modelling provided therefore fails to effectively demonstrate that the impact on the highway network of an additional net increase of an additional 628 parking spaces on this essential part of the highway network would not be severe.

The additional traffic and the associated congestion arising will not only have an effect on journey times, but will also have significant impact on the quality of the pedestrian environment, clear and

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unobstructed cycle routes, air quality and the ability of emergency services to use this route. It will also impact on the free flow of passenger transport on this and other parts of the highway network.

(ii) Access / Visibility

The accesses to the car park have been demonstrated to have a visibility of 2.4m x 25m, although the bus stop is in the visibility splay of those exiting the car park. This would require redesign. The principle of access at these locations depends on a number of factors, including potential queue lengths, the operation of safety barriers into the car park and the time taken to access the car park, safety reviews and visibility at the junction of Montague Hill South with Dighton Street. The safe and efficient operation has not been demonstrated, and it is not clear that the queues will not impact on the safe and effective operation of Dighton Street and Marlborough Street.

The indicative proposals for landscaping and road layout in Eugene Street suggest that it would not be considered suitable to be retained as adopted highway. It would therefore be expected that Eugene Street would be stopped up as highway, as this would no longer serve a highway function, merely access to the car park, and private servicing for the Hospital. The access for the 25 deliveries / week for King Edward Yard / Pharmacy could be maintained.

To maintain public access to Kingsdown, Montague Hill South would therefore be required to be constructed to an adoptable standard and offered for adoption. This would be a more suitable route for access by servicing vehicles, particularly into Alfred Parade, which is the main servicing access for the Hospital site, receiving 400 deliveries a week. In response to this, the applicant stated that Montague Hill South is a private road within the control of UHB and cannot be constructed to adoptable standards. This raises concerns regarding access around this site to neighbouring areas.

Marlborough Hill between Eugene Street could then be closed to vehicular traffic to take the opportunity to improve pedestrian access in this area.

A swept path has been provided of a refuse collection vehicle turning within the Alfred Hill access, which results in the rear of the vehicle touching the wall of the King Edward Building which is unacceptable. Alternative arrangements must be made or the building redesigned / footway realigned to avoid this.

(ii)(a) Pedestrian Environment

The Transport Assessment describes a walking distance of 300m from the hub, to the UHB buildings. There has been no satisfactory assessment of the existing walking environment along this route, to assess the suitability of this route for an increased number of pedestrians, especially the mobility impaired. The pedestrian environment in the area is particularly difficult and not pleasurable to negotiate. Whilst the footways along Marlborough Street are continuous, have a width of between 3-6m, and are protected from the roads with guard rails, they are also heavily used, undulating and relatively narrow, and run along one of the most congested and polluted routes in the City Centre. There is no level permeable link from this site into the centre of the precinct, and the proposal does little to address or alleviate this which goes against policy aims.

(ii)(a) Alterations to Eugene Street

Eugene Street is proposed to be the main access for all traffic entering the car park. The TA mentions the widening of footways in Eugene Street, but within the plans, Eugene Street is shown as a hybrid of a shared surface with a 1.6m footway to the south of the site, on the opposite side of the development. It also indicates shared use at its junction with Marlborough Hill. Swept paths demonstrate that swept paths cannot be achieved comfortably without the removal of kerbed areas,

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which are provided to form protected footways for pedestrians.

National guidance has recently indicated that shared spaces are not suitable for public spaces and the indicative proposals for Eugene Street are not satisfactory. The proposals for the access to the car park in Eugene Street are considered unsafe. The opportunity to make this area safe has not been demonstrated, with no definitive plans submitted. The applicant states that this aspect would be covered in the relevant application for reserved matters, however, officers consider that this is a matter of safety and access and therefore falls to be considered under this application, since Access is being applied for.

(ii)(b) New junction Marlborough Street / Dighton Street

The application suggests a potential alteration to the junction at this location, which was discussed with officers in principle prior to the scoping exercise, and which is indicated thematically in the Figures section of the Transport Statement. No safety auditing has been undertaken of the proposals. This part of the proposal would also result in the removal of trees which would be of concern, as this is not reflected in the indicative Tree Survey. This aspect of the scheme would essentially be assessed at Reserved Matters stage (for Layout), however, the principle of the scheme being put forward is in some ways reliant upon the potential for benefits and improvements to the streets surrounding the proposal site, which is a missed opportunity.

(ii)(c) Decommissioned Car Parks

Parking is proposed to be removed from the existing MSCP within the red line of the application, as well as spaces within the MEMO car park, the access road, and the SWEB car park. The latter two are outside of the red line boundary of the application, and there is therefore no ability to ensure the control of the removal of these spaces through a condition.

(ii)(d) Car Park Use

No additional staff parking should be being provided. The Hospital's long term strategy has always been on the basis that there will be a reduction in trips to and from the hospital precinct by staff and patients, due to the relocation of services to more local health centres in the north and the south of Bristol. The various developments permitted throughout the Hospital Development which removed parking throughout the site have been approved with this in mind.

Whilst there would be around 34 disabled bays, the drawings are indicative only as Layout will be for a Reserved Matters application. There is limited detail in the application on how the application would bring about new benefits to vulnerable patients, it says that an electric shuttle could be explored, but there is no commitment to this, or other measures, and it is officers' view that this is an issue of principle, and should not be left to reserved matters.

There is insufficient information about the proposed split of spaces and the management of the car park which would include, but not be limited to details such as the means of operation of access, the split of parking provision between visitors, patients and staff, restrictions on use by the public. The parking policy for the Hospital has not been indicated, and charging information has not been provided, which is an essential part of parking management.

(ii)(e) Bus Hub

The bus hub is indicated and contained within the red line. Swept paths have not been provided, nor details of any waiting facilities (shelters, / raised kerb etc.), crossing points, visibility for turning and visibility around the corner or Montague Hill South. The proposed stop is within the visibility splay for the car park. The turning takes place within 15m of the egress of the car park. The

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proposal is not considered satisfactory or safe.

(iii) Travel Plan

The Council has a Travel Planning guidance document, which provides information on the preparation of a Travel Plan. The Updated Travel Plan submitted with the application refers to a Travel Plan that was made in 2013, although the original document has not been submitted. The submitted update does not contain the detail that would normally be required by the Council's template and guidance.

The update offers less commitment to the existing Green Travel Plan secured by the previous developments on the site. It is vague and does not make any commitments. There are no budgets identified and no measures, targets or incentives outlined. No additional transport facilities have been proposed other than additional cycle parking facilities. It states that achieving a reduction in car trips may be considered impractical given the varying levels of mobility of patients and visitors, however, according to the Hospital's survey only 32% travel by car for medical reasons. There is no commitment to any increase in alternative facilities other than an increase in cycle parking and potential diversion of the HUB bus to a less accessible area for passengers to disembark.

The applicant counters that the Travel Plan Update is robust, and that it provides an acceptable level of detail, however, for the reasons outlined below, it is not considered that the travel plan goes far enough to minimise car trips.

(iii)(a) Staff

From recent surveys of staff, 24% were car trips made alone, 7% as passengers, and the remaining 61% by other modes of transport. According to the surveys, an increase in use by public transport, and a small increase in walking and cycling has been seen since the 2013 travel plan surveys were undertaken.

Whilst the decrease in vehicle trips is welcomed, it is unlikely that the Travel Plan has been instrumental in achieving this reduction. It is more likely to have been influenced by the implementation of the residents parking schemes in St Pauls and Southville in 2014/15. The additional cycle parking provision has been identified as already needed to address a shortfall, with surveys showing 900 members of staff cycling to the site, and fewer than 900 spaces, even with the additional cycle parking provision, available for users.

(iii)(b) Visitors / Patients

No figures have been put forward about the numbers of patients and visitors arriving by car to the site, but according to the Travel Plan, 57% of trips are made by car, 43% by other modes.

A qualitative target has been set for the travel plan, which is the majority of vehicular trips made to the site, because the applicants suggest that patients vary by mobility:

"This Travel Plan will target to provide information to visitors and patients over the lifespan of the Travel Plan, to encourage regular trips to be undertaken by walking, cycling and by public transport through effective user based journey planning."

However, according to the Transport Assessment, 32% who do drive do so for medical reasons. This would indicate that of those 57% arriving by car, there are 68% who don't have medical conditions requiring them to use a car. This would indicate capacity for a reduction in trips by car by patients and staff.

The original Green Travel Plan (2013 update) outlines staff parking policy, which should be reflected in any future Travel Plan. Charging for staff car parking should also be brought more in

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line with local car park charges.

(v) Benefits to Hospital Users

It is not disputed that the scheme would bring about benefits to users of the hospital, and comments describing the need for additional parking have been given careful consideration. As explored above, there are flaws in the applicant's Travel Plan and junction modelling which lead to unrealistic picture of the size of the car park needed as well as the capacity of the road network to accommodate a proposal of this scale.

(vi) Spire Hospital comparison

Noting the applicant's comparison to the Spire Hospital application (BCC ref: 17/00799/F - described in the history section above), in transport grounds, the following are pertinent differences between applications.

- Spire: not within City Centre, with less public transport accessibility than the BRI site
- Spire: net increase in 81 parking spaces (an increase of 29%)
- BRI car park: net increase in 628 spaces (an increase of 94% across the whole site)
- Spire: There are limited opportunities to park elsewhere off-street
- BRI car park: 1200 spaces within off street car parks within 5 minutes' walk to the Hospital
- Spire: committed to a robust Framework Travel Plan and parking management plan for staff
- BRI car park: Travel plan not currently considered adequate to effectively reduce traffic, and no parking management regime included
- Spire: provided full justification for the need of the additional parking, which included reducing the impact of parking congestion on the highway network.
- BRI car park: inadequate justification of need

To conclude the transport consideration, the introduction of an 820 space car park would substantially worsen existing levels of congestion by creating delays to traffic flow throughout the entire city centre and along the M32.

Congestion along the M32, Newfoundland Way, Bond Street and St James Barton roundabout are a major concern on every day of the week throughout the day. The application fails to adequately demonstrate that the impact on the surrounding highway network will not be severe with regard to safe and effective flow of the network. Furthermore, it is not appropriate to increase the size of the car park without demonstrably effective mitigation measures to offset the impacts on safety and congestion, which have not been provided.

The proposal is therefore contrary to the NPPF, BCS10, DM27, BCAP29, BCAP30, BCAP 34 and BCAP43.

(D) AIR QUALITY

Section 15 of the NPPF states that Planning policies and decisions should sustain and contribute towards compliance with relevant limit values or national objectives for pollutants, taking into account the presence of Air Quality Management Areas and Clean Air Zones, and the cumulative impacts from individual sites in local areas. Opportunities to improve air quality or mitigate impacts should be identified, such as through traffic and travel management, and green infrastructure

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provision and enhancement. Planning decisions should ensure that any new development in Air Quality Management Areas and Clean Air Zones is consistent with the local air quality action plan.

Core Strategy policy BCS23 States that development should be sited and designed in such a way as to avoid adversely impacting upon environmental amenity by way of fumes, dust, noise, or any form of air pollution.

Site Allocations and Development Management policies - policy DM33 states that development which has the potential for an unacceptable impact on environmental amenity by way of pollution, but is considered desirable for reasons of economic or wider social need will be expected to provide an appropriate form of mitigation. For development in Air Quality Management Areas, mitigation should take the form of on-site measures, or where appropriate, a financial contribution to off-site measures. The policy also states that development in designated Air Quality Management Areas should take account of existing air pollution and include measures to mitigate its impact on future occupiers where possible and consistent with other policies of the development plan, such as those on climate change and urban design.

The Air Quality Assessment (AQA) submitted with the application uses a modelling approach to predict the significance of the impacts of development on air quality, and this has been assessed by the Council's Air Quality manager who raised objections which are discussed below.

The applicant was provided with the opportunity to respond to initial objections, and in response submitted a rebuttal in the form of a revised Air Quality Assessment which is available on the Council's website. The initial concerns remain in place, as the substantive conclusions of the document remain the same - which is that the development would result in 'moderate' (rather than 'negligible') air quality impacts - meaning that air quality in the area would be worsened.

The increase in pollutant levels is reported as being an over-estimation, however the report provides no evidence for this. The applicant claims that no new trips will be generated by the development, but as set out in the Transport Key Issue, officers disagree. Hence the increase in pollutant levels cannot be regarded as an over-estimation.

Secondly, the AQA does not demonstrate that an acceptable degree of mitigation could be provided. The development will lead to an unacceptable impact on existing concentrations of NO₂ at sensitive receptors. The maximum impact is shown as 1.91 ug_m-3 (microgrammes per metre cubed), which was at the R8 receptor point (which is at IQ Students to the east of the site). The theoretical scenario in Appendix B shows an even higher impact so it is possible that the range of impact could be as high as "substantial" if this scenario is realised.

This actively works against the Council's legal obligation to reduce concentrations of NO₂ to below the legal maximum in the shortest time possible. Under direction from the Secretary of State, officers are developing a plan to do this, and this development will work against the plan, and as a result fall foul of the requirement in the NPPF, which states that new development shall be consistent with the local air quality action plan.

Furthermore, this corridor (Park Row Marlborough Street) is particularly problematic in terms of concentrations due to its topography which traps pollutants, coupled with existing high levels of traffic congestion. This means, that even with measures such as a Clean Air Zone, it would be very difficult for the Council to control nitrogen dioxide (NO₂) concentrations to below 40ug_m-3. The 40ug_m-3 limit is as described within the air quality regulations - and the prior UK Air Quality Strategy. The Council is directed by the government to publish a plan to reduce concentrations to this level in the shortest time possible.

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Officers have also reviewed the scheme's reported impact on dust particulates (PM10 PM2.5), and these are predicted to be of negligible significance, though it is regrettable that any increase is predicted as the Council is also trying to bring concentrations of these pollutants down, not up. In terms of construction impacts, these should be managed as described, by a construction management plan which would be secured by a condition in the event of an approval.

On a wider point, it is of concern that the hospital continues to place so much reliance on catering for journeys by the private car. Emissions from private cars directly impact the health of all citizens and disproportionately affect vulnerable populations.

Some mitigation is suggested within the AQA, however this is focussed only on the construction phase and is not regarded as suitable mitigation for the effect of the car park itself on air quality, and it would not affect the moderate impact predicted.

The development would therefore work against the aims of the Council's developing Clean Air Plan by introducing a moderate impact on air quality in relation to annual mean concentrations of NO₂ and the associated air quality objective. It would worsen air quality in an area proven to have already high concentrations of pollutants, with no commensurate mitigation, and would prevent the Council from fulfilling its legal obligations of reducing nitrogen dioxide to below the legal maximum in the shortest time possible.

(E) AMENITY

Good design and protection and enhancement of the environment are critical components of central government guidance, as identified in the NPPF. Adopted Bristol Core Strategy Policy BCS21 expects development to safeguard the amenity of existing developments and create a high-quality environment for future occupiers. Furthermore, Policy DM27 states that proposals should not prejudice the existing and future development potential of adjoining sites or the potential for the area to achieve a coherent, interconnected and integrated built form.

The nearest residential properties to the application site are at Montague Court and Dighton Court, off Montague Hill South and opposite the application site to the north-east. There are also residences at Hamilton Court and Blenheim Court to the south as well as slightly further up the hill from the site, properties on Marlborough Hill Place to the north-west. There is a play area to the north-west of the site known as Dove Street play area, the enjoyment of this space would also be affected.

The main amenity impacts that would be felt would arise from transport and air quality, and these aspects have already been discussed above. In view of the large volumes of traffic that would be experienced in the local area, the proposal would fail to safeguard the amenity of nearby residents, including users of the play area to the immediate north west of the site.

The application does not explore the impact of the development in terms of a potential loss of daylight or sunlight for nearby occupiers. In Marlborough Street South, there are habitable rooms facing onto the application site. The Planning Statement says that the building would not be the tallest building in the local area, given its position to other existing tall structures and that it will not give rise to any significant adverse impacts on local residents due to overshadowing or loss of light.

This is not considered to be an adequate assessment of the issue, and no BRE Sunlight and Daylight Assessment has been submitted that would enable this conclusion to be quantified. Officers have used the proposed section drawings to carry out a BRE 25° test in respect of properties on Marlborough Hill South and it is likely that the 8 storey car park would result in a loss

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of light from the residential buildings identified, which would result in a harmful loss of amenity, and this shall form a reason for refusal.

Whilst the Layout details are reserved, the application would incorporate improvements to public realm on Montague Steps and other areas surrounding the site, therefore there is potential for the scheme to bring about amenity benefits in this regard. In the event of an approval, a high quality hard and soft landscaping scheme would be an expectation.

(F) SUSTAINABILITY

As embedded in the NPPF, sustainability should be integral to all new development, and should encourage opportunities for development to draw its energy supply from decentralised, renewable or low carbon energy supply systems. BCS13 requires development to mitigate climate change by encouraging walking, cycling and the use of public transport instead of journeys by private car and by incorporating other measures to mitigate and adapt to climate. BCS14 sets out a heat hierarchy for new development, and an expectation that new development will connect to existing CHP/CCHP distribution networks. The same policy also expects development to provide sufficient renewable energy generation to reduce carbon dioxide emissions from residual energy use in the buildings by at least 20%. BCS15 requires developments to demonstrate through a Sustainability Statement how they have addressed energy efficiency; waste and recycling; conserving water; materials; facilitating future refurbishment and enhancement of biodiversity. This policy also expects non-residential development to score a BREEAM 'Excellent' rating.

The above policies seek to ensure that developments respond to climate change in their physical characteristics, however there is a discussion embedded in this report, on the principle of whether this development would be sustainable in itself, being that it is for a car park that would encourage car trips into the city centre.

The application is accompanied by a Sustainability Statement. This sets out that there is no BREEAM certification method that is appropriate for a car park, and therefore the scheme has not been assessed for BREEAM. It states that the development would actively promote sustainable travel, particularly for commuting purposes, through the provision of high quality secure facilities for cyclists. It would also include provision for electric vehicle charging which would mitigate emissions from cars. The document states that the building has been designed so that the more habitable spaces (ancillary office, changing rooms and WCs) have been located away from the south facing elevation to reduce solar gain. It would be constructed with a steel frame to enable easier recycling of materials at the end of the structure's life. The Council's Flood team has commented that a Sustainable Urban Drainage System (SUDS) would need to be incorporated at detailed design stage, and this would be assessed at Layout stage under Reserved Matters, secured by condition in the event of an approval. In order to mitigate the impact of the development, the supporting documents suggest that the car park could incorporate green walls or living roofs as well as a landscaping scheme.

The Energy Statement sets out the building's energy needs and how savings in residual carbon emissions using renewable energy would be achieved against the Part L baseline. This document sets out that the building would have an unregulated energy demand as a result of the demand for electric vehicle charging, notwithstanding also the fact that more accurate energy demands would be known at detailed design stage. The document sets out that the building would be made ready for District Heat connection (to either the Trust operated system or the eventual Council-wide system). This would be secured via s106 in the event of an approval. To reduce the building's energy demands by 20% required by policy, Air Source Heat Pumps and Photovoltaic panels are identified as appropriate forms of renewable technology. In the event of an approval, the applicant

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would be required to incorporate these technologies with further details of the calculations provided at Reserved Matters stage.

To apply the above policies, the sustainability officer made the comments included earlier in this report which are based on the how the scheme could be developed further through the design process in response to sustainability policies.

To an extent, the sustainability merits of this scheme lie within the principle of development itself. These are discussed in other key issues of this report, mainly Transport (Key Issue C) with the Travel Plan being a critical driver on determining the actual need for the car park, and its size. If a car park is to be accepted on this site, officers would need to be assured that all other possible options had been explored by the Trust, through findings of the travel plan and transport modelling, and that adequate mitigation was being proposed for its negative impacts. As explored above, this has not been carried out to the satisfaction of officers, therefore the development is not justified, and would not be a sustainable development.

(G) DESIGN AND CONSERVATION

The NPPF requires that developments:

- a) will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- b) are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;
- c) are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d) establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;
- e) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
- f) create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

Requiring good design is also at the heart of Bristol planning policy, and BCS21 expects a high quality design in all developments, which contributes positively to an area's character and identity, creating or reinforcing local distinctiveness.

DM29 requires all new buildings to (amongst other things) be clearly organised in terms of their form and internal layout and circulation to reflect the hierarchy of function they will accommodate, the uses they will serve and the context they will address; and allow for future adaptation or extension to accommodate alternative uses. The same policy requires buildings to respond to the changing future needs or circumstances of occupiers and to incorporate exteriors and elevations that provide visual interest from a range of viewing distances with a high quality detail of an appropriate scale and proportion.

Whilst the application is in Outline format, for Scale and Access only, certain design matters fall to be assessed within this application and give rise to concern as set out below. There is no indication that these concerns could be overcome in the detailed design phases.

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(i) Harm to Heritage Assets

Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that in considering whether to grant planning permission for development which affects a listed building or its setting, the local planning authority shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses. The Authority is also required (under Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990) to pay special attention to the desirability of preserving or enhancing the character or appearance of the conservation area. The case of R (Forge Field Society) v Sevenoaks DC [2014] EWHC 1895 (Admin) ("Forge Field") has made it clear where there is harm to a listed building or a conservation area the decision maker "must give that harm considerable importance and weight." [48]. This is applicable here because there is harm to the setting of the conservation area caused by the proposals as set out below.

Section 16 of the national guidance within the National Planning Policy Framework (NPPF) 2019 states that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation, with any harm or loss requiring clear and convincing justification.

Paragraph 193 of the NPPF states that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight shall be).

Further, paragraph 194 states that any harm to, or loss of, the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification. It states that substantial harm or loss of grade II listed buildings or grade II registered parks or gardens should be exceptional, and assets of the highest significance should be wholly exceptional.

Paragraph 195 states that where a proposed development would lead to substantial harm to (or total loss of significance of) a designated heritage asset, LPAs should refuse consent unless it is demonstrated that the harm is necessary to achieve substantial public benefits that outweigh the harm or loss.

Paragraph 196 states that where a proposed development will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including securing its optimum viable use.

Finally, paragraph 197 says that the effect of an application on the significance of a non-designated heritage asset should be taken into account when determining the application. In weighing applications that directly or indirectly affect non-designated heritage assets, a balanced judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset.

In addition, the adopted Bristol Core Strategy 2011 within Policy BCS22 and the SADMP policy DM31 seek to ensure that development proposals safeguard or enhance heritage assets in the city.

Officers have undertaken the assessment required under the Sections 66 and 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990 and have given special regard to the desirability of preserving the assets, their setting and features of special architectural or historic interest which they possess. They have given this harm considerable importance and weight.

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In accordance with s66 and 72, considerable importance and weight has been given to the harm caused to the nearby listed buildings and the setting of the Kingsdown and Stokes Croft Conservation Areas.

Montague Court, Montague Hill South (2 Dighton Street) is Grade II Listed. This building has 3 storeys, an attic and basement, and is a mid-Georgian townhouse. The Heritage Statement submitted with the application describes it as having high heritage significance, but that the setting makes a negligible, positive contribution to the building's overall significance in view of the later, large-scale development around it. The proposed transport hub is considered by the Heritage Statement to have an overall slight negative impact on the setting and significance of this listed building, with the removal of the existing car park seen as having a positive impact.

Officers consider that by intruding above the roofline of the listed building, the introduction of an eight storey structure as backdrop would pose harm to the setting of this asset. Negative impact would also result from development on the setting of grade II Listed assets on Kings Square when viewed from the east side and from Dove Lane; here the height of development will likely result in a significant intrusion over the roofscape of the west side of the square. This is poorly demonstrated in the Visual Impact Assessment and inadequately addressed in the Heritage Impact Assessment.

The impact on the Kingsdown Conservation Area has not been adequately assessed particularly in terms of the impact on views from Marlborough Hill Place and Marlborough Hill where the proposed development is likely to have a significant and harmful impact. A key part of the character of the Kingsdown Conservation Area is how it is influenced by local topography and geology, giving rise to skyline views of the city. Similarly, the views from, and overall character of the Stokes Croft conservation area would also be harmed. These views out of the conservation area would be interrupted by the scale of the proposed development which would result in less than substantial harm to its setting which would not be outweighed by any proportionate benefit. It is not considered that the proposal would harm the setting of the St James Parade conservation area, due to the presence of tall buildings between the application site and this conservation area, as well as the Marlborough Street thoroughfare, both of which provide a degree of separation.

The Eugene Street flats are the earliest examples Bristol Corporation's planned slum clearance development in the city, and were built between 1925 and 1933. The submitted Heritage Statement accompanying this scheme notes the blocks as "of negligible heritage significance and as such, are not considered to be of sufficient evidential, historical, aesthetic or communal value to be considered non-designated heritage assets." Since the application was submitted, the Eugene Street flats have been included on the Local List and fall to be regarded as non-designated Heritage Assets. The flats have a strong character and lend an attractive air to the locality. They represent a major landmark in the housing of the poor in the city centre and are of strong aesthetic and historic interest.

The NPPF requires the LPA to apply a balanced judgement having regard to the scale of any harm or loss and the significance of the heritage asset in such incidences, and in this case, in view of the historic value that the properties have within the townscape, it is considered that substantial harm would be brought about by their loss, with no proportionate public benefit offered by the scheme. Consequently the proposed demolition of these flats would have a negative impact on the character of the area, in addition to the harm that would be inflicted on the setting of the listed building and conservation area, and is contrary to the NPPF, and Local Plan policies BCS22; DM26 and DM31.

(ii) Loss of trees

The proposal would result in a loss of green space and trees on site. The proposed green roof would to some extent help offset some of ecological impact of the proposal, but the amenity value

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given by existing trees on the site would be lost. This aspect is discussed in more detail at Key Issue (H).

(iii) Townscape Views

The submitted Views Analysis show that the proposed building would appear unduly prominent in key townscape views, and overplay what would essentially be a car park, as a prominent building.

The scale of the proposed building set within this topography is likely to cast significant shadows in the area. The bulk of this impact will be on areas of public realm, making them cold, damp and unattractive. As an example, the feature public space is primarily north facing and is likely to be shade for most of the day. This will have a negative impact on the quality of the spaces and public realm. No shadow analysis has been submitted to verify this.

(iv) Pedestrian Movement

A transport hub would anchor vehicles, cyclists and pedestrian movement, but the movement would be transitory in nature and without meaningful dwell time. Therefore despite anchoring movement, the proposal is unlikely to facilitate quality streets and spaces for people to enjoy. A clear understanding of impact of the proposed development on pedestrian/cycle movement, focusing on the quality of experience, has not been provided. A genuinely design lead approach to design of the public realm is needed to improve and not further disadvantage users of pedestrian, cycle and public transport experience of the urban environment.

(v) Public Art

The proposal does not incorporate any provision for public art and no commitment has been made about its incorporation at reserved matters stage.

Although the design of the building and the public realm is a reserved matter, the proposal does not provide a convincing response to demonstrate that it would effectively mitigate the negative effects of introducing a car park onto the site. The harm to the heritage assets that would be brought about, would not be outweighed by any public benefit.

(H) LOSS OF TREES

Policy BCS9 states that individual green assets should be retained wherever possible, and that development should incorporate new or enhanced green infrastructure of an appropriate type, standard and size.

Policy DM17 seeks to protect Important Open Spaces, Unidentified Open Spaces, Urban Landscape and Trees and recognises the role these features have in providing landscape and visual amenity quality.

Policy BCAP25 expects development to incorporate as many street trees or other trees as possible.

The application is accompanied by a Tree Survey which assesses the condition of trees on and surrounding the site. There are 13 trees within the site within category B (moderate quality with 20+ years remaining contribution), and 5 within category C (Low quality with minimum 10+ years contribution).

Whilst it is clear that at least 10-12 Category B trees would have to be removed in order to deliver the car park, at this stage, the application does not commit to removing or retaining specific trees elsewhere on the site, some being indicatively shown on the site layout plan. The Bristol Tree

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Replacement Strategy has not been applied, and the application does not offer a commensurate replacement tree plan or financial contribution.

Because of the unmitigated loss of trees, the lack of proposed tree replacement shall form a reason for refusal, however, in the event of an approval, (or an appeal) the applicant would be asked to apply the BTRS calculation to provide replacement trees on the site, or to agree to a monetary contribution to mitigate for the loss of trees.

CONCLUSION

The application would bring about benefits to users of the BRI Hospital which are described in the application, and it is recognised that there is much support for the scheme. Unfortunately the basis on which the application is made is unfounded. The application is not supported by robust supporting documents which justify the proposal in transport terms, and there is no mechanism offered by which to secure the removal of nearby car parks.

The applicant's own Air Quality Assessment reports an increase in pollutants, which would go against the Council's commitment to improving air quality in the City Centre, with no mitigation proposed.

The demolition of the Eugene Street flats would result in a loss of viable family sized accommodation in a sustainable location and the proposal as a whole would pose an unwelcome addition in the street scene, leading to harm to amenity of nearby residents, an unmitigated loss of trees as well as harm to heritage assets.

The application is recommended for refusal.

COMMUNITY INFRASTRUCTURE LEVY

This is an outline application. The CIL regulations require that CIL liabilities are calculated when reserved matters applications are submitted as until the reserved matters stage it is not necessarily clear as to the exact level of CIL liable floor space.

RECOMMENDED REFUSE

The following reason(s) for refusal are associated with this decision:

Reason(s)

1. The application proposes excessive and unjustified levels of parking within the City Centre, and puts forward no mechanism by which to secure the closure of car parks outside the site. It fails to adequately demonstrate that the impact on the surrounding highway network will not be severe with regard to safe and effective movement within the highway network for all users. Furthermore, the proposals fail to demonstrate safe and adequate access onto local highway network. The proposal would therefore be an unsustainable development, and would give rise to congestion and resultant unacceptable impacts on highway safety, and a loss of amenity, without commensurate mitigation. It is therefore contrary to NPPF, BCS10, BCS11, BCS13, BCS21, DM27, BCAP29, BCAP30, BCAP34 and BCAP43.

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2. The proposal would worsen air quality in an area proven to have already high concentrations of pollutants, with no commensurate mitigation. This in turn would prevent the Council from fulfilling its legal obligations of reducing nitrogen dioxide to below the legal maximum in the shortest time possible, and would fail to safeguard the amenity and wellbeing of nearby occupiers and other users of the area. The development is therefore contrary to the NPPF, Core Strategy policy BCS21, BCS23, and SADMP policy DM33.
3. The proposed loss of 36 family sized houses in a sustainable city centre location is unjustified, and with no satisfactory benefit being proposed as mitigation for the loss, is contrary to the NPPF and Core Strategy policy BCS5.
4. The proposal would result in substantial harm through total loss of a series of three Locally Listed buildings as non-designated heritage assets, and harm to the setting of the Grade II Listed Building at 2 Dighton Street and harm to the Kingsdown conservation area. The degree of harm would not be outweighed by any proportionate public benefit. The building would appear unduly prominent in key townscape views, which, at the scale proposed would be unable to be mitigated through detailed design work. The proposal is therefore contrary to the Planning (Listed Buildings and Conservation Areas) Act 1990, the NPPF, policies BCS21, BCS22, DM26, DM27, DM28, DM30 and DM31.
5. The proposal would result in the removal of a number of trees, which offer amenity value to the streetscene, and this loss is not mitigated by a tree replacement plan using the Bristol Tree Replacement Strategy. The application therefore fails to mitigate for the loss of existing green infrastructure, contrary to the NPPF, BCS9 of the Core Strategy 2011, DM17 of Site Allocations and Development Management Policies 2014 and BCAP25 of the Bristol Central Area Plan 2015.
6. The application fails to demonstrate through proper assessment that there would be no loss of Daylight or Sunlight to nearby residential properties (particularly those on Marlborough Hill South), therefore it is likely that a loss of amenity would be inflicted in this regard. The application is therefore contrary to the NPPF, BCS21 of the Core Strategy and DM27 of SADMP.

Advice(s)

1. Refused Applications Deposited Plans/Documents

The plans that were formally considered as part of the above application are as follows:-
 171030-FATKIN-XX-XX-DR-AX-30120 P3 Proposed levels 02 and 03, received 15 October 2018
 171030-FATKIN-XX-XX-DR-AX-30120 P4 Proposed level 0, received 15 October 2018
 171030-FATKIN-XX-XX-DR-AX-30130 P3 Proposed levels 04 and 05, received 15 October 2018
 171030-FATKIN-XX-XX-DR-AX-30140 P3 Proposed levels 06 and 07, received 15 October 2018
 171030-FATKIN-XX-XX-DR-AX-30140 P3 Proposed Roof Level, received 15 October 2018
 171030-FATKIN-XX-XX-DR-AX-30501 P2 Existing and proposed SW elevation, received 15 October 2018
 171030-FATKIN-XX-XX-DR-AX-30502 P2 Existing and proposed SE elevation, received 15 October 2018
 171030-FATKIN-XX-XX-DR-AX-30503 P2 Existing and proposed NW elevation, received 15 October 2018
 171030-FATKIN-XX-XX-DR-AX-30504 P2 Existing and proposed NE Elevation, received 15

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8CC**

October 2018

171030-FATKIN-XX-XX-DR-AX-30601 P2 Existing and proposed section 1, received 15
October 2018

171030-FATKIN-XX-XX-DR-AX-30602 P2 Existing and proposed section 2, received 15
October 2018

171030-FATKIN-XX-XX-DR-AX-30603 P2 Existing and proposed section 3, received 15
October 2018

171030-FATKIN-XX-XX-DR-AX-31701 P2 Proposed typical indicative cladding detail A,
received 15 October 2018

171030-FATKIN-XX-XX-DR-AX-90100 P1 Site location plan, received 15 October 2018

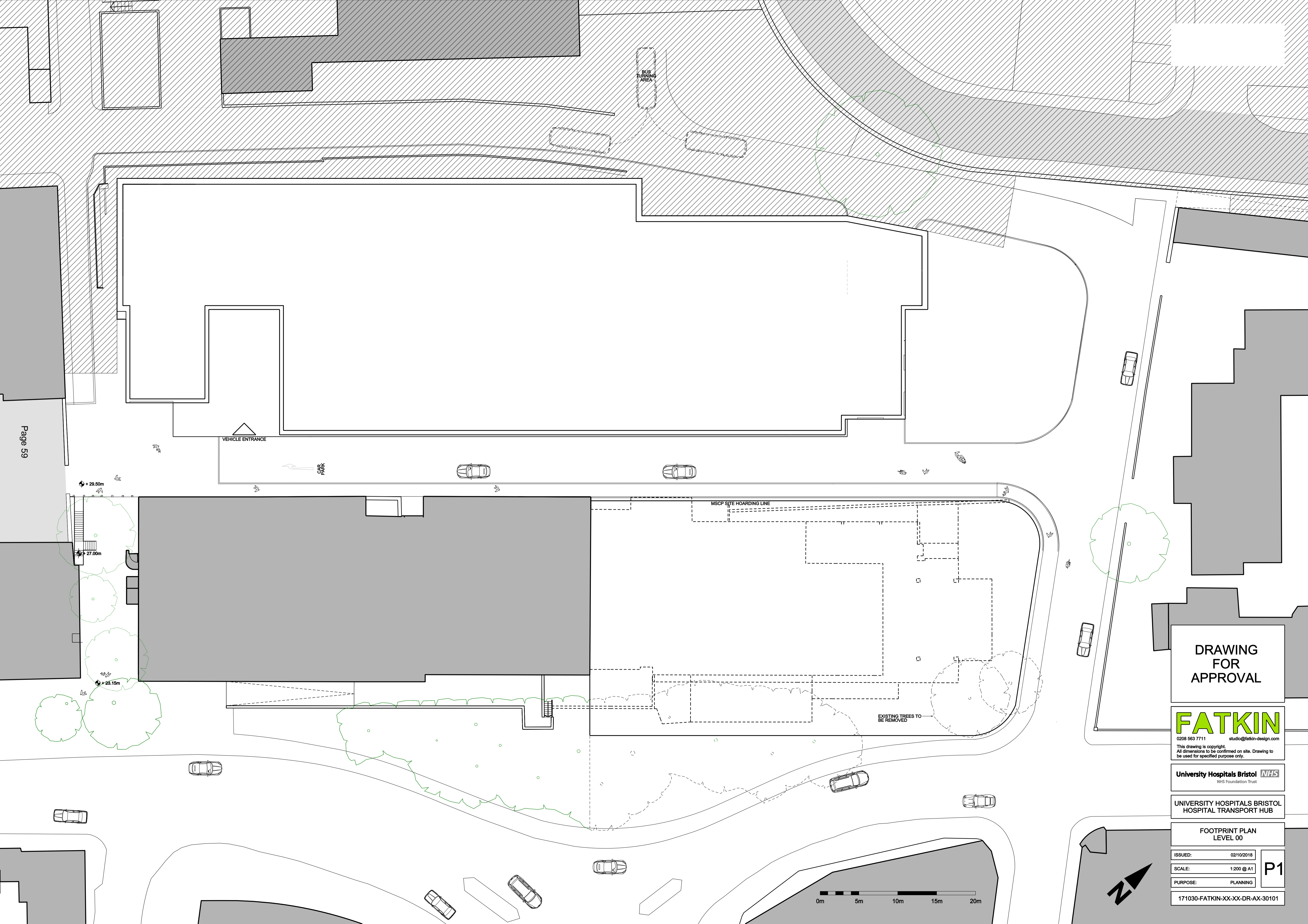
171030-FATKIN-XX-XX-DR-AX-30100 P4 Proposed Level 00, received 15 October 2018

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Supporting Documents

1. Trust Headquarters, Marlborough Street

1. Footprint plan level 00
2. Illustrative general arrangement plan level 00
3. Illustrative South East elevation
4. Illustrative South West elevation
5. Map showing current UHB on-site car park
6. Map showing patient and visitor car parks
7. Visualisations



DRAWING FOR APPROVAL

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FOOTPRINT PLAN
LEVEL 00

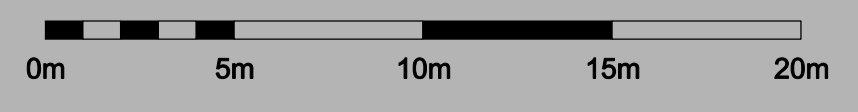
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PURPOSE: PLANNING

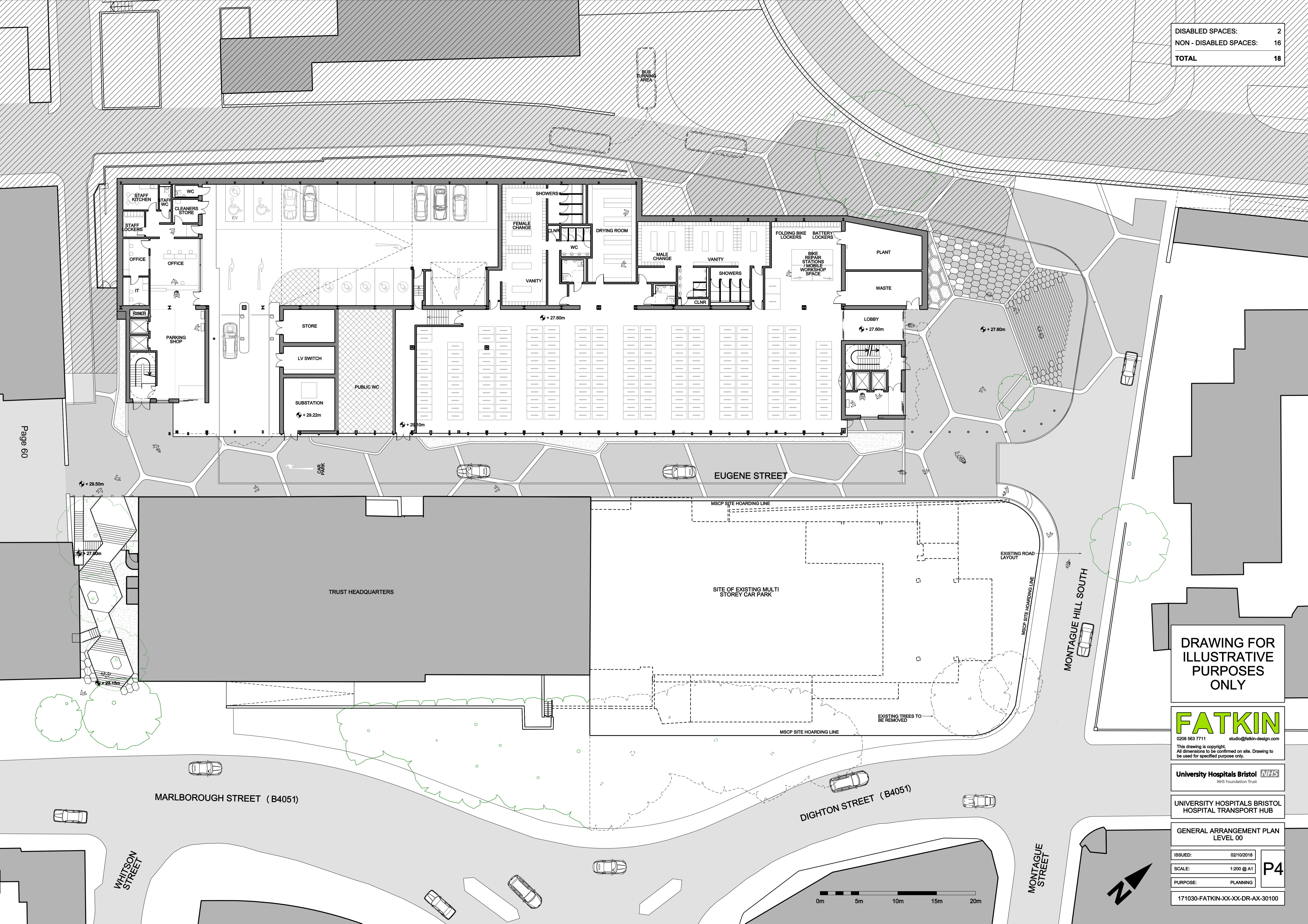
171030-FATKIN-XX-XX-DR-AX-30101

P1



DISABLED SPACES:	2
NON - DISABLED SPACES:	16
TOTAL	18

Page 60



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GENERAL ARRANGEMENT PLAN
LEVEL 00

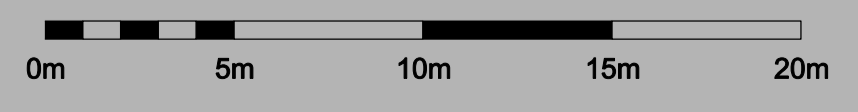
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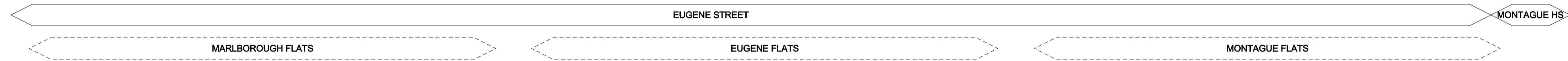
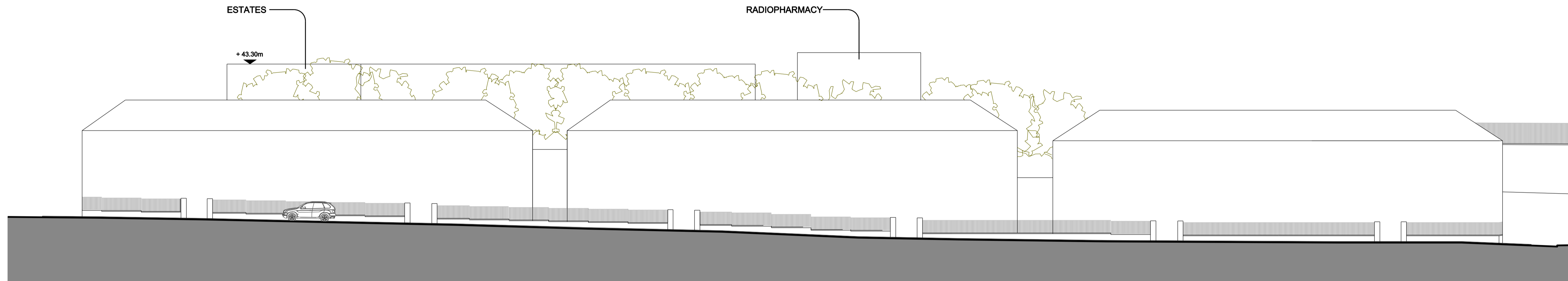
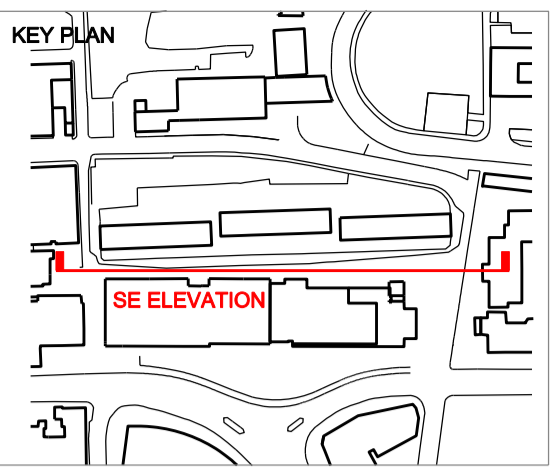
PURPOSE: PLANNING

171030-FATKIN-XX-XX-DR-AX-30100

P4

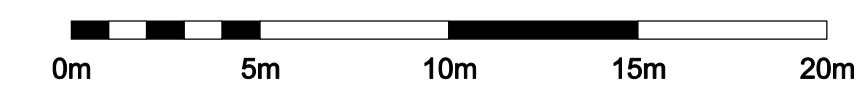
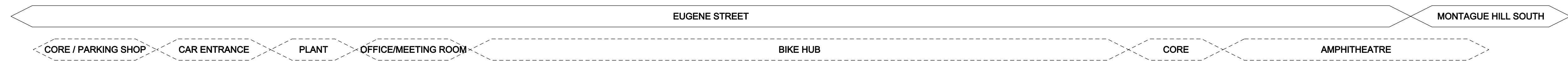
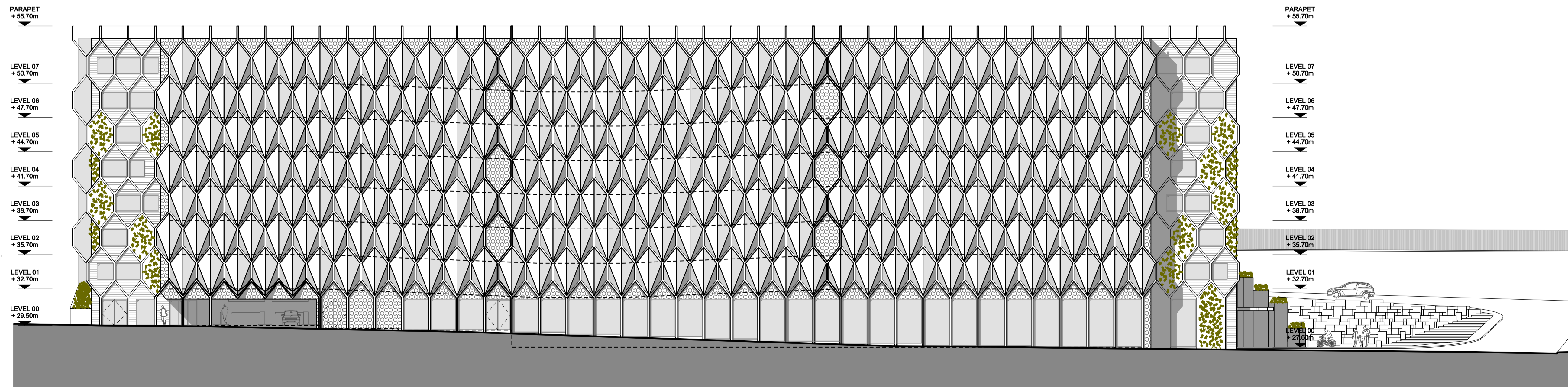


EXISTING



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PROPOSED



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University Hospitals Bristol NHS
 NHS Foundation Trust

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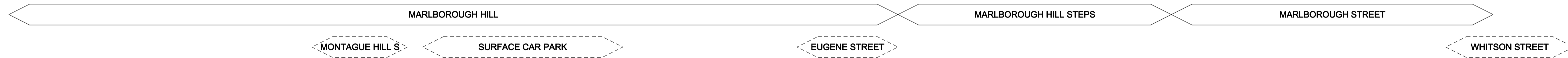
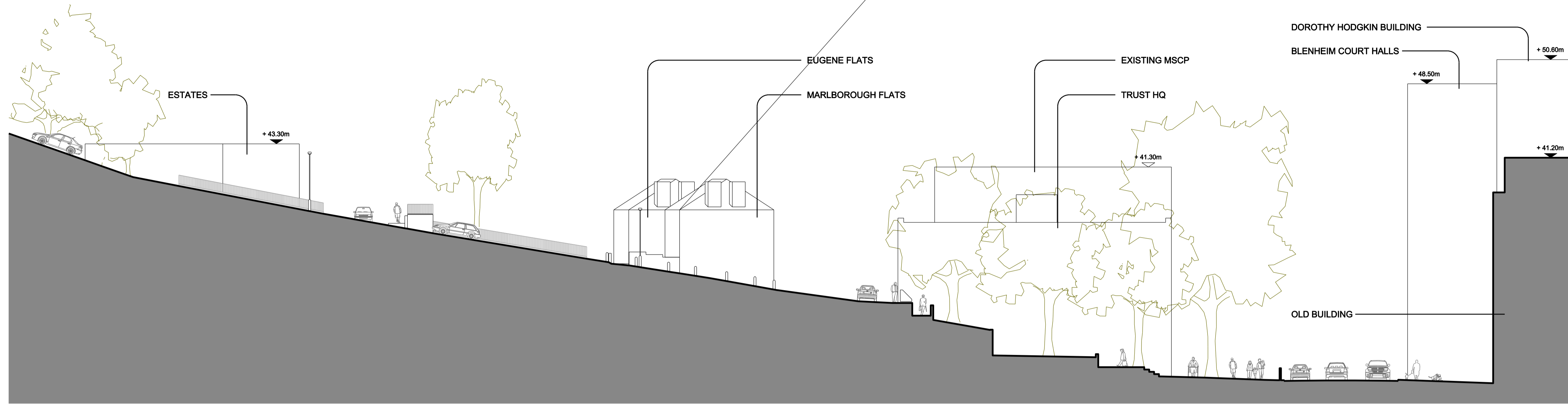
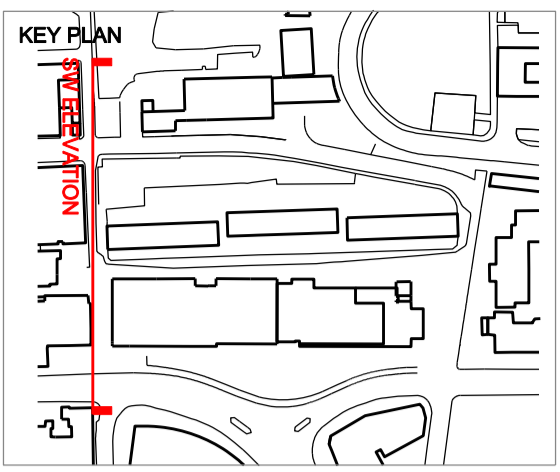
SOUTH EAST GENERAL ARRANGEMENT ELEVATION

ISSUED: 02/10/2018
 SCALE: 1:200 @ A1
 PURPOSE: PLANNING

P2

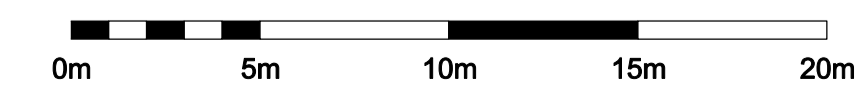
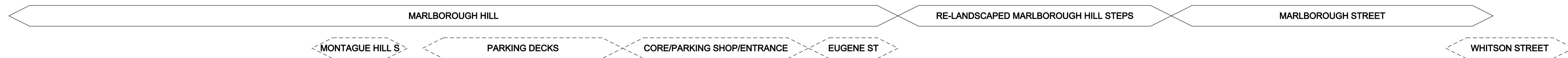
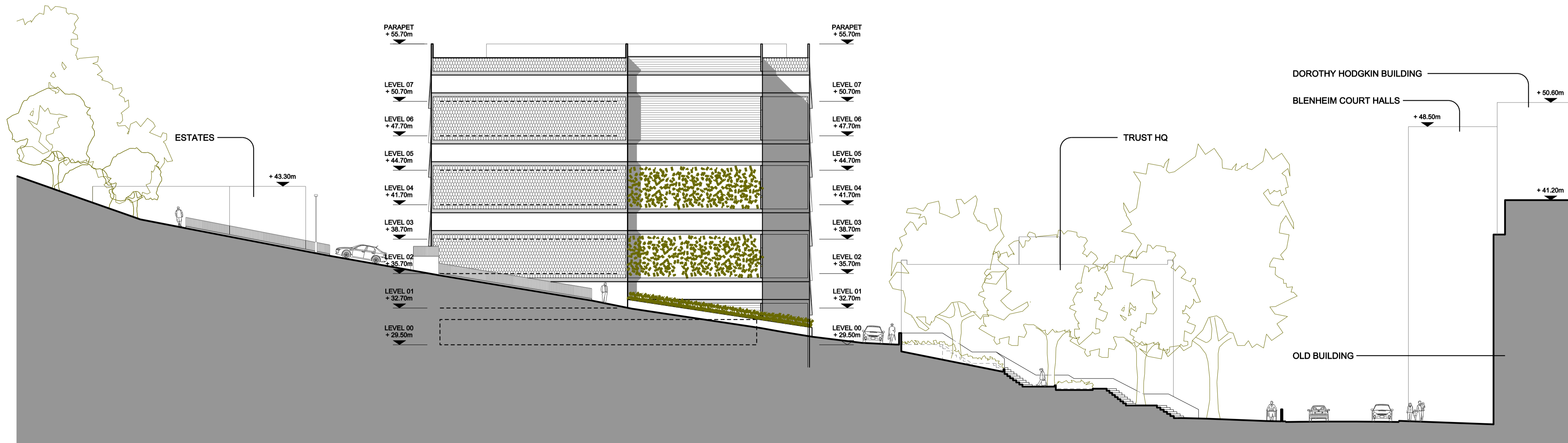
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EXISTING



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PROPOSED



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UNIVERSITY HOSPITALS BRISTOL HOSPITAL TRANSPORT HUB

SOUTH WEST GENERAL ARRANGEMENT ELEVATION

ISSUED: 02/10/2018

SCALE: 1:200 @ A1

PURPOSE: PLANNING

171030-FATKIN-XX-XX-DR-AX-30501

P2

4.3 Car Parking

Table 4a – Map showing current UHBristol Staff on-site car parks (including disabled bays)

University Hospitals Bristol

NHS Foundation Trust

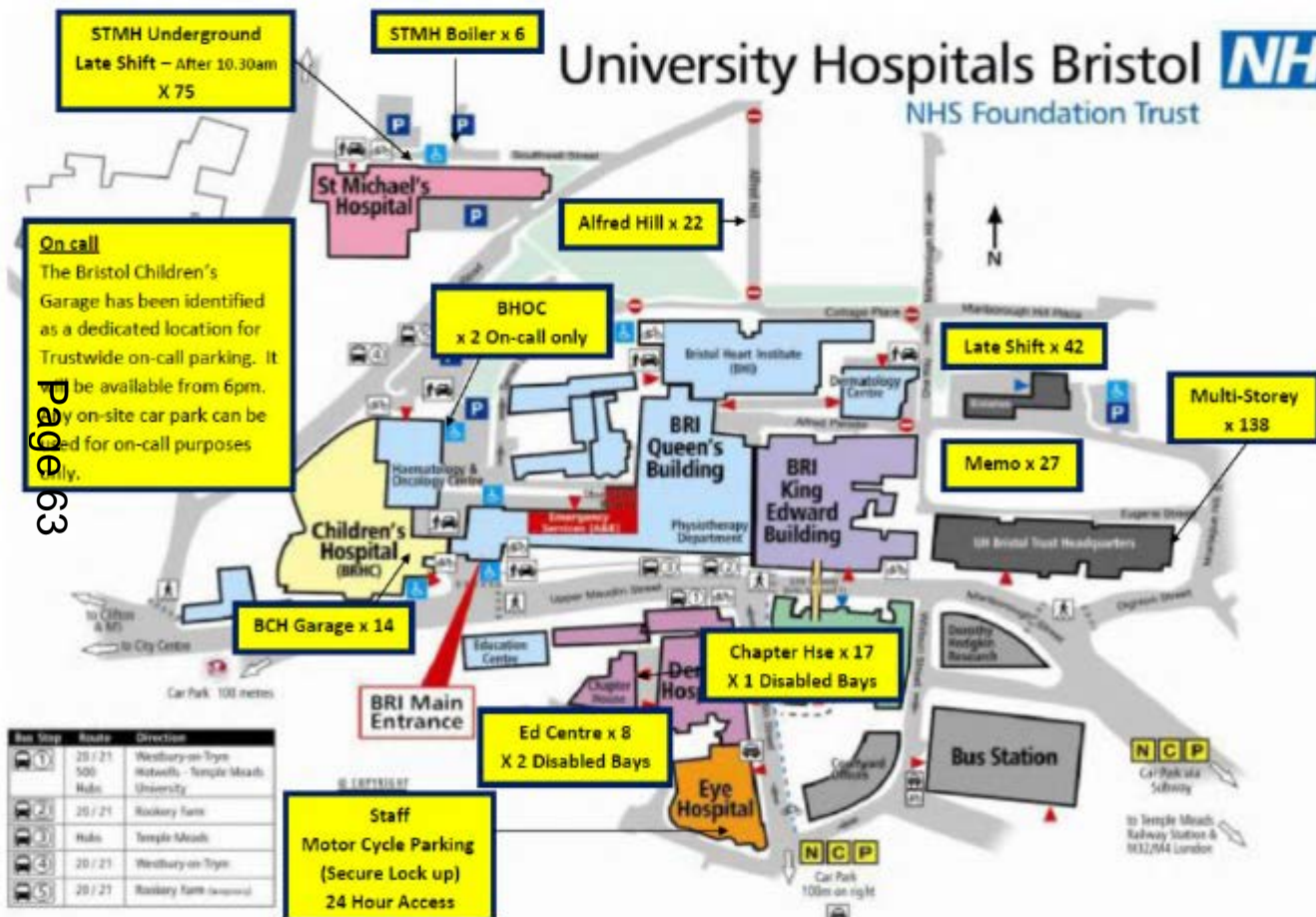
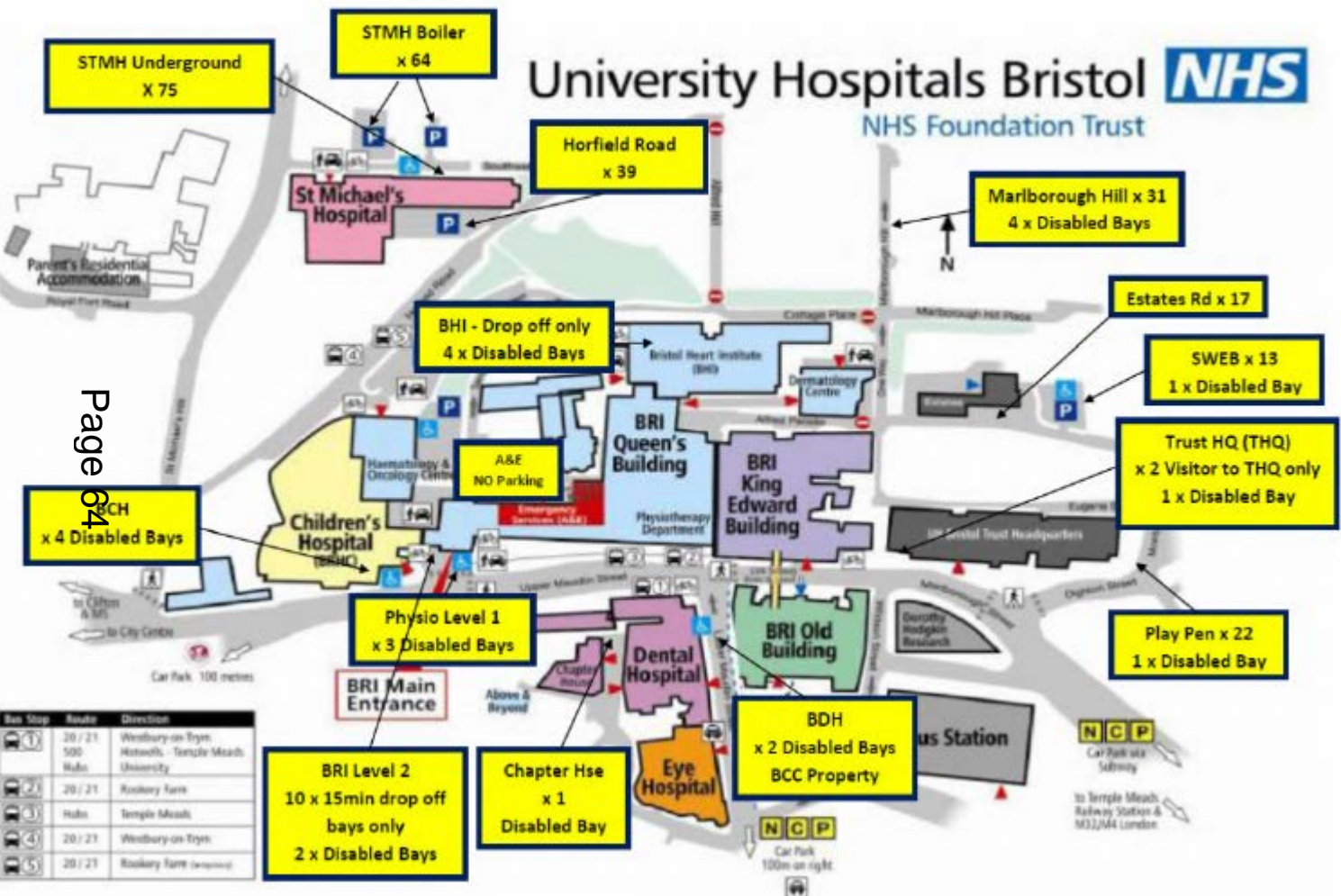


Table 4b – Map showing current UHBristol Patient & Visitors car parks (including disabled bays)

University Hospitals Bristol

NHS Foundation Trust



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4.00

WARD: Knowle CONTACT OFFICER: Alex Hawtin

SITE ADDRESS: Broadwalk Shopping Centre Broad Walk Bristol BS4 2QU

APPLICATION NO: 18/05184/P Outline Planning

DETERMINATION DEADLINE: 1 January 2019

Outline planning application with all matters reserved other than access for the partial demolition, refurbishment and redevelopment of Broadwalk Shopping Centre and adjacent land to provide a mixed use scheme comprising residential apartments (C3), retail floorspace (A1/A2), cafes, bars and restaurants (A3/A4/A5), offices (B1), community (D1) and leisure uses (D2). Provision of vehicular and pedestrian .cycle accesses, parking, servicing, landscaping, public realm and associated works.

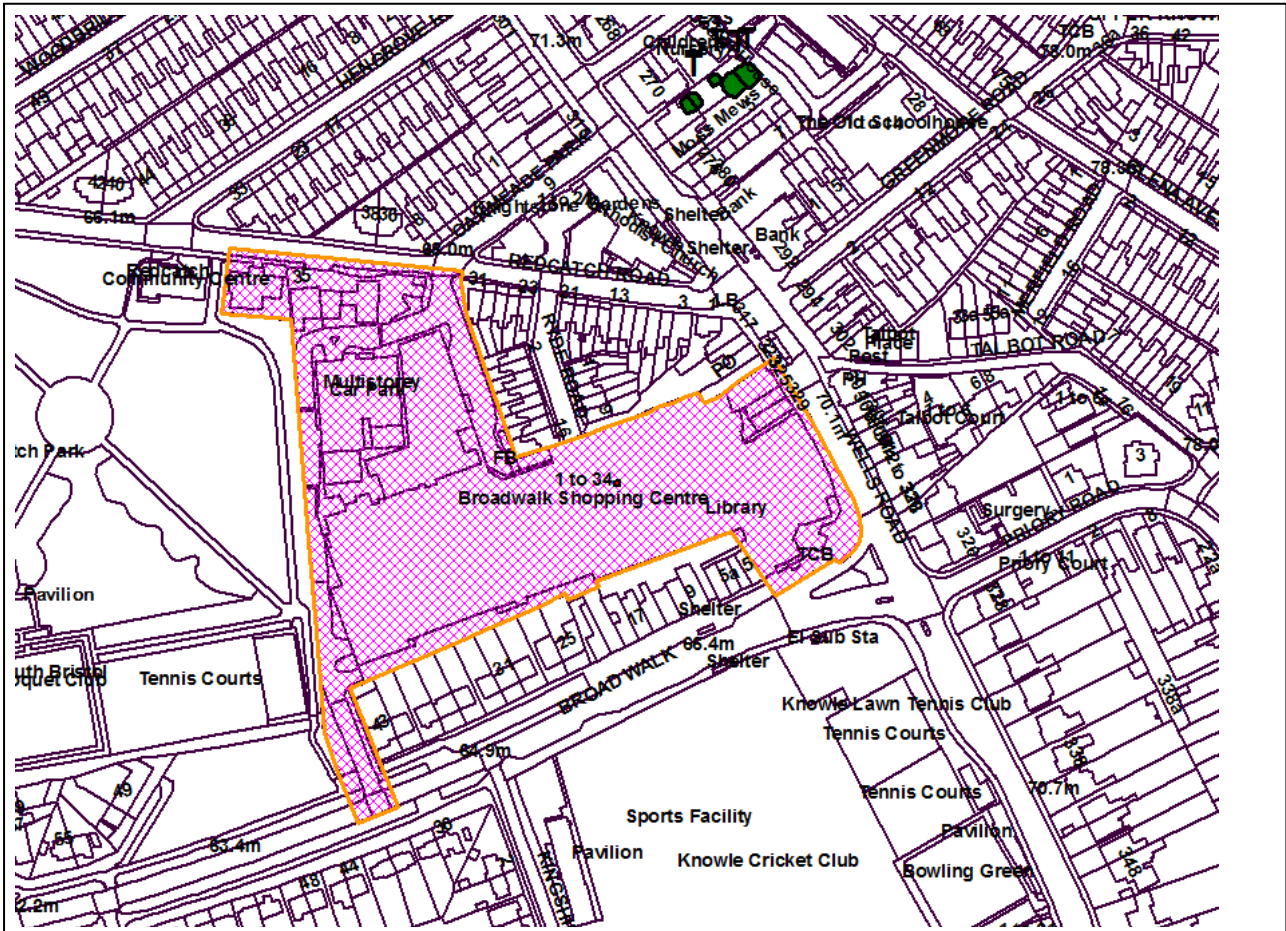
RECOMMENDATION: GRANT subject to Planning Agreement

AGENT: Lichfields
The Quorum
Bond Street
Bristol
BS1 3AE

APPLICANT: Pelican Development Management Ltd

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



Development Control Committee B – 13 March 2019**Application No. 18/05184/P : Broadwalk Shopping Centre Broad Walk Bristol BS4 2QU****1.0 SUMMARY**

1.1 This application seeks outline planning permission for the refurbishment and redevelopment of the Broadwalk Shopping Centre in Knowle, South Bristol. All matters are reserved except access; however consideration has been given to whether the site could accommodate the quantum proposed.

1.2 The application is referred to Development Control Committee B due to the significant level of public interest (over 480 comments received). No referral has been received from Local Ward Members.

1.3 The application proposes the refurbishment of and an extension to the main shopping centre building and the demolition of a number of underused and derelict buildings and the construction of up to 385 homes. An office block on the site would be converted into 35 residential flats.

1.4 The principle of development is accepted and considered to contribute positively to the delivery of new homes on previously developed land. This is supported by the NPPF and Local Plan Policies BCS5, BCS7 and BCS20.

1.5 The proposed loss of retail and leisure buildings is sufficiently justified by the refurbishment and re-provision of facilities elsewhere within the application site, which would be within a designated Local Centre in accordance with Policy BCS7.

1.6 Delegated authority is sought to conclude outstanding transport and highways issues, however there is sufficient information provided to satisfy officers that these issues can be agreed following the Development Control Committee Meeting.

1.7 Sufficient information has been provided and design work has been undertaken for officers to be confident that the site can accommodate the amount of development proposed and that an acceptable design solution can be found.

1.8 A number of the comments that have been raised by Consultees and members of the public would be addressed at reserved matters stage.

1.9 The application for outline planning consent is recommended for approval, subject to conditions and planning agreement.

2.0 SITE DESCRIPTION

2.1 This application relates to the land and buildings located to the east of Redcatch Park and bounded by Redcatch Road to the north, Wells Road to the east and Broad Walk to the south. The site is within the Knowle Ward, south east Bristol.

2.2 The land is currently in a mix of uses but is predominantly in use as the Broadwalk Shopping Centre; a retail centre with over 30 shops, leisure and entertainment uses and food outlets. Offices and a public library are located the south-eastern extent of the site, at the corner of Broad Walk with Wells Road. A multi-storey car park is located in the western part of the site and car parking is also provided at roof level above the Shopping Centre. At the northern extent of the site there is a former petrol station on Redcatch Road, which is currently in use as a car wash. The former Knowle library, a single-storey building located on Redcatch Road, is also included within the application site.

2.3 Vehicular access to the site is gained via Redcatch Road, with egress onto Broad Walk. The main pedestrian entrance to the Shopping Centre is at the corner of Broad Walk with Wells Road.

Development Control Committee B – 13 March 2019**Application No. 18/05184/P : Broadwalk Shopping Centre Broad Walk Bristol BS4 2QU**

2.4 The site is designated within the Knowle Local Centre and as a Primary Shopping Area in the Bristol Local Plan. Redcatch Park is designated as Important Open Space.

2.5 The surrounding area is largely residential in nature. No's. 1 to 16 Ryde Road are located immediately to the north of the site and no's. 25 to 43 Broad Walk located immediately to the south. These buildings are a combination of Victorian terraces and larger, post-war houses. There are also a number of retail units with residential flats above, located along Redcatch Road, Broad Walk and Wells Road. Redcatch Community Centre is located to the west of the former library building.

2.6 Knowle Methodist Church is a Grade II Listed Building, located to the north of the site on the corner of Wells Road and Redcatch Road, circa 50 metres from the site. The Old Schoolhouse, 28 Maxse Road is a Grade II Listed Building, located circa 100 metres from the site.

2.7 The location is well-connected to the City Centre and South Bristol, with bus stops located on Broad Walk, Wells Road and Redcatch Road.

3.0 RELEVANT HISTORY

3.1 The site has a long history, with numerous applications for various alterations and advertisements. The most recent and relevant history is included below:

18/04648/SCR - Request for a Screening Opinion as to whether an Environmental Impact Assessment (EIA) is required for the proposed redevelopment of the Broadwalk Shopping Centre, Knowle. (Please note that this is not a planning application and therefore we are not carrying out public consultation on the proposal at this stage). – EIA NOT REQUIRED (Oct 2018)

07/05426/F - Part demolition of existing multi-storey car park and erection of part two, part three-storey residential development (45 units) and provision of access cores, refuse, cycle parking and car parking areas. – GRANTED subject to condition(s) (Feb 2009)

06/05015/F and 10/01314/R Demolition of existing buildings. Erection of three-storey development to provide 38 flats, associated underground parking area and 124 square metres of B1 class office space

04/05196/F - Change of use of part of first floor from nightclub (Use Class D2) to library (Use Class D1). – GRANTED subject to condition(s) (Mar 2005)

03/03955/F - Erection of a two-storey extension for retail use adjacent to existing multi-storey car park. New entrance and improved elevations to Wells Road. Refurbished Malls and new rooflight. – GRANTED subject to condition(s) (Feb 2004)

03/00755/F - Change of use from Use Class A1 Retail to Use Class A3 Food and Drink. – GRANTED subject to condition(s) (Apr 2003)

01/01464/F - Demolition of two levels of car parking and erection of new retail unit at mall level with related storage and service provision at road level. – GRANTED subject to condition(s) (Dec 2001)

4.0 APPLICATION

4.1 This application seeks outline planning permission for the partial demolition, partial refurbishment and redevelopment of the Broadwalk Shopping Centre, multi-storey car park, Redcatch Road Hand Car Wash and the former Knowle Library Building. All matters are reserved except for access.

Development Control Committee B – 13 March 2019**Application No. 18/05184/P : Broadwalk Shopping Centre Broad Walk Bristol BS4 2QU**

4.2 The following buildings are proposed for demolition:

- Former Knowle Library
- Redcatch Road Hand Car Wash
- Multi-storey car park
- Retail Units 17A, 17B, 46 and 47 (B&M, Vacant, Wilko and Bingo Hall /Fit4Less Gym)

4.3 The redevelopment of the site would include the following mix of uses:

- Up to 420 residential apartments (use class C3)
- Up to 11,500sqm of new or refurbished floorspace, including retail, leisure, office and community floorspace (use classes A1, A2, A3, A4, A5, D1 and D2) including a nursery
- Up to 420 car parking spaces (circa 160 residential and 260 shopping)
- Public realm enhancements

4.3 The proposed residential uses would be located above the main entrance to the shopping centre at Wells Road and in the western part of the site, fronting Redcatch Park. 35 apartments are proposed to front Wells Road and would be formed through the conversion of existing upper floor office space. A further 385 units would front Redcatch Park, in blocks ranging between 4 and 12-storeys.

4.4 The existing multi-storey car park is proposed for demolition, with the re-provision of up to 420 car parking spaces within a lower ground floor beneath the proposed redeveloped shopping centre.

4.5 Vehicular access to the site would remain as existing, with vehicular access from Redcatch Road and egress onto Broad Walk. Pedestrian access would be retained at the corner of Wells Road and Broad Walk, with a further access from Redcatch Park.

5.0 EQUALITIES ASSESSMENT

5.1 The public sector equalities duty is a material planning consideration as the duty is engaged through the public body decision making process.

“S149 of the Equalities Act 2010 provides that a public authority must in the exercise of its functions have due regard to:

- (a) eliminate discrimination, harassment, victimisation and any other conduct prohibited under the Act
- (b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it
- (c) foster good relationships between persons who share a relevant characteristic and those who do not share it.

5.2 During the determination of these applications due regard has been given to the impact of the scheme upon people who share the protected characteristics of age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation. In their assessment of these applications your officers are satisfied that any adverse impacts can be addressed and mitigated through the detailed design of the buildings and the imposition of appropriate conditions.

6.0 PRE-APPLICATION COMMUNITY CONSULTATION

6.1 The Applicant has carried out pre-application community consultation, as detailed in the Statement of Community Involvement submitted with the planning application. This sets out the details of consultation activities including, but not limited to:

Development Control Committee B – 13 March 2019**Application No. 18/05184/P : Broadwalk Shopping Centre Broad Walk Bristol BS4 2QU**

- Key stakeholder and Ward Councillor meetings,
- A public exhibition event
- Media / Social Media and a website
- Presentation at Bristol Urban Design Forum

6.2 The statement sets out the feedback from public consultation was largely positive however, key concerns raised included loss of privacy, parking and disruption during construction.

6.3 The statement sets out how responses were considered and how, if appropriate, they were considered as part of the proposals.

7.0 RESPONSE TO PUBLICITY AND CONSULTATION**General response from the public**

7.1 As of 28 February 2019, a total of 482 comments have been received from 393 interested parties.

7.2 171 comments were received in support of the proposed development.

7.3 284 comments were received in objection to the proposed development. The following planning issues were raised in objection to the scheme:

- Objection to the scale and massing of the proposed development
- Objection to the density of the proposed development
- Concerns that the proposed development is out of character with the surrounding area
- Concerns about the visual impact of the development and the potential to block views
- Concerns about the impact upon residential amenity from overlooking, overbearing and overshadowing
- Concerns about construction impacts upon residential amenity
- Concerns about the noise impacts of the proposed development
- Concerns about the impacts upon air quality
- Concerns about the impact of the proposed development upon parking in the area.
- Concerns about a lack of public transport in the surrounding area
- Concerns about congestion in the area
- Concerns about the impact of the proposed development upon local resources and facilities
- Concerns about the flood risk and drainage

7.4 A total of 15 of the comments were neutral in nature. The following planning issues were raised in neutral comments:

- Support for the redevelopment of the shopping centre
- Support for the increased connectivity to Redcatch Park
- Concerns about the density of the proposed development
- Concerns about to the scale and massing of the proposed development
- Concerns about parking

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- Request that the link to Redcatch Road / Redcatch Park is made accessible for pedestrians and cyclists
- Concerns about the housing mix and the emphasis on one- and two-bedroom flats
- Concerns about the impact of the proposed development upon local resources and facilities
- Concerns about the potential impact upon trees in Redcatch Park

7.5 A comment was received neither objecting to nor supporting the application by the Local Ward Member, Councillor Gary Hopkins.

Response from internal consultees

7.6 Affordable Housing – No objection

“I confirm that I am now in a position, now that the Viability Assessment have been completed, to accept on behalf of the Council a figure of 13.4% (54 AH units on the current scheme of 402 units) affordable housing be delivered on this scheme, with nil public subsidy.

7.7 It is noted that the affordable housing provision will be compliant with Local Plan BCS 17 and deliver 77% social rent (based on 2015 Rent Scheme) and 23% shared ownership (based on 40% equity sale and 1.5% rental); with service charges not exceeding £650pa per unit. All the AH units are required to meet BCC space standards and meet the Homes West development standards (based on the former HCA Development standards 2007).”

7.8 City Design Group – No objection

7.9 Several rounds of consultation were undertaken with City Design Group (CDG) to assess the proposals in terms of urban design and to define key principles to be embedded within any future reserved matters applications for appearance, scale, layout and form.

7.10 Notwithstanding the scale of the building elements, which is not in the main excessive, the overall visual impact of the group of buildings appears as a large block more characteristic of a single institution rather than a collection of residential buildings.

7.11 An indicative approach has been taken to building design (through the submission of an addendum to the DAS in December 2018) in order to break down the scheme into its constituent elements and to better respond to aspects such as the scale and character of Redcatch Road, the relationship with existing properties and their amenity, and to create a more appropriate visual scale of development and relationship with the edge of Redcatch Park.

7.12 Further evolution of the design approach is reassuring that the broad quantum of development could be accommodated on this site. Notwithstanding this the number of units have been reduced in order to help better articulate the massing of the development toward the centre of the scheme, and to reduce any potential overbearing to the rear of properties facing Ryde Road.

7.13 The design evolution gives reassurance with regard to the acceptability of approximately 400 units on the site and the application includes a number of welcome aspects including the number of cores, the use of dual aspect apartments and an indication of usable communal open space the scheme.

7.14 An assessment has been undertaken by CDG in accordance with Urban Living SPD “Making successful places at higher densities”. This is summarised below.

7.15 The scheme is considered to be successful in its approach to the following issues:

- The proposed is located in a sustainable location close to a local centre with public facilities and sustainable transport routes and utilises the opportunity for intensification.

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- A site-wide masterplan is in place that aims to retain and rejuvenate the shopping centre.
- The proposal would diversify the existing housing stock.
- The proposal has benefited from pre-application engagement. Scheme is seen as a catalyst for change with increased density.
- The blocks positively address the adjoining streets and spaces with active frontage.
- A number of entrances are proposed from the public realm, and the scheme makes improvements to existing entrance onto Broadwalk.
- There would be a number of well-maintained public outdoor spaces, semi-private courtyards and balconies, and informal play space.
- The site is larger in size and offers potential for a taller building. The building is integrated into the proposed development block. The building is located in a less sensitive location.
- The proposed development has been designed considering secured by design principles.
- The proposed development would enhance natural surveillance of Redcatch Park and surrounding streets.

7.16 Reserved matters submissions should seek to address the following:

- The scale, massing and bulk of the development should be informed by an assessment of liveability conditions for future and existing occupiers.
- Variation should be provided throughout the tall building and clearly define its 'top'.
- The new public and communal space appears to be narrow in comparison to the height of surrounding buildings and could limit sunlight penetration.
- The linear blocks oriented in E-W direction create distinct north and south facing frontages, and while some of the apartments are designed with dual aspect, many will still be north-facing receiving little sunlight or south-facing causing overheating issues.
- The proposed vehicular and pedestrian access from Redcatch Road should be enhanced to improve the environment from its current use as a service access.
- The proposal relies on high number of individual units accessed from long corridors.
- Noise and vibration from the car park should be minimised through careful design.
- Dwellings should as a minimum meet the Nationally Described Space Standard (NDSS) for one-bedroom, two person units to allow for future flexibility.

7.17 Transport Development Management – Further information required.

7.18 The Transport Development Management (TDM) comment is summarised below:

7.19 The principle of development is accepted by TDM however the applicant will need to take account of the comments relating to the traffic modelling, sustainable transport, parking, Travel Plan, access and internal arrangements.

7.20 TDM has assessed the traffic impact of the proposal and requires the 2018 with development analysis for the Broad Walk/ Wells Road junction. To accompany this, a signalised crossing across the left turn from Broad Walk into the main pedestrian access of the centre is required. It is noted this may affect junction performance.

7.21 In order to rationalise the low parking ratio provided on site the implementation of sustainable travel options is an important element required of this proposal. The applicant must decide on which option they would prefer in regards to the implementation of the travel plans. Furthermore in order to deter overspill vehicular parking from residents a parking scheme must be implemented that covers a significant area. A contribution is therefore required.

7.22 To further enhance the sustainable options for residents and promote cycling, a contribution is sought towards the implementation of a designated cycle route along Broad Walk. This will include footway widening, signage and lining. This will be vital in encouraging modal shift and offering a viable option to residents who may not feel confident on road.

Development Control Committee B – 13 March 2019**Application No. 18/05184/P : Broadwalk Shopping Centre Broad Walk Bristol BS4 2QU**

7.23 In regards to public transport, the applicant has established that the site is served by a number of bus services. The applicant should note contributions will be required for the upgrading of the Hengrove Road bus stops which are situated on Redcatch Road. Further contributions towards the installation of a 12-line RTI display in a prominent position within the development. As outlined above the possibility of a bus gate must be explored.

7.24 In terms of the off-site highway works these have been identified to ensure adequate safe pedestrian facilities are provided around/into the site.

7.25 In reference to servicing, it is unclear whether the development would decrease servicing trips given almost 25% of the site is unoccupied and the units will be split into more marketable sizes. Notwithstanding this the use of the freight consolidation centre should be explored in order to reduce the number of servicing vehicles on the highway network. Concerns are also raised regarding deliveries for residents of the units and where servicing vehicles will wait. In addition to this confirmation must be sought from Bristol Waste to clarify if they will service the central area given the Northern access is unadopted.

7.26 Therefore taking into account the submitted information the principle of development is acceptable however further information is required.

7.27 Contaminated Land Environmental Protection – No objection

7.28 Given the scale of demolition required, a site wide intrusive investigation should occur following demolition. This could be coincided with the below ground infrastructure removal at the former petrol station which has to be subject to a validation process. The petrol station must be decommissioned as soon as possible as the tanks are currently water filled (which is meant to be a temporary measure but these tanks have been in this way for a number of years now).

7.29 A demolition method statement for the former petrol station site (outlining how the tanks and associated infrastructure will be removed and validated) should be supplied prior to commencement of the development.

7.30 Sustainable Cities – No objection

7.31 The Sustainable Cities Team have made the following comments:

“BCS13 – climate adaptation

Layout, scale, and massing principles – I understand these are to be agreed within the outline consent. BCS13 requires that development adapts to climate change through site layouts which provide resilience to climate change. Climate change impacts include increased temperatures. It is not clear from the current submission how the site layout has been designed to provide resilience to this. There is little information provided about the layout of residential unit that the proposed forms would contain, however given the block depths the presumption might be for single aspect units. This is a concern as single aspect units are at a higher risk of overheating.

The proposals for green infrastructure across the site are supported and will also contribute to policies BCS9, BCS15 – we'd need to see specifications and maintenance/ management plan submitted at RM stage, particularly for the green roof and walls.

BCS14 - Energy calculations methodology

The predicted energy consumption and CO2 emissions have been based on CIBSE TM46 benchmarks. The SAP and NCM methodologies will need to be utilised at RM application stage to demonstrate compliance with the policies. This may lead to significantly different figures.

Development Control Committee B – 13 March 2019**Application No. 18/05184/P : Broadwalk Shopping Centre Broad Walk Bristol BS4 2QU**

BCS14 - Energy efficiency measures

The proposal to exceed building regulations Part L requirements using energy efficiency measures is supported.

BCS14 - Heat Hierarchy

The proposal to utilise a centralised system and CHP for this development is supported in line with the heat hierarchy, subject to air quality considerations. The proposal to be future-proofed for connection to a heat network is also supported.

BCS14 - Renewable energy

The applicant has misunderstood the 20% renewable energy requirement in the energy statement.

It says: *“In order to achieve the 20% reduction beyond the Part L compliant “baseline” renewable energy will need to contribute around 7% of the energy consumption of the development once fabric-improvements and energy efficiency improvements have been carried out”.*

The policy requires a 20% reduction in CO2 emissions beyond residual emissions not the Part L ‘baseline’.

Note - residual emissions are the emissions after energy efficiency measures (which include CHP) have been included to lower emissions beyond the Part L compliant baseline.

The energy statement suggests that 900m² roof area will be required to reduce CO2 emissions by 7% from renewable energy.

Significantly more roof area will be required to meet the policy requirement of 20% CO2 reduction.

The applicant should identify on the roof plan that there is sufficient additional roof space available for this. A total 2,571m² roof area would be required (assuming the same angle and orientation as the PV proposed currently) based on the current calculations.

This could be reduced by improving the fabric further to lower residual emissions, or giving further consideration to communal heat pump technology (air or ground) which would contribute to the 20% requirement. The figure will also change once calculations utilising the correct methodology have been undertaken.

BCS15 – BREEAM

Given the scale of this development, a BREEAM communities assessment will be required at reserved matters stage.

A BREEAM new construction (2018) assessment will also be required, and an excellent rating will need to be demonstrated. This assessment needs to have been started early in the design process and should have informed the site layout. No pre-assessment has been submitted and I am therefore concerned that the site has been set out without due consideration of the requirements which may make achieving an ‘excellent’ rating more difficult. The energy strategy may also need to be adjusted in order to achieve an ‘excellent’ rating.

Development Control Committee B – 13 March 2019**Application No. 18/05184/P : Broadwalk Shopping Centre Broad Walk Bristol BS4 2QU**

Conditions

A full energy statement, sustainability statement, BREEAM communities assessment, and BREEAM assessment should be secured at RM stage for approval.”

7.32 Flood Risk Management – No objection

7.33 The Flood Risk Management Team have made the following comments:

“The outline surface water drainage strategy has been formed in accordance with pre-application discussions with the Lead Local Flood Authority, and demonstrates that the site can be drained in accordance with the relevant technical standards and the parameters agreed through the pre-application meeting. We therefore have no objections or requests for further information at this stage. A detailed surface water drainage design will be required through reserved matters which must be formed in accordance with the drainage strategy presented in this outline application.”

8.0 RELEVANT POLICIES

8.1 National Planning Policy Framework – February 2019

8.2 Bristol Local Plan comprising Core Strategy (Adopted June 2011), Site Allocations and Development Management Policies (Adopted July 2014) and (as appropriate) the Bristol Central Area Plan (Adopted March 2015) and (as appropriate) the Old Market Quarter Neighbourhood Development Plan 2016 and Lawrence Weston Neighbourhood Development Plan 2017.

9.0 KEY ISSUES**A. IS THE PROPOSED DEVELOPMENT ACCEPTABLE IN PRINCIPLE?**

9.1 Paragraph 118 of the National Planning Policy Framework (NPPF) sets out that substantial weight should be given “to the value of using suitable brownfield land within settlements for homes and other identified needs” and that planning decisions should promote and support the development of under-utilised land and buildings, especially if this would help to meet identified needs for housing.

9.2 Policy BCS1 sets out that the Core Strategy identifies South Bristol as a priority focus for development and comprehensive regeneration. This includes:

- Around 60,000m² of net additional office floorspace focused on centres and the major regeneration areas;
- Up to 10 hectares of new industrial and warehousing land focused on the major regeneration areas;
- The provision of around 8,000 new homes of a mix of type, size and tenure.”

9.3 Policy BCS1 is clear that development in South Bristol will primarily occur on previously developed land.

9.4 Policy BCS5 sets out that the Core Strategy aims to deliver new homes within Bristol's existing built up areas. Between 2006 and 2026, 30,600 new homes will be provided in Bristol.

9.5 Policy BCS7 states that retail development and higher density forms of residential development should be provided within local centres. Mixed-use development will be supported where it takes advantage of underused land and uses that contribute to maintaining the vitality, viability and diversity of centres will be encouraged. Active ground floor uses will be maintained and enhanced throughout the centres.

Development Control Committee B – 13 March 2019**Application No. 18/05184/P : Broadwalk Shopping Centre Broad Walk Bristol BS4 2QU**

9.6 Policy BCS18 of the Core Strategy states that all new residential development should maintain, provide or contribute to a mix of housing tenures, types and sizes to help support the creation of mixed, balanced and inclusive communities.

9.7 Policy BCS20 states that development should maximise opportunities to re-use previously developed land.

Retail and Leisure Uses

9.8 The application proposes up to 11,500sqm of retail, leisure, and community floorspace. The layout and form of these is to be determined at detailed planning stage, however indicative locations and quantum has been provided.

9.9 The proposal would see the retention of 32 of the 37 existing retail units on the site, and a further 2,100sqm of newly created retail space.

9.10 The existing leisure uses onsite; the bingo hall, snooker hall and gym are proposed for demolition. 2,400sqm of leisure space would be included as part of the proposed development, largely located at roof level on top of the refurbished shopping centre building.

The existing library and dentist onsite would be retained.

9.11 The application site is within the Broadwalk Local Centre designated within the Local Plan. As such, the proposed retention and re-provision of retail uses is considered acceptable in principle. As set out within Policy BCS7, the provision of leisure and community uses would help to support the role and function of the Local Centre and are considered acceptable in principle.

9.12 The loss of some retail and leisure uses is considered acceptable and sufficiently justified in an attempt to take advantage of underused land on the site and to diversify the range of uses onsite. This is supported by paragraph 121 of the NPPF which sets out: local planning authorities should “take a positive approach to applications for alternative uses of land which is currently developed... they should support proposals to reuse retail and employment land for homes in areas of high housing demand.”

9.13 It is considered that the proposed refurbishment and redevelopment of retail and leisure uses on site is acceptable in principle.

Residential Use

9.14 The application seeks outline planning consent for up to 420 residential units including 385 new residential flats proposed as part of redevelopment of the western half of the site. This area is currently occupied by the former petrol station, former library building, multi-storey car park, bingo hall and other retail uses. A further 35 units are proposed within the office building fronting onto Wells Road.

9.15 The proposed residential element of the development would contribute positively to the targets set for the delivery of new homes in South Bristol as set out within Policies BCS1 and BCS5.

9.16 The proposed development would accord with Policy BCS7 by promoting mixed-use development within an existing accessible centre by taking advantage of underused land.

9.17 The principle of residential development on the site of the multi-storey car park has been established with the issue of planning consent as part of application 07/05426/F for 45 units. This added two and three-storeys of residential use to the existing area of car parking.

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9.18 The principle of redevelopment of the library and petrol station fronting Redcatch Road has been established through the approval of application 06/05015/F for 38 apartments.

9.19 The Census housing statistics for Knowle Ward indicate that the area is dominated by 3-bedroom houses (65%). The proposal for up to 420 flats would diversify the housing type within the area, as flats currently represent circa 15% of housing stock.

9.20 Whilst the exact accommodation schedule and housing mix is a reserved matter, the principle mix of 1, 2 and 3 bedroom flats is considered to help to diversify the housing mix within the area, providing smaller units for first-time buyers and those wishing to downsize, whilst contributing to the number of family-sized dwellings (three-bedrooms and above), in accordance with BCS1 and BCS5.

9.21 The proposed re-use of brownfield land would be supported by the NPPF and Policy BCS20. The proposed conversion of the existing vacant office building which fronts Wells Road for residential use would utilise underused buildings in accordance with para. 118 of the NPPF.

Conclusion

9.22 The proposed refurbishment of existing retail space and creation of additional leisure uses are considered to support the Local Centre and accord with Policy BCS7.

9.23 The principle of redevelopment of the site for residential uses is considered to be in accordance with the NPPF and Local Plan policies by helping to meet housing needs and utilising underused land.

B. IS THE PROPOSED DEVELOPMENT VIABLE, AND DOES IT PROVIDE AN APPROPRIATE LEVEL OF AFFORDABLE HOUSING?

9.24 The proposed scheme is a mixed use development including a significant level of residential accommodation that falls within Use Class C3 of the Use Classes Order, meaning that it is required to address the Council's Affordable Housing Policies. Whilst the application is outline, with only access being determined at this stage (meaning that everything else is indicative) it is envisaged that 403 dwellings will be included. Therefore the scheme is required to comply with Core Strategy Policy BCS17, which seeks the provision of up to 30% affordable housing (121 affordable dwellings in this case) from development schemes in South Bristol, subject to scheme viability.

9.25 The National Planning Policy Framework (NPPF) and the associated Planning Practice Guidance (PPG) were revised in July 2018, and these revisions are pertinent to the viability assessment of the Broad Walk redevelopment scheme.

9.26 In simple terms, a development is considered to be viable if the Residual Land Value (RLV) of the development is greater than the Benchmark Land Value (BLV).

9.27 The RLV is calculated by ascertaining the value of the completed development, and subtracting from this all the costs involved in bringing the development forward (e.g. build costs, professional fees, legal costs, financing costs etc.) and the developers profit. All inputs are based on present day costs and values.

9.28 The revised PPG includes the following statements about BLV:

“To define land value for any viability assessment, a benchmark land value should be established on the basis of the existing use value (EUV) of the land, plus a premium for the landowner.”

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9.29 Both officers and the Applicant acknowledge that because elements of the scheme such as scale and massing, materials, and the exact size of the scheme in square metres (for the commercial element) and number of dwellings (for the residential element) do not form part of the application, it is very difficult to accurately assess the viability of the scheme.

9.30 However, as the level of affordable housing to be provided needs to be addressed at this stage, the Applicant initially commissioned a small London based company (Edge Ahead Limited) to submit a viability appraisal on their behalf. This concluded that 20% affordable housing could be provided. It soon became clear to both officers and the Applicant that a number of the inputs contained in this appraisal were highly optimistic (for example the assumption that housing associations would pay 85% of open market value for affordable properties). Consequently, the applicant submitted a second viability appraisal, which was undertaken by the Bristol office of Knight Frank. This concluded that no affordable housing could be provided.

9.31 Officers commissioned DVS (the property arm of the Valuation Office Agency) to assess the viability information and advise the Council as to whether the conclusion of the Knight Frank appraisal was reasonable. DVS have assessed the values and costs associated with the development, and have reported their conclusions to officers accordingly.

9.32 DVS agreed with many of the inputs into the Knight Frank appraisal, however there were a number of areas of disagreement, which are set out in the table below.

Input	Knight Frank	DVS
Benchmark Land Value	£7,250,000	£6,900,000
Commercial Yield	10%	9% (a lower yield increases values)
Profit	20% on value for Open Market Residential 6% on value for Affordable Housing 15% on value for Commercial Uses	15% on value for Open Market Residential 6% on value for Affordable Housing 15% on value for Commercial Uses

9.33 Based on the DVS inputs, they conclude that the scheme could provide 27 affordable dwellings (7%).

9.34 However, whilst officers acknowledged that the original appraisal submitted by Edge Ahead Limited can broadly be disregarded, it had concluded that the scheme could be delivered based on a profit level of 13% on value for the Open Market Residential and the Commercial Uses. Given that the Applicant had submitted this as part of the application, officers consider that they must have signed off the appraisal on the basis that they were prepared to develop the scheme for a 13% profit on value.

9.35 Whilst DVS consider this to be a low profit margin, they have run sensitivity testing on a 13% profit scenario. This concludes that, with 13% profit, the scheme could provide 54 affordable dwellings (13% affordable housing).

9.36 As set out above, this is an outline application with many elements yet to be finalised. Consequently, any attempts at assessing the viability of the scheme are little more than an educated guess and the eventual viability of the scheme may well turn out to be significantly different.

9.37 Following productive discussions with the Applicant and the Affordable Housing Team, the Applicant has offered the following affordable housing provision:

- 54 affordable dwellings, of which 77% would be for social rent and 23% for shared ownership, meaning that a policy compliant tenure is achieved. This equates to an initial provision of 13% affordable housing

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- Upward only viability reviews to be undertaken prior to the commencement of each development phase, to assess whether additional affordable housing can be provided

9.38 Based on the DVS advice and their appraisal run with 13% profit on value, and the productive and constructive discussions between the Applicant and the Affordable Housing Team; officers recommend that the scheme be approved with 54 affordable dwellings (13% affordable housing) and upward only viability reviews undertaken prior to the commencement of each development phase. This would be secured via a Section 106 Agreement.

Recommendation in respect of Affordable Housing provision

9.39 Approve subject to the Applicant entering into a Section 106 Agreement to cover the following:

- Provision of 54 affordable dwellings, of which 42 (77%) would be for social rent and 12 (23%) would be for shared ownership

- Upwards only viability reviews to be undertaken prior to the commencement of each development phase

C. IS THE IMPACT OF THE PROPOSED DEVELOPMENT UPON TRANSPORT AND HIGHWAYS ACCEPTABLE?

9.40 Policy BCS10 states that developments should be designed and located to ensure the provision of safe streets. Development should create places and streets where traffic and other activities are integrated and where buildings, spaces and the needs of people shape the area.

9.41 Policy DM23 of the Site Allocations and Development Management Policies outlines that development should not give rise to unacceptable traffic conditions and will be expected to provide safe and adequate access onto the highway network. With regard to parking, Policy DM23 states that this must be safe, secure, accessible and usable.

9.42 The proposed residential development would utilise the existing access for servicing from Redcatch Road to the north of the site and vehicles would exit the site via an undercroft between the shopping centre and the residential development moving south onto Broad Walk.

9.43 The servicing of the shopping centre would remain as existing, with access and egress via the road from Redcatch Road with deliveries to be undertaken within the beneath the retail centre.

9.44 There are four main pedestrian entrances to the site: the existing access from the corner of Wells Road with Broad Walk; access from Broad Walk to the south, access via Redcatch Park and access from Redcatch Road to the north.

9.45 The proposals for pedestrian access to the units to the northwest of the site from Broad Walk are considered acceptable; with a clear pedestrian entrance onto Broad Walk adjacent to the proposed nursery. These dwellings can also be easily accessed by car via the existing route from Redcatch Road.

9.46 Any through route to the residential element of the development to the south in the evening as the shopping centre would also be closed at this time. It is important that this pedestrian route is enhanced at reserved matters stage and that the risk of conflict between pedestrian and vehicle users is minimised. Clarification has been sought from the applicant / BCC Parks team to confirm that the park would be accessibly in evening / at night.

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9.47 There would be a level difference of 6 metres between the shopping centre and the proposed 'plaza'. As a result, the 'through route' from the shopping centre is not easily navigable. It is noted that the level difference is difficult to address given the position of the proposed vehicular undercroft. This access would need to be addressed at reserved matters stage to maximise permeability and ensure that all members of society can use the route.

9.48 Concerns have been raised by City Design Group with regard to the nature of the podium fronting Redcatch Park. At the moment this creates a sheer cliff-like wall and the treatment of this wall should be clarified as part of reserved matters applications. It is noted that there would be private space at the top of the podium; however some form of access and a softer elevational treatment must be secured at reserved matters stage.

9.49 The proposed arrangements would create a multi-storey car park and undercroft access adjacent residential properties. The Noise Impact Assessment submitted with the outline application details that there would be no significant noise impacts on neighbouring occupiers. Details of measures to minimise potential noise impacts from vehicles should be provided at reserved matters stage. The car park and undercroft structures should be designed in such a way to absorb sound and limit the impact upon neighbouring properties and future occupiers.

9.50 A total of 420 car parking spaces are proposed within the multi-storey car park and a total of 660 cycle spaces are proposed within two-tier stacking storage. It should be noted that a proportion of the cycle spaces provided should be Sheffield stands as these are more easily accessed by users. This should be secured at reserved matters stage.

9.51 Transport Development Management (TDM) was consulted as part of the application process. Concerns were raised with regard to the quantum of parking onsite and the likelihood that this could result in overspill parking in the surrounding area. TDM has recommended that a resident's parking scheme is implemented and that a financial contribution would be required to put this in place. This has been agreed in principle, with a s.106 agreement to be put in place and the size of the contribution to be determined. Delegated authority is sought to conclude the exact details of mitigation and the sums required.

9.51 TDM has requested further information regarding vehicular movements and junction modelling, however it has not been possible to prepare and assess this information prior to DC Committee. Delegated authority is sought to review and determine whether this information is acceptable.

9.52 Further contributions are sought to enhance sustainable travel options including: the implementation of a designated cycle route along Broad Walk; the upgrading of the bus stops on Redcatch Road and the installation of a 12-line RTI display in a prominent position within the development.

9.53 Off-site highway works are required to ensure adequate safe pedestrian facilities are provided around/into the site. This would be secured via s.278 agreement.

9.54 The Design and Access Statement sets out that bin stores would be located at each core within a secure and ventilated area. This waste would be transferred to a central refuse area located in the secure loading bay area under the existing shopping centre by the management company. A private waste contract would be maintained in the new development to remove waste from the central refuse area.

9.55 Further details of management of waste and recycling and details of the stores, demonstrating that they are accessible and well-ventilated should be provided at reserved matters stage. A total of 72 bins would be required for the residential units.

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9.56 In conclusion, it is considered that the proposed development is acceptable in transport and highways terms, subject to conditions, financial contributions and highways agreements being in place. Delegated authority is sought to conclude the details of the conditions and contributions.

D. ALTHOUGH SCALE IS RESERVED FOR SUBSEQUENT APPROVAL, WOULD THE PROPOSED DEVELOPMENT SITE BE ABLE TO ACCOMMODATE UP TO 420 DWELLINGS?

9.57 Paragraph 118 of the NPPF states that planning decisions should promote and support the development of under-utilised land and buildings, especially if this would help to meet identified needs for housing where land supply is constrained and available sites could be used more effectively.

9.58 Paragraph 123 of the NPPF sets out that “it is especially important that planning policies and decisions avoid homes being built at low densities, and ensure that developments make optimal use of the potential of each site.”

9.59 Section 12 of the NPPF sets out that “the creation of high quality buildings and places is fundamental to what the planning and development process should achieve.”

9.60 Policy BCS21 of the Core Strategy aims to ensure that all new development in Bristol achieves high standards of urban design.

9.61 Policy DM21 states that development of garden land should not result in harm to the character and appearance of an area.

9.62 Policies DM26-28 of the Site Allocations & Development Management Policies require development to contribute to the character of an area through its layout, form, public realm and building design.

9.63 Policy DM30 states that alterations to existing buildings should be physically and visually subservient to the host building and should respect the scale, form, proportions, materials and details of the host property.

9.64 A parameter plan has been submitted by the applicant demonstrating that up to 420 residential units can be accommodated by the site. This is subject to change in terms of scale, layout and form as part of the reserved matters applications and an iterative design process but sets out a maximum threshold for the site in terms of capacity.

9.65 The parameter plan sets out that the proposed development would range from 67m to 103 metres AOD. This equates to between 4 and 12-storeys, and would include the following elements:

- A single-storey roof extension to the existing shopping centre.
- A four-storey residential block fronting onto Redcatch Road.
- An eight-storey residential block fronting Redcatch Park with a 12-storey and 10-storey block behind.
- A nine-storey residential block fronting Redcatch Park with a five-storey block behind.

9.66 The proposed scheme would equate to a total of 402 apartments, including the 35 created from the conversion of the existing office block.

9.67 It is considered that the application site is within a suitably sustainable location, within a Local Centre that would be appropriate for densification in an urban living approach. In accordance with the Urban Living SPD, an assessment has been undertaken by CDG to identify what works well for the site, and what would need to be addressed as part of future reserved matters applications.

9.68 The large nature of the site lends itself as a natural location for a large building and the larger elements of the scheme would be well-integrated into the proposed development block.

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9.70 Further work is required at reserved matters stage to break down the massing of the proposed building blocks. At present, the indicative elevations appear 'institutional' in nature and increase the visual impact of the proposals. The design and access statement addendum goes some way to addressing these concerns by applying an approach which separates the larger elements of the scheme into three distinct blocks. This should be implemented across the scheme at reserved matters stage, including the Redcatch Park elevation.

9.71 In accordance with the Urban Living SPD, the proposed development would utilise an underused site, with an opportunity for intensification and aims to retain and rejuvenate the shopping centre.

9.72 With regard to the existing context, the blocks positively address the adjoining streets and create active frontage as well as providing natural surveillance of the park.

9.73 A number of new public and semi-private, communal spaces are proposed; however careful planning is required to ensure that sunlight can penetrate these spaces at reserved matters stage. Currently the height of the existing buildings could limit the amount of natural light that reaches the plaza and courtyards.

9.74 The multi-storey car park would be contained within a podium to limit its visual impact. Further work is required at reserved matters stage to address the fact that the principle access for certain residential blocks appears to be via cores from the car park. Pedestrian entrances to and from the site and to the various cores should be enhanced.

9.75 In terms of internal layouts, City Design Group raised several concerns based upon the indicative plans provided within the Design and Access Statement and how this creates comfortable and user-friendly spaces for future residents. Reserved matters applications should address the following:

- Reduce the number of individual units accessed from long corridors
- Resolve the movement strategy and user journey for residents across the site
- Reduce the amount of spaces created by the proposed development that would be in shadow for prolonged periods and may not be attractive
- Reduce the number of single aspect flats (it is noted that some duplex dual aspect flats are proposed, however the south facing flats used to flank car parking remain unresolved)

9.76 A landscape and visual impact assessment should be provided at reserved matters stage to assess the impact of the proposed development upon long-distance views towards the city centre from south Bristol.

9.77 It is concluded that the design evolution gives sufficient reassurance with regard to the acceptability of delivering up to 420 dwellings on the site. This figure is subject to a detailed design process with scale, layout and form to be assessed in full as part of a reserved matters application.

E. WOULD THE PROPOSED DEVELOPMENT CAUSE ANY UNACCEPTABLE HARM TO RESIDENTIAL AMENITY?

9.78 Paragraph 123c of the NPPF sets out that when considering applications for housing, authorities should take a flexible approach in applying policies or guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site (as long as the resulting scheme would provide acceptable living standards).

9.79 Policy BCS15 that development should provide flexibility and adaptability, allowing future modification of use or layout, facilitating future refurbishment and retrofitting'.

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9.80 Policy BCS18 sets out that residential development should provide sufficient space for everyday activities and to enable flexibility and adaptability by meeting appropriate space standards.

9.81 Policy BCS21 outlines that development in Bristol is expected to safeguard the amenity of existing development and create a high-quality environment for future occupiers.

9.82 Policy DM29 sets out that new buildings will be designed to ensure that the existing and proposed development achieves appropriate levels of privacy, outlook and daylight.

9.83 Supplementary Planning Document 2 “A Guide for Designing House Alterations and Extensions” (SPD2) whilst providing guidance for house alterations, is relevant in this case given the existing context. SPD2 states that the best way of ensuring privacy between houses is to avoid windows to habitable rooms directly facing one another. Where this cannot be achieved, and habitable rooms face each other, as a ‘rule of thumb’ a gap of 21 metres should generally be provided. In more densely developed, inner urban locations this distance may be less.

9.84 SPD2 sets out that where windows to habitable rooms face the end wall of a house, the distance should be not less than 12 metres. Where the distance between windows and walls is less than this, there is potential for the proposed development to be overbearing.

9.85 BRE Report 209, “Site Layout Planning for Daylight and Sunlight – A Guide to Good Practice” sets out that when a development is opposite existing neighbouring windows and a line drawn at 25 degrees from the lowest habitable window intersects the proposed development, further information is required to determine impact upon residential amenity. Where a development sits below this line, there would be limited impact upon residential amenity through overshadowing and no overbearing.

9.86 The BRE Report 209 states that new development should not reduce the Vertical Sky Component (VSC) to existing residential windows to less than 0.8 times their former value.

9.87 The BRE guide recommends that main living room windows should receive at least 25% of the total Annual Probable Sunlight Hours (APSH). It also recommends that at least 5% of the APSH should be received during the period between 21st September and 21st March.

Amenity for existing residents

9.88 The proposed five-storey buildings situated to the rear of houses on Broadwalk would be situated beneath a 25 degree line drawn from neighbouring ground floor windows. Concerns are raised about the adjoining nine-storey building which fronts the park. This would intersect a 25 degree line measured from ground floor windows at 39-43 Broad Walk.

9.89 The proposed four-storey buildings situated to the west of Ryde Road would sit below a 25 degree line drawn from neighbouring ground floor windows of 2-16 Ryde Road. In addition, the proposed building would be of a similar height or lower than the existing building currently occupied by Wilko. There would also be the added benefit that part of this would be a ‘green wall’ rather than the current blockwork wall.

9.90 The proposed four-storey buildings fronting onto Redcatch Road would marginally intersect (less than 2 metres above) a 25 degree line drawn from neighbouring ground floor windows of 36 & 38 Redcatch Road and the rear of 33 Hengrove Road.

9.91 To properly assess the potential impact upon these neighbouring properties, a Daylight and Sunlight study (Right of Light Consulting, 2018) was submitted as part of the application.

9.92 The daylight and sunlight study assessed the potential impact of the proposed development upon 13 residential properties in the surrounding area:

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- 2 to 16 Ryde Road (even only)
- 23 to 31 Broadwalk (odd only)

9.93 Of the 72 windows assessed, a total of 20 windows were determined to have failed the BRE guidance in terms of impact upon VSC. Of these 20 windows, none were reduced to a ratio of less than 0.7.

9.94 In terms of sunlight to windows (APSH), a total of 7 windows fail the BRE Guidance by receiving less than 25% sunlight hours. Of these windows, 4 already received less than 25% sunlight hours and are not assessed as being unacceptably affected by the proposed development. The impact upon a fourth window is considered acceptable as this is a bathroom/ W/C.

- 16 Ryde Road: APSH before: 32%. APSH after: 23% (Fail).
- 16 Ryde Road: APSH before: 30%. APSH after: 19% (Acceptable as Bathroom window)
- 14 Ryde Road: APSH before: 11%. APSH after: 8% (Acceptable)
- 10 Ryde Road: APSH before: 13%. APSH after: 6% (Acceptable)
- 8 Ryde Road: APSH before: 14%. APSH after: 6% (Acceptable)
- 2 Ryde Road: APSH before: 13%. APSH after 8% (Acceptable)
- 4 Ryde Road: APSH before: 30%. APSH after: 24% (Fail)

9.95 Whilst the Daylight and Sunlight study identifies impacts upon 20 windows in terms of VSC and two windows in terms of ASPH, the proposed impact of the development upon adjoining is considered acceptable as there would be no significant reduction in daylight to these properties. The proposal would be in accordance with the NPPF, which states that “authorities should take a flexible approach in applying policies or guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site.”

9.96 By providing a gap in excess of 21 metres, the proposed development would be located sufficiently far from dwellings on Broad Walk so as to avoid overlooking. A gap of 24 metres would be provided between the proposed four-storey residential block at no's. 2-16 Ryde Road. The elevation has been arranged in such a way as to minimise overlooking by providing oblique windows (see C.2 Indicative Elevations within the Design and Access Statement). This would be secured as a condition of any reserved matters application.

9.97 Overall, it is considered that the proposed development would be situated sufficiently far from neighbouring occupiers to minimise overlooking.

9.98 There would be no unacceptable harm to existing residential amenity through overshadowing, overbearing or overlooking.

F. DOES THE PROPOSED DEVELOPMENT GIVE SUFFICIENT CONSIDERATION OF SUSTAINABLE DESIGN AND CONSTRUCTION?

9.99 Policy BCS13 sets out that development should contribute to both mitigating and adapting to climate change, and to meeting targets to reduce carbon dioxide emissions.

9.100 Policy BCS14 sets out that development in Bristol should include measures to reduce carbon dioxide emissions from energy use by minimising energy requirements, incorporating renewable energy sources and low-energy carbon sources. Development will be expected to provide sufficient renewable energy generation to reduce carbon dioxide emissions from residual energy use in the buildings by at least 20%.

9.101 Policy BCS15 sets out that sustainable design and construction should be integral to new development in Bristol. Consideration of energy efficiency, recycling, flood adaptation, material consumption and biodiversity should be included as part of a sustainability or energy statement.

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9.102 Although for approval at reserved matters stage, information has been provided to indicate how the proposals could meet the LPA's suite of sustainability policies contained within the Core Strategy. This information has been reviewed by Sustainable Cities and a request for the following to be submitted at reserved matters stage has been issued:

- A full energy and sustainability statement
- A BREEAM Communities Assessment
- A BREEAM 2018 pre-assessment
- Specification of the proposed green walls and roofs to be implemented (NOT sedum)
- SAP and NCM methodologies to be utilised to demonstrate 20% reduction in CO2 emissions
- Specification of the proposed solar PV panels and a roof plan showing the location and orientation of these panels
- Details of the proposed methods for heating and hot water, with a preference for CHP, subject to meeting air quality standards

9.103 A condition has been requested to be attached to any outline consent requiring further information of how daylight and sun-lighting has been considered as part of the current proposals and how the risk of overheating and poor quality living environment could be addressed.

10.0 CONCLUSION

10.1 The proposed development affords an opportunity to secure the regeneration of this important site in the South of Bristol, consistent with the first Core Strategy Policy in the adopted Development Plan.

10.2 The proposed development would contribute to the delivery of market and affordable homes on previously developed land in accordance with the NPPF and as per the requirements of Policies BCS1, BCS5 and BCS20.

10.3 The proposed development would provide leisure and retail space within a Local Centre, in accordance with Policy BCS7.

10.4 Delegated authority is sought to agree conditions and planning obligations to ensure that the proposed development would be acceptable in terms of transport and highways.

10.5 On balance, the proposed development is considered acceptable in planning terms and as such, this application is recommended for approval.

11.0 COMMUNITY INFRASTRUCTURE LEVY (CIL)

11.1 This is an outline application. The Community Infrastructure Levy Regulations 2010 require that CIL liabilities are calculated when reserved matters applications are submitted as until the reserved matters stage it is not necessarily clear as to the exact level of CIL liable floor space.

12.0 PLANNING AGREEMENT

12.1 The following financial contributions are sought to ensure that the proposal is acceptable in planning terms:

- £11,000 towards the upgrading of Hengrove Road (Westbound) and Hengrove Road (Eastbound) bus stops. These works would include the installation of a 3m raised kerb with the appropriate lining.
- A financial contribution of £10,000 towards the installation of a 12-line RTI display in a prominent position within the development.

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- £140,000 towards the widening of the footway along Broad Walk to incorporate a cycle lane including the appropriate signing and lining. This will provide a link to the Filwood Quietway and offer an attractive off road cycle route towards the Centre and the West of the City.
- £TBC towards the implementation of an area wide RPS
- £5395 would be due in respect of each TRO required and the applicant would be responsible for the implementation of the measures necessary to give effect to the TROs on site. Statutory notices for road humps or pedestrian crossings are subject to the same fee.
- Travel Plan
- Affordable Housing
- Connection to Redcatch Park

13.0 PROPOSED PLANNING CONDITIONS

13.1 Delegated authority is sought for the conclusion of conditions, however it is anticipated that there would be conditions covering the following items. It should be noted that this is not an exhaustive list and there may be further conditions added following the Development Control Committee Meeting.

- Time limits for commencement of development
- General arrangement plans for the highway works (to be agreed)
- Construction management plan
- Site investigation
- Method statement for petrol station decommissioning
- Remediation scheme
- Implementation of oblique-angled windows to limit overlooking (locations to be determined at reserved matters)
- Implementation of a landscaping scheme
- Provision of green walls to the rear of Ryde Road and fronting onto Redcatch Park
- Submission of a full energy statement and sustainability statement as part of a reserved matters application
- BREEAM communities assessment and a BREEAM 2018 pre-assessment as part of a reserved matters application
- Submission of a travel plan as part of a reserved matters application
- Submission of a Refuse and Recycling Servicing and Management Plan as part of a reserved matters application
- Compliance with parameter plans
- List of approved plans and drawings

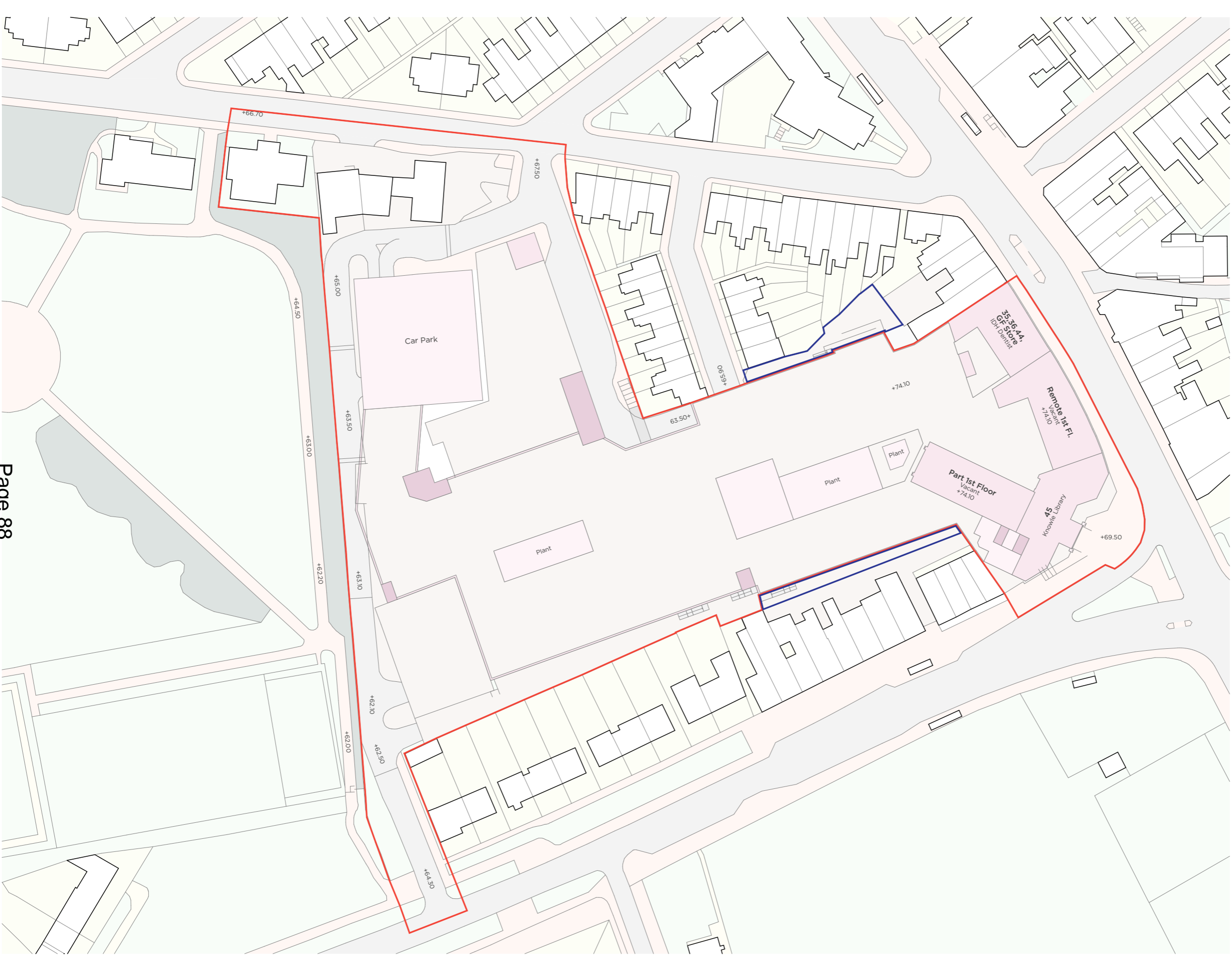
RECOMMENDED GRANT subject to Planning Agreement (as set out in Section 12)

Delegated authority is sought to prepare a legal agreement consistent with the details set out in Section 12 and a decision notice that will include the conditions set out in Section 13.

Supporting Documents

2. Broadwalk Shopping Centre, Broad Walk

1. Existing plan level 1
2. Demolition plan
3. Parameter plan
4. Extracts from Townscape visual impact appraisal



NOTES

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All omissions and discrepancies to be reported to Keep immediately.



Key

- Application Boundary
- Other land in FREP Knowle Ltd - In Administration's Ownership
- Broadwalk Shopping Centre

First issue	KA	SC	28/09 2018	-
Notes	DR	CH	Date	Rev



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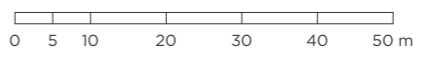
Project
 Broadwalk Bristol

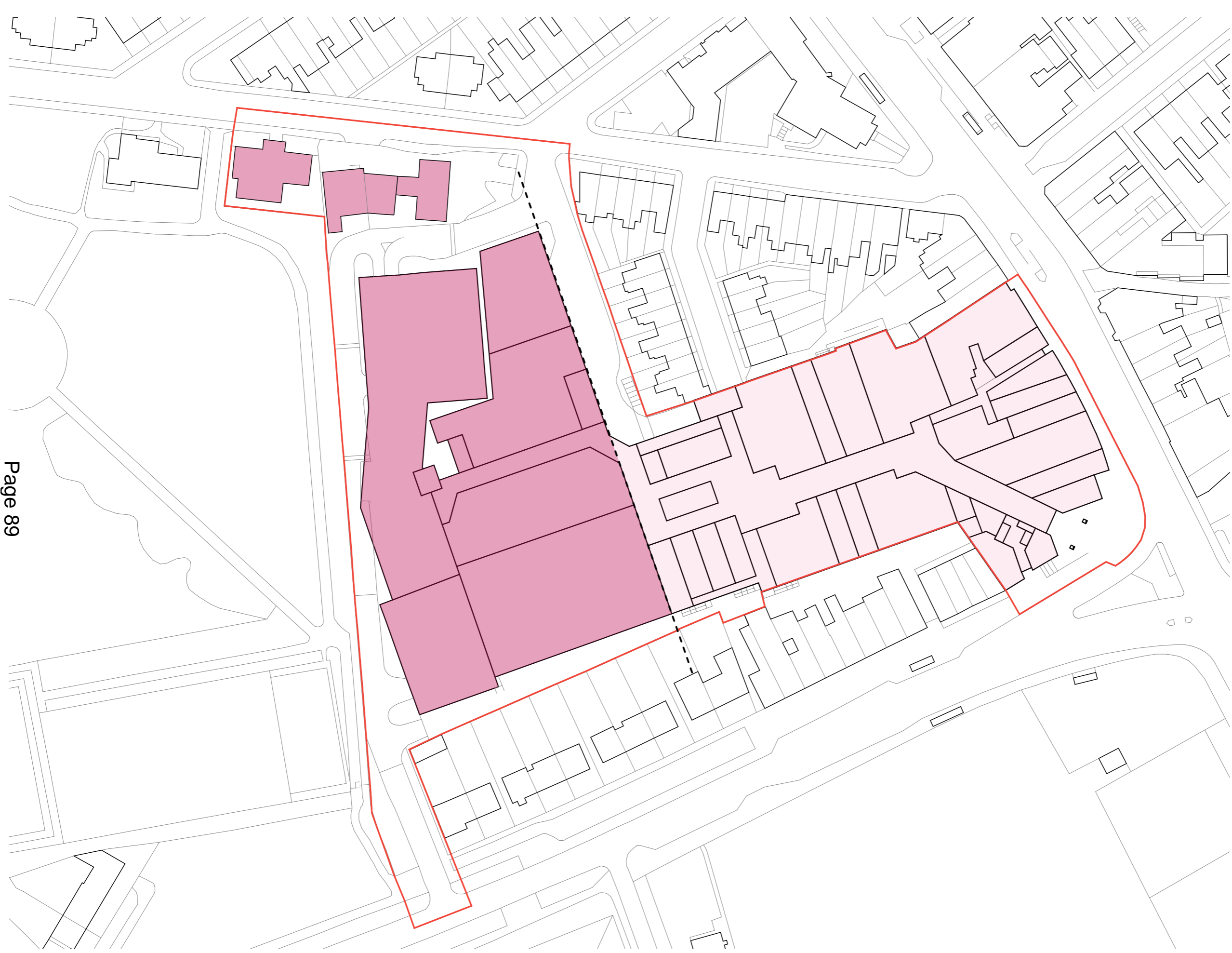
Drawing
 Existing Plans - 1 Floor

Drawing number
 1370.P.004

Scale
 1:1000 @ A3

Revision number
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NOTES
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- Key**
- Application Boundary
 - Retained Building
 - Demolished Building
 - - - Demolition Line

First issue	KA	SC	28/09	-
Notes	DR	CH	Date	Rev

Keep
 Architecture

Keep Architecture Limited t: 0117 325 0359
 2.6 Temple Studios e: info@keeparchitecture.co.uk
 Bristol, BS1 6QA w: keeparchitecture.co.uk

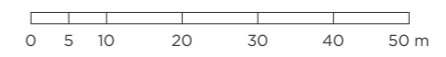
Project
 Broadwalk Bristol

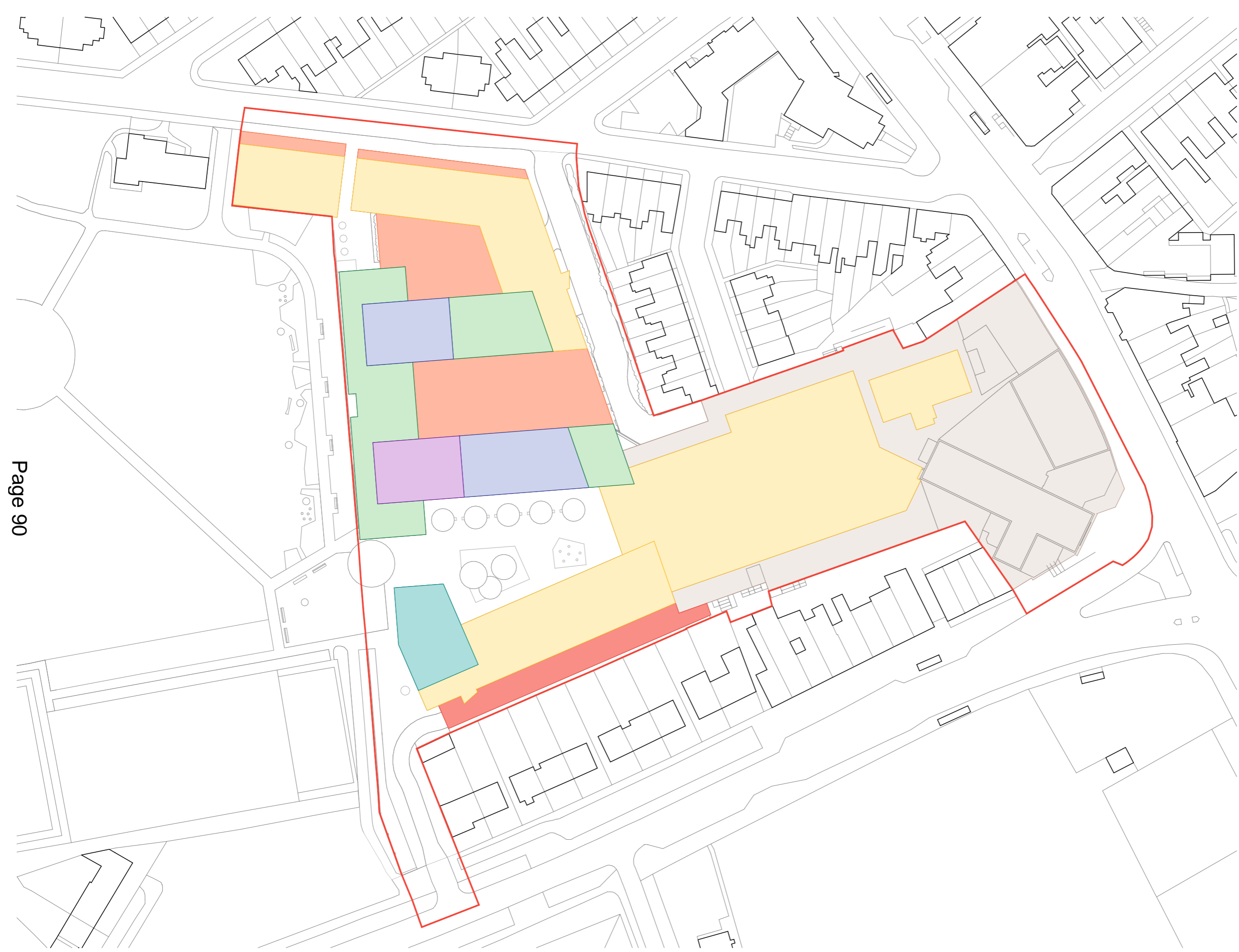
Drawing
 Demolition Plan

Drawing number
 1370.P.011

Scale
 1:1000 @ A3

Revision number
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- Key**
- Planning Application Boundary
- Heights Parameter Plan**
- Maximum 103.00m AOD
 - Maximum 97.00m AOD
 - Maximum 94.00m AOD
 - Maximum 91.00m AOD
 - Maximum 82.00m AOD
 - Maximum 77.00m AOD
 - Maximum 67.00m AOD
 - As Existing

Amendments	KA	SC	21/12 2018	A
First Issue	KA	SC	28/09 2018	-
Notes	DR	CH	Date	Rev

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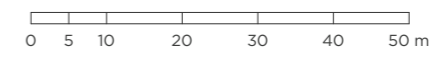
Project
 Broadwalk Bristol

Drawing
 Parameter Plan 2 - Heights

Drawing number
 1370.P.013

Scale
 1:1000 @ A3

Revision number
 A








View 02a - Proposed - AVR1

View from Wells Road, looking north-west



Legend






-  Existing shopping centre (to be converted)
Visible areas only.
-  Proposed leisure, and other uses, storeys.
(On top of existing shopping centre)
-  Proposed leisure, and other uses, storeys.
Where occluded by surrounding context.
-  Proposed residential.
-  Proposed residential.
Where occluded by surrounding context.

View 05a - Proposed - AVR1

View from Broad Walk, looking north-east








Legend

-  Existing shopping centre (to be converted)
Visible areas only.
-  Proposed leisure, and other uses, storeys.
(On top of existing shopping centre)
-  Proposed leisure, and other uses, storeys.
Where occluded by surrounding context.
-  Proposed residential.
-  Proposed residential.
Where occluded by surrounding context.

View 06a (Winter) - Proposed - AVR1
View from Redcatch Park, looking south-east



Legend

-  Existing shopping centre (to be converted)
Visible areas only.
-  Proposed leisure, and other uses, storeys.
(On top of existing shopping centre)
-  Proposed leisure, and other uses, storeys.
Where occluded by surrounding context.
-  Proposed residential.
-  Proposed residential.
Where occluded by surrounding context.

WARD: Eastville **CONTACT OFFICER:** Alex Hawtin
SITE ADDRESS: Alexandra Park Public Car Park Alexandra Park Fishponds Bristol BS16 2BG

APPLICATION NO: 18/06358/F Full Planning

DETERMINATION DEADLINE: 13 March 2019

Proposed sui-generis residential development of 34 studio units, a communal space, an external communal deck, a refuse store and a cycle store in prefabricated container units located an existing surface car park at the end of Alexandra Park, Fishponds, Bristol.

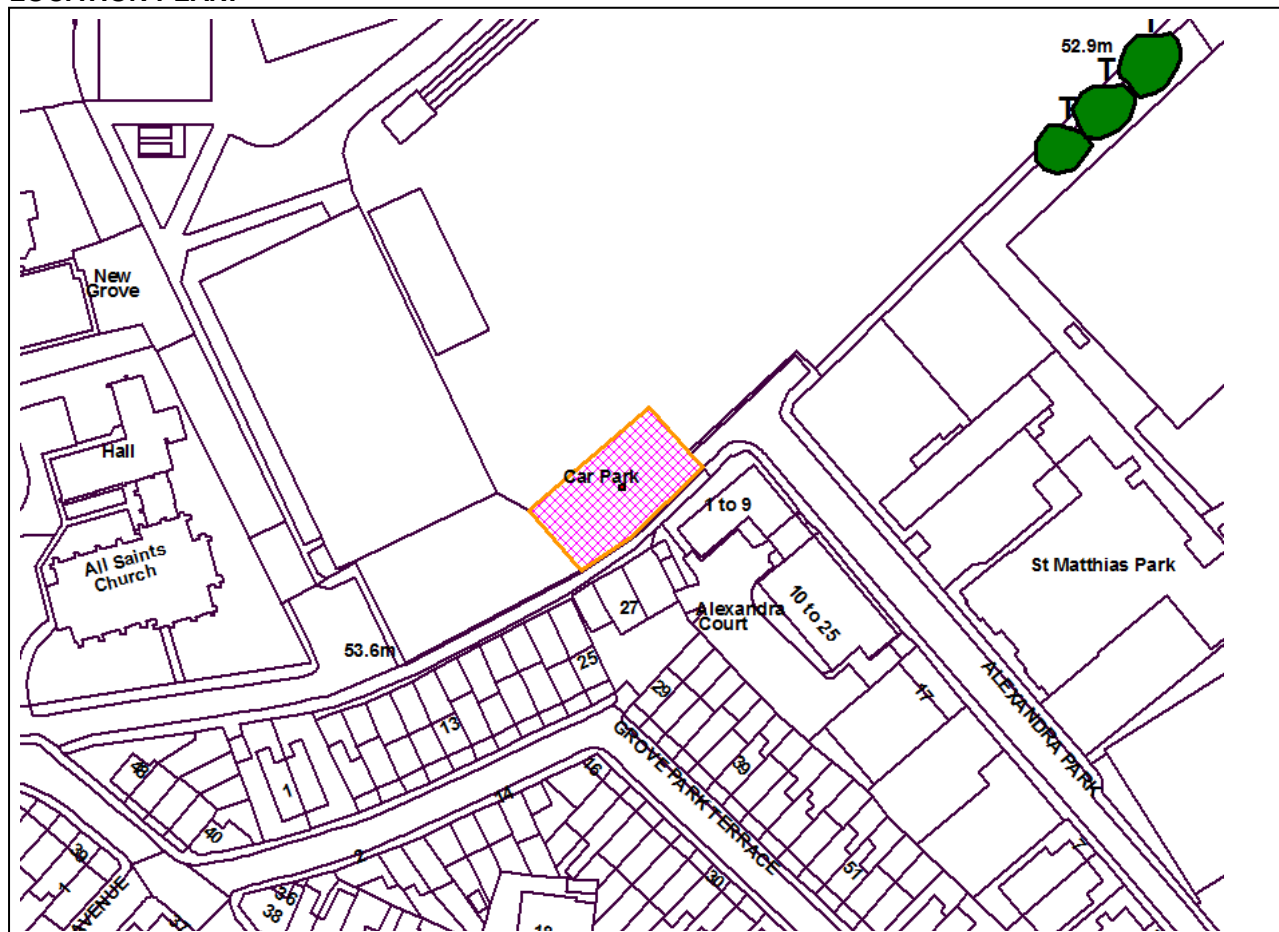
RECOMMENDATION: GRANT subject to Planning Agreement

AGENT: Alec French Architects
 27 Trenchard Street
 Bristol
 BS1 5AN

APPLICANT: United Communities Housing Association
 Eden House
 10 Eastgate Office Park
 Eastgate Road
 Bristol
 BS5 6XX

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



Development Control Committee B – 13 March 2019**Application No. 18/06358/F : Alexandra Park Public Car Park Alexandra Park Fishponds Bristol BS16 2BG****SITE DESCRIPTION**

The application relates to a car park located at the northern extent of Alexandra Park, in Eastville, east Bristol.

The site consists of hardstanding accessed from the south-eastern corner. Metal fencing surrounds the remainder of the perimeter. A total of nine trees are located on or adjoining the site. The car park is owned and managed by Bristol City Council, however it is considered surplus to requirements at this point in time.

The car park is bounded by playing fields associated with the Bristol Metropolitan Academy to the north, east and west and a footpath linking Alexandra Park with Grove Road to the south. A three storey block of flats and a collection of single storey sheds for car repairs are located to the south of the site.

The site is located on the edge of the Fishponds Town Centre and adjoins the Important Open Space designation associated with the school fields.

RELEVANT HISTORY

There is no relevant history.

APPLICATION

Full planning permission is sought for the erection of 31 modular studio units on the car park for students and care leavers aged 16 to 25.

Agreement has been reached with the Council to lease the site to a housing association (United Communities) to house young people for up to two years.

The modular units would be clad in metal sheets to create the appearance of shipping containers which would be green and yellow in colour. The units would be accessed via a semi-external staircase. The entrance to the staircase would be gated.

The proposed modular units would be arranged in two sets of seven 'containers' with a central corridor. The units would be stacked to create a three-storey structure.

At ground floor level, two of the units would be used for bin and cycle storage and a further two units would be conjoined to create a communal area. A communal deck would be located in the north western corner of the site. A total of eight studio units would be located at ground floor level.

At first floor level, the building would consist of 13 studio units with a central corridor. Three units in close proximity to the apartment block to the south of the building would include louvres on windows.

At second floor level, three studio units closest to the existing apartment block have been omitted to reduce the bulk and massing of the building. As a result, the arrangement is of a set of six and four units connected by a central corridor.

Each studio unit would contain a bedroom / living area, a kitchen and a bathroom and would consist of circa 22sqm of floorspace.

Development Control Committee B – 13 March 2019**Application No. 18/06358/F : Alexandra Park Public Car Park Alexandra Park Fishponds Bristol BS16 2BG**

In terms of resident mix, it is anticipated that the proposed development would be a 50/50 split of students and care leavers.

The proposed development would be managed by the United Communities Housing Association.

EQUALITIES ASSESSMENT

The public sector equalities duty is a material planning consideration as the duty is engaged through the public body decision making process.

“S149 of the Equalities Act 2010 provides that a public authority must in the exercise of its functions have due regard to:

- (a) eliminate discrimination, harassment, victimisation and any other conduct prohibited under the Act*
- (b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it*
- (c) foster good relationships between persons who share a relevant characteristic and those who do not share it.”*

During the determination of these applications due regard has been given to the impact of the scheme upon people who share the protected characteristics of age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation. In their assessment of these applications your officers are satisfied that any adverse impacts can be addressed and mitigated through the detailed design of the buildings and the imposition of appropriate conditions.

PRE-APPLICATION COMMUNITY CONSULTATION

The Applicant has carried out pre-application community consultation, as detailed in the Statement of Community Involvement (SCI) report submitted with the planning application. This includes details of public and stakeholder engagement undertaken prior to the submission of the planning application and how key stakeholders and the public were notified of the proposals.

Consultation activities included, but were not limited to:

- Ward Councillor meetings
- A public exhibition event
- Social Media, including engagement with Fishponds Planning Group
- Wider Press and PR
- Meetings with local businesses and community groups

The statement sets out the feedback from public consultation was largely positive. However, key concerns raised included parking, securing affordable housing for young people and tree planting.

The statement sets out how responses were considered and how, if appropriate, they were considered as part of the proposals.

Development Control Committee B – 13 March 2019

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RESPONSE TO PUBLICITY AND CONSULTATION

General response from the public

In response to the proposals as originally submitted 13 comments were received from 12 respondents from interested parties in response to the application.

Of the 13 comments on the application submitted, 11 of these comments were in objection to the scheme.

The following issues were raised:

- Concerns that the scheme would result in over development of the site
- Concerns about lack of parking, with related concerns about congestion and trip generation
- Concerns for the change of use from car park to residential development and the impact the local high street and community uses (Fishponds Rd and Dance Station)
- Concerns about loss of light/overshadowing of adjacent properties of Alexandra Court
- Concerns about overlooking and loss of privacy of residents within Alexandra Court due to proximity between habitable rooms
- Concerns about noise generated by future residents through the communal area
- Concerns that the proposed scheme is poorly sited for future residents in terms of transport connections and proximity to the city centre
- Concern about the suitability of the proposed area for deliveries and waste collection outlined in the Delivery and Service plan
- Concerns that the proposed design is out of keeping with local area

In addition to the above, other non-material planning considerations that were raised within public consultation related to impacts on local business and trade, alongside impact on surrounding property values.

Response from interest groups and organisations

BRISTOL CIVIC SOCIETY

The Bristol Civic Society made a comment in support for the proposed scheme. The following key issue was raised:

- Concerns about the proximity of proposed accommodation to the properties to the south and the potential overlooking issues that are required to be re-addressed.

Development Control Committee B – 13 March 2019**Application No. 18/06358/F : Alexandra Park Public Car Park Alexandra Park Fishponds Bristol BS16 2BG****Response from internal consultees**

CITY DESIGN GROUP – No objection

“CDG lauds the social benefits of the proposed scheme and supports the development in principle. However, the site is very constrained and a number of design related issues remain to be addressed. CDG is unable to support the current application and recommends the applicants to reconsider the some aspects of design in order to overcome the concerns.

The issues that remain to be addressed are;

- Removal of trees – the proposal is not supported by a necessary documentation and method statement. Please refer to the Tree Officer’s Comments. It is recommended to minimise loss of Category A & B trees, provide method statement and mitigation measures to address this concern.

- Site planning – the proposed arrangement of 11 metres overlap with the existing three storey apartment blocks with only 6 metres separation distance is not acceptable arrangement. It is recommended to review the layout and massing of the proposal in order to realise an acceptable separation distance for a suburban location. The overlap on the ground floor can be addressed by reviewing opening on the ground floor. On the first and second floor it is recommended to review the design of/ possibly omit units (2.11, 2.12 and 2.13) to achieve acceptable privacy and relation with the residents of the existing neighbours.

- Space standards – A planning advisory note is currently being prepared looking at relaxing space standards for time limited emergency short term housing in certain circumstances. It appears that the proposal meets the criteria; however, the case officer is advised to confirm this aspect.

- Appearance – the site is not a sensitive location so there is no objection to design aesthetics of the proposed pre-fabricated development. The only reservation is the large ‘LaunchPad’ signage on the north east elevation. It is recommended to explore creative solutions that can be effectively used to portray the identity of scheme in a less obvious manner.

CDG looks forward to engaging with the applicants to review some of the aspects of the proposed development in order to address the outstanding concerns.”

In response to the comments above, further information was provided in relation to trees. The scheme was amended to address concerns about the impact upon amenity by omitting units 2.11, 2.12 and 2.13 to reduce any overbearing or overshadowing and louvres were added to windows to reduce any overlooking.

TRANSPORT DEVELOPMENT MANAGEMENT – No objection

The site is located at the end of Alexandra Park, a no through road which has a turning head, is subject to a 20mph speed limit and has on-street parking, some of which cannot be used between the hours of 7am and 6.30pm as well as double yellow lines. Running along the sites boundary is a footpath - Public Right of Way (BCC/254/20) which connects with Thingwall Park. The area is primarily residential in nature although there is St Matthias Academy on the opposite side of the carriageway to the site in question.

The Transport Statement sets out that the site is in a sustainable location as nearby Fishponds Road is a major bus route and the Bristol and Bath Railway Path is a short cycle ride away. The TRICs data submitted sets out that the site would at most generate around three vehicular movements a day. Given the demographic status of the intended residents it is highly unlikely that they will be car

Development Control Committee B – 13 March 2019**Application No. 18/06358/F : Alexandra Park Public Car Park Alexandra Park Fishponds Bristol BS16 2BG**

owners. Whilst the loss of the car park may cause problems for parents wishing to drop their children off at the neighbouring academy, there is some on-street parking available and the site can easily be reached on foot.

No car parking is proposed. Based on the analysis within the Transport Statement this is acceptable. In respect of cycle storage, an internal store is proposed that will be able to accommodate 34 cycles. Two Sheffield Stands will also be provided adjacent to the buildings entrance for the use of visitors. Both are acceptable.

An internal waste store is proposed which is acceptable providing it is suitably ventilated and that the doors which open out onto the adjacent footpath are at least 1.5m wide. Bristol Waste has indicated that the location of the store is acceptable.

Whilst the use of the car park has been agreed by Property Services a Highway Condition Survey must be undertaken prior to the siting of the building. The applicant must be made aware that they will be liable to make good any damage caused during its temporary placement.

Transport Development Management has no objection to the proposal providing a series of bollards are provided to protect the building and its occupants from vehicles utilising the turning head.

A Traffic Regulation Order (TRO) was requested to remove the car park from the definitive map and a minor s.278 agreement is required for the installation of bollards at the threshold of the site. This would also require a s.171 licence.

Conditions were requested for a general arrangement plan to be provided indicating highway works and a highway condition survey. Compliance conditions are requested for installation and implementation of cycle and refuse storage.

CONTAMINATED LAND ENVIRONMENTAL PROTECTION – No objection

The proposed development is sensitive to contamination and is situated on or adjacent to land which has been subject to land uses which could be a potential source of contamination. These uses include a landfill site, engineering works, saw mills and a leather factory.

The submitted desk study recommends further intrusive investigation which we support. The applicants are also advised that we hold a number of other site investigation reports on sites in the local area which can be provided upon request.

Conditions are recommended to be applied to any future planning consent for further site assessment, site characterisation, remediation scheme, and reporting of unexpected contamination.

ENVIRONMENTAL HEALTH – No objection

“I would agree with the Design Note that the background noise levels due to traffic will be low, but would have more concerns here with noise from the adjoining Bristol Metropolitan Academy playing fields and multi-use games area (MUGA) than noise from traffic. The playing fields and MUGA are not floodlit, there are already residential properties around the perimeter of the academy and I am not aware of any complaints regarding noise from the academy and therefore I feel that it is very unlikely that noise from the academy will cause any harm to the occupants of the proposed development.”

A framework premises management plan was submitted to address concerns about noise from nearby residents. In response to the information submitted, the Environmental Health Officer stated:

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“I’ve read through the Management Plan and am happy that there is a plan in place to manage the premises. I would ideally need the plan to give a bit more detail as to how the premises will actually be managed. I assume that the site will not have an on-site manager but some visits/checks will be and there may be some remote monitoring and a 24 hour call out system. This is the sort of information I would like to see in the management plan, so whilst I am happy with what is provided and this doesn’t give me any concerns I would still like to see the Management Plan condition placed on any approval.”

NATURE CONSERVATION – No objection

Several trees are proposed to be removed as part of this proposal. All species of wild birds, their eggs, nests and chicks are legally protected until the young have fledged. The following planning condition is recommended which accords with the recommendations in the ecological survey dated October 2018.

In accordance with Policy DM29 in the Local Plan, the provision of living (green/brown) roofs is recommended to provide habitat for wildlife. Policy DM29 states that ‘proposals for new buildings will be expected to incorporate opportunities for green infrastructure such as green roofs, green walls and green decks.’ It is recommended that the feasibility of incorporating living roofs which minimise the use of Sedum (stonecrop) is considered.

Conditions are requested that a suitably qualified ecological consultant monitors any tree loss.

SUSTAINABLE CITIES – No objection

Further information regarding the design life of the building, ventilation, enhancements to daylighting and overheating was submitted by the applicant to address concerns from the Sustainable Cities team.

Concerns have been raised that the proposed use of electric heating and hot water would not meet the heat hierarchy set out with the Core Strategy.

Details of how the scheme would meet the requirements to reduce carbon dioxide emissions, as set out within Policy BCS14, should be secured via condition. The principle of the use of solar PV panels onsite has been agreed, with the details on the quantum and the location to be finalised.

ARBORICULTURE – No objection

The Arboricultural Impact Assessment (AIA) and Arboricultural Method Statement (AMS) set out that six trees would be lost to enable development and proposes the planting of 12-13 replacements adjacent to the site. The principle of this is agreed with the details of the exact location to follow via the Amendment Sheet / by condition.

The proposed approach to use pile and beam foundations is considered acceptable and would limit detrimental impacts upon the retained trees T7 and T8.

At the time of writing, conditions are being agreed to ensure that tree loss is mitigated and these will follow by way of the Amendment Sheet.

FLOOD RISK MANAGEMENT – No objection

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A full detailed drainage strategy using Sustainable Drainage Systems will be required by way of condition.

RELEVANT POLICIES

National Planning Policy Framework – February 2019

Bristol Local Plan comprising Core Strategy (Adopted June 2011), Site Allocations and Development Management Policies (Adopted July 2014) and (as appropriate) the Bristol Central Area Plan (Adopted March 2015) and (as appropriate) the Old Market Quarter Neighbourhood Development Plan 2016 and Lawrence Weston Neighbourhood Development Plan 2017.

KEY ISSUES**A. IS THE PROPOSED DEVELOPMENT ACCEPTABLE IN PRINCIPLE?**

Paragraph 118 of the National Planning Policy Framework (NPPF) sets out that substantial weight should be given “to the value of using suitable brownfield land within settlements for homes and other identified needs” and that planning decisions should promote and support the development of under-utilised land and buildings, especially if this would help to meet identified needs for housing.

Policy BCS5 sets out that the Core Strategy aims to deliver new homes within Bristol's existing built up areas. Between 2006 and 2026, 30,600 new homes will be provided in Bristol.

Policy BCS18 of the Core Strategy states that all new residential development should maintain, provide or contribute to a mix of housing tenures, types and sizes to help support the creation of mixed, balanced and inclusive communities.

Policy BCS20 states that development should maximise opportunities to re-use previously developed land.

Policy DM2 sets out that specialist student housing schemes and shared housing will not be permitted where they cause excessive noise and disturbance, unacceptable levels of on-street parking, a detrimental impact to existing buildings or inadequate storage for refuse and cycles.

Housing for Care leavers

The proposed development would contribute positively to the meeting housing needs for both students and young people.

The proposed reuse of a car park, considered to be surplus to requirements, for housing would accord with paragraph 118 of the NPPF and Policy BCS20 of the Local Plan.

The proposed mix of tenancy types is to be commended as an approach to addressing significant shortages of housing supply for care leavers whilst offering the opportunity for different types of people to mix and to foster a community.

It is noted that the proposed development would meet immediate, short term needs for affordable housing for vulnerable people with tenancies of one to two years.

Within the Fishponds Road Local Super Output Area (LSOA), flats make up approximately 41% of housing stock, and approximately a quarter of dwellings have one bedroom only.

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It is considered that the proposed additional one bedroom dwellings would contribute positively to the housing mix of the area and would not result in an unacceptable prevalence of this housing type.

Student accommodation

Students within the Fishponds Road LSOA equate to 6.4% of the population. As a result of this scheme, the proportion of students within the LSOA would increase to 7.1% of the total population.

This is considered to be a minor increase in the number of students and there would remain a relatively low number of students similar to the Bristol average of 7.2%.

As such, it is considered development would not lead to such a harmful concentration of students within the area.

Summary

The proposed development is not considered to result in any of the detrimental impacts defined within Policy DM2 listed at the start of this key issue. The impacts set out in Policy DM2 are addressed fully in the respective amenity, design and transport key issues, however the assessment of the proposed development against this policy is summarised below.

Having reviewed the framework Premises Management Plan, submitted by the applicant, the Environment Health Officer is satisfied that there would not be any unacceptable impacts in terms of noise and disturbance. Further detail is provided within Key Issue D.

No car parking is proposed and the type of tenure is considered unlikely to lead to many, if any car trips to and from the site. Given the existing car park use on-site, vehicular trips to the site are expected to decrease as part of the proposals. The provision of refuse/recycling storage and cycle storage is considered acceptable. Bristol Waste has set out that it would collect refuse and recycling. Further discussion of this issue is provided within Key Issue C.

The design of the proposals and impact upon the existing area is set out within Key Issue B.

In conclusion, the principle of student and residential development for care leavers is considered to be acceptable.

B. WOULD THE PROPOSED DEVELOPMENT BE OUT OF SCALE AND/OR CONTEXT WITH THE SURROUNDING AREA?

Policy BCS21 of the Core Strategy aims to ensure that all new development in Bristol achieves high standards of urban design.

Policies DM26-28 of the Site Allocations & Development Management Policies require development to contribute to the character of an area through its layout, form, public realm and building design.

The proposed development would consist of a part two-storey, part-three storey building constructed of 31 modular units with semi-external access stairs and internal and external communal space.

The modular units would be constructed to appear like shipping containers, clad in green and yellow metal.

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The application site is not considered to be a sensitive location in terms of design, situated away from Listed Buildings and outside of any conservation area. The proposed development would be in close proximity to industrial-type buildings such as the “Dance Station” and the car repair site also constructed in metal to the south of the site.

The development has been designed to appear temporary in its nature and this is accurately reflected in the pre-fabricated construction and the shipping container aesthetic. In order to limit the visual impact of the proposed development on the area, a condition limiting the use of the site for residential purposes for a period of no more than 10 years should be attached to any permission.

The proposed scale and massing of the building has been reduced to limit the impact upon neighbouring properties, and this is outlined in more detail in Key Issue D. Overall, the scale of the building is not considered out of character with the surrounding area given the nearby three-storey block of flats to the south and three-storey school building on Alexandra Park.

The provision of communal facilities is supported and is considered to help engender a sense of community on site that will restrict issues of anti-social behaviour.

City Design Group has raised no objection to the proposals, and has requested that conditions are attached to provide further details of the proposed signage on-site.

The proposed development would result in the removal of six trees; however replacement trees are proposed to be planted in accordance with the tree replacement standards set out within Policy DM17. It is considered that the loss of the trees would not result in any unacceptable harm to the character of the area.

The proposed development is considered acceptable in terms of design and would be in accordance with Policy BCS21 and Policies DM26 to DM29.

C. IS THE IMPACT OF THE PROPOSED DEVELOPMENT UPON TRANSPORT AND HIGHWAYS ACCEPTABLE?

Policy BCS10 states that developments should be designed and located to ensure the provision of safe streets. Development should create places and streets where traffic and other activities are integrated and where buildings, spaces and the needs of people shape the area.

Policy DM23 of the Site Allocations and Development Management Policies outlines that development should not give rise to unacceptable traffic conditions and will be expected to provide safe and adequate access onto the highway network. With regard to parking, Policy DM23 states that this must be safe, secure, accessible and usable.

Transport Development Management was consulted as part of the application process and raised no objection to the proposals.

The proposed loss of the car park is considered acceptable as it is deemed surplus to requirements by the Council due to the high number of alternative car parks in the surrounding area that serve the Town Centre.

The proposed development would be ‘car-free’ with no parking proposed onsite and no car ownership being a condition of residents’ tenancy. Given the demographic of residents, it is considered unlikely that residents would have access to a car and as such, vehicular movements to and from the site limited to drop-offs and servicing. These are anticipated to be minimal, as demonstrated within the

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TRICS analysis undertaken (Transport Report, KTC) which shows vehicle movements would be approximately three movements a day.

The site is within a sustainable location, located circa 200 metres from shops and services on Fishponds Road as well as a high-frequency bus route providing links to the city centre, Keynsham and Emersons Green.

Cycle storage would be located at ground floor level and include a total of 30 spaces in two-tiered, stacking storage. A further two Sheffield stands are proposed for residents, with four visitor spaces at the entrance to the site. This is in excess of the guidance set out within Appendix 2: Parking Standards Schedule of the SADM Policies and is considered acceptable.

Refuse and recycling storage would be located at ground floor level and would be within a sufficient distance from the highway to be considered acceptable by Bristol Waste. Further details of the quantum of the provision will be secured via condition. This should equate to the same level of provision set out within Bristol Waste's consultation response of 11 January 2019.

Transport Development Management has no objection to the proposal subject to bollards being provided to protect the building and its occupants from vehicles.

A TRO was requested to remove the car park from the definitive map and a minor s.278 agreement is required for the installation of bollards at the threshold of the site. This would also require a s.171 licence.

Conditions were requested for a general arrangement plan to be provided indicating highway works and a highway condition survey. Compliance conditions are requested for installation and implementation of cycle and refuse storage.

It is considered that, subject to conditions and conclusion of highways agreements, the proposed development would be acceptable in terms of transport and highways.

D. WOULD THE PROPOSED DEVELOPMENT CAUSE ANY UNACCEPTABLE HARM TO RESIDENTIAL AMENITY?

Paragraph 123 part (c) of the NPPF sets out that when considering applications for housing, authorities should take a flexible approach in applying policies or guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site (as long as the resulting scheme would provide acceptable living standards).

Policy BCS15 that development should provide flexibility and adaptability, allowing future modification of use or layout, facilitating future refurbishment and retrofitting.

Policy BCS18 sets out that residential development should provide sufficient space for everyday activities and to enable flexibility and adaptability by meeting appropriate space standards.

Policy BCS21 outlines that development in Bristol is expected to safeguard the amenity of existing development and create a high-quality environment for future occupiers.

Policy DM29 sets out that new buildings will be designed to ensure that the existing and proposed development achieves appropriate levels of privacy, outlook and daylight.

BRE Report 209, "Site Layout Planning for Daylight and Sunlight – A Guide to Good Practice" sets out that when a development is opposite existing neighbouring windows and a line drawn at 25 degrees

Development Control Committee B – 13 March 2019**Application No. 18/06358/F : Alexandra Park Public Car Park Alexandra Park Fishponds Bristol BS16 2BG**

from the lowest habitable window intersects the proposed development, further information is required to determine impact upon residential amenity. Where a development sits below this line, there would be limited impact upon residential amenity through overshadowing and no overbearing.

Existing residents:

The proposed development would consist of a part two-storey, part three-storey building located at the northern extent of Alexandra Park. The proposed development would be located circa 6 metres from a three-storey apartment building to the south of the site known as Alexandra Court. The building is on the southern side to the footpath linking Alexandra Park and Grove Road. The playing field associated with the Bristol Metropolitan Academy is located immediately to the north of the site.

Each of the proposed studio units would contain a window up to 2.1 metres in height and 1.8 metres wide. The windows of the proposed units 1.11, 1.12 and 1.13 (as indicated on the first floor plan) would contain metal louvres designed to screen views to Alexandra Court.

In response to concerns from officers and neighbours in relation to overbearing and overshadowing, the scheme was amended to omit the third storey of the building where it overlaps with Alexandra Court.

As a result of this amendment, the proposed development would sit below a 25 degree line drawn from the ground floor windows of those apartments to the south at Alexandra Court and would accord with BRE Guidance.

Future residents:

The proposed development would be located circa 6 metres from Alexandra Court. This is approximately half the ideal distance that is set out in guidance. Whilst failing to meet the guidance, the impact upon amenity is limited due to the orientation of the site to the north. This would limit the amount of overshadowing to Alexandra Court but would likely result in shadowing of units 1.11-1.13 within the proposed scheme.

Whilst typically this would create an unacceptable living environment for future occupiers of the application site, the proposed short-term nature of the tenancies would limit potential detrimental impacts. It should also be noted that this only applies to three of the 31 studio units proposed and overall, the living environment is considered to be acceptable for future occupiers.

Any issues of overlooking to habitable rooms of neighbouring dwellings would be mitigated by the inclusion of louvres to windows. A post occupation condition (Condition 21) has been included which requires these windows to be obscured to safeguard the amenities of the adjoining premises from overlooking and loss of privacy.

In terms of the living environment for future occupiers, the proposed development would benefit from communal internal and external space. The proposed studios would include ventilation and extraction to minimise overheating and odours for future residents. The proposed modular units are designed to meet Building Regulations, which would be assessed as part of a separate application.

There is no requirement for student accommodation to meet the Nationally Described Space Standards (NDSS); however they would typically be applicable to the care leavers. Given the nature of the modular housing and the recent approval of the 'Snug' House by DC Committee (app. ref. 18/04579/F), which utilises modern methods of construction; it is considered that the NDSS are not applicable to this site. NDSS is not considered appropriate to this case as they do not take into account modern methods of construction nor do they consider the short-term nature of this housing.

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In order to minimise potential detrimental impacts upon future residents associated with a limited living space, a condition should be attached to any consent limiting the length of tenancy to a maximum of 24 months.

Noise pollution from the site would be managed through the implementation of a Premises Management Plan, which builds upon the framework plan submitted as part of this application. The Environmental Health Officer has requested that this is secured via condition.

Summary:

In accordance with the NPPF, impacts upon neighbouring properties have been minimised where possible and the less-than-ideal living environment for future occupiers is considered to be outweighed by the efficient use of the site to meet significant unmet need for short-term housing.

E. DOES THE PROPOSED DEVELOPMENT GIVE SUFFICIENT CONSIDERATION TO SUSTAINABLE DESIGN AND CONSTRUCTION?

Policy BCS13 sets out that development should contribute to both mitigating and adapting to climate change, and to meeting targets to reduce carbon dioxide emissions.

Policy BCS14 sets out that development in Bristol should include measures to reduce carbon dioxide emissions from energy use by minimising energy requirements, incorporating renewable energy sources and low-energy carbon sources. Development will be expected to provide sufficient renewable energy generation to reduce carbon dioxide emissions from residual energy use in the buildings by at least 20%.

Policy BCS15 sets out that sustainable design and construction should be integral to new development in Bristol. Consideration of energy efficiency, recycling, flood adaption, material consumption and biodiversity should be included as part of a sustainability or energy statement.

The proposed development would utilise modern methods of construction and would consist of modular units, assembled quickly on-site.

Whilst permission is sought for a period of 10 years, it is anticipated that these buildings would have an extended design life of double that and as such, they would be readily relocated to other appropriate sites.

The proposed method of electric heating and hot water would not meet the heat hierarchy set out in Policy BCS14. The applicant has set out that air source heat pumps were not considered technically feasible due to short-term use and the constrained nature of the site. The proposals for electric heating and hot water were justified by the applicant as a suitably sustainable solution due to the decarbonisation of the grid; however this approach is not supported by Sustainable Cities. The increased costs associated with electric heating proposals would not be passed on to the residents as rent payable will include all bills.

The proposed use of extractors to minimise potential odours and condensation is supported. This would reduce the need to open windows and the result increases in energy demand and running costs.

A smaller proportion of a reduction on carbon emissions to meet Policy BCS14 has been agreed in principle. This would be based on the likely lifetime of the use on-site of 10 years and would likely result in the provision of PV panels onsite, where feasible, or the inclusion of additional panels on another

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site being progressed by the applicant. This is considered acceptable by Sustainable Cities subject to the details being provided. An update will be provided to Members via the amendment sheet.

A condition should be attached to any permission for further details of the proposed solar PV panels.

In conclusion, the proposed development has given sufficient consideration of sustainable design and construction. In terms of planning balance, whilst the proposed development would utilise electric heating and hot water, this is considered to be outweighed by the benefits of utilising modern methods of construction and the socially sustainable benefits of the scheme.

F. WOULD THE PROPOSED DEVELOPMENT RESULT IN ANY UNACCEPTABLE IMPACTS UPON TREES?

Policy BCS9 states that:

"Individual green assets should be retained wherever possible and integrated into new development. Loss of green infrastructure will only be acceptable where it is allowed for as part of an adopted Development Plan Document or is necessary, on balance, to achieve the policy aims of the Core Strategy. Appropriate mitigation of the lost green infrastructure assets will be required."

Policy DM17 sets out that where tree loss or damage is essential to allow for appropriate development, replacement trees of an appropriate species should be provided, in accordance with the tree compensation standard.

The proposed development would result in the removal of six trees and the pruning of others which adjoin and overhang the site.

An Arboricultural Method Statement and an Arboricultural Impact Assessment have been submitted to set out the approach to tree works and the proposals are considered acceptable to the Arboricultural Officer.

The proposed tree loss would be compensated in accordance with the tree replacement standard set out within Policy DM17 through the planting of approximately 12 new trees adjacent to the site on the Bristol Metropolitan Academy playing fields. This has been agreed between the management of the school site and the applicant.

The proposed approach to use pile and beam foundations is considered acceptable and would limit detrimental impacts upon the retained trees T7 and T8. It is considered that there would be no unacceptable impacts upon trees.

At the time of writing, conditions are being agreed to ensure that tree loss is mitigated and these will follow by way of the Amendment Sheet.

CONCLUSION

The proposed development is considered acceptable in principle by addressing a specific need for short-term housing for care leavers and provides affordable student accommodation.

There would not be an overconcentration of student accommodation within the area, and nor would the proposed development result in any of the harmful impacts set out within Policy DM2.

The proposed development would be acceptable in terms of design and transport and highways.

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Sufficient mitigation is proposed to replace trees identified to be removed, with details to follow by way of the Amendment Sheet.

Concerns have been raised in terms of residential amenity however these are considered to be addressed by reducing the bulk and massing of the building and providing louvres to windows. The living environment is considered acceptable given the short-term nature of occupancy and the need for this type of housing.

Both the applicant / agent and officers have worked extensively to address issues of sustainability, and the social sustainability benefits of the project are laudable. In terms of ensuring that the proposal sufficiently meets Policy BCS14, a lower reduction in carbon emissions has been agreed in principle and the details of this are to be addressed by way of the Amendment Sheet. The proposal for electric heating and hot water does not meet the heat hierarchy but in terms of planning balance, it is considered that this is outweighed by the other benefits of the scheme.

The application for full planning permission is recommended for approval subject to conditions and planning agreement.

CIL

The proposed development is liable for CIL totalling £57,304.63.

PLANNING AGREEMENT

The following items are sought via planning agreement to ensure that the proposal is acceptable in planning terms:

- Installation of bollards at entrance to the site
- 13 no. replacement trees on adjacent land

Conditions**Time limits for commencement of development**

1. Full planning permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

Pre commencement conditions

2. Further details – materials

Prior to the commencement of the relevant part of the works, samples or precedent images showing the following shall be submitted and approved in writing by the Local Planning Authority:

- Proposed cladding and materials
- Proposed windows and doors
- Proposed signage

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The development shall be completed in accordance with the approved details.

Reason: In the interests of securing high quality urban design.

3. Further details – refuse and recycling

Prior to the commencement of the relevant part of the works, details of the proposed quantum and type of storage for refuse and recycling shall be submitted to and agreed in writing by the Local Planning Authority. The approved storage shall be implemented prior to the first occupation of the use and retained thereafter in perpetuity.

Reason: To safeguard the amenity of the occupiers of adjoining premises, protect the general environment, and prevent obstruction to pedestrian movement, and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

4. Further details – solar PV panels

Prior to the commencement of the relevant part of the works hereby approved, details relating to the photovoltaic panels (including the exact location, dimensions, design/technical specification and method of fixing) shall be submitted to and agreed in writing by the Local Planning Authority. The approved equipment shall be installed and operational prior to the first occupation of the use which they serve and retained as operational thereafter in perpetuity.

Reason: To ensure that the development contributes to mitigating and adapting to climate change and to meeting targets to reduce carbon dioxide emissions and to ensure that the external appearance of the building is satisfactory.

5. Highway Works

Prior to commencement general arrangement plan(s) indicating the following works to the highway shall be submitted and approved in writing by the Local Planning Authority:

- Installation of bollards at the threshold of the site

Indicating proposals for:

- Threshold levels of the finished highway and building levels
- Alterations to waiting restrictions or other Traffic Regulation Orders to enable the works
- Locations of lighting, signing, street furniture, street trees and pits
- Structures on or adjacent to the highway
- Extents of any stopping up or dedication of new highway

These works shall be completed prior to occupation of the development to the satisfaction of the Local Highway Authority

Reason: In the interests of public safety and to ensure that all road works associated with the proposed development are planned and approved in good time to include any statutory processes, are undertaken to a standard approved by the Local Planning Authority and are completed before occupation.

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NB: Planning consent is not consent to work in the highway. A Highway Agreement under Section 278 of the Highways Act 1980 must be completed, the bond secured and the City Council's technical approval and inspection fees paid before any drawings are considered and approved and formal technical approval is necessary prior to any works being permitted."

6. Highway Condition Survey

No development shall take place (including investigation work, demolition, siting of site compound/welfare facilities) until a survey of the condition of the car park has been submitted to and been approved in writing by the Local Planning Authority. The survey must consist of:

- A plan to a scale of 1:1000 showing the location of all defects identified;
- A written and photographic record of all defects with corresponding location references accompanied by a description of the extent of the assessed area and a record of the date, time and weather conditions at the time of the survey.

N.B. On the removal of the building any damage caused to the car park during the temporary siting of the modular building must be made good to the satisfaction of the Highway Authority. For further information please contact the Transport Development Management Team by emailing transportdm@bristol.gov.uk

Reason: To ensure that any damage to the car park sustained during the temporary siting of the building can be identified and subsequently remedied at the expense of the developer.

7. Further site assessment

A site specific risk assessment and intrusive investigation shall be carried out to assess the nature and extent of the site contamination and whether or not it originates from the site. The investigation and risk assessment must be undertaken by competent persons and a written report of the findings must be produced. The results of this investigation shall be considered along with the reports submitted with the original application. The written report of the findings shall be submitted to an approved in writing by the Local Planning Authority prior to any works in connection with the development, hereby approved, commencing on site. This investigation and report must be conducted and produced in accordance with DEFRA and the Environment Agency's Model Procedures for the Management of Land Contamination, CLR 11.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other off site receptors.

8. Land affected by contamination - Submission of Remediation Scheme

No development shall take place until a detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property and the natural and historical environment has been prepared, submitted to and been approved in writing by the Local Planning Authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

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Reason: To ensure that risks from land contamination is understood prior to works on site both during the construction phase to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

9. Land affected by contamination - Implementation of Approved Remediation Scheme

In the event that contamination is found, no development other than that required to be carried out as part of an approved scheme of remediation shall take place until the approved remediation scheme has been carried out in accordance with its terms. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Following completion of measures identified in the approved remediation scheme, a verification report that demonstrates the effectiveness of the remediation carried out must be produced, and be approved in writing of the Local Planning Authority.

Reason: To ensure that risks from land contamination both during the construction phase and to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

10. Arboriculturally-led design of foundations

CONDITION WORDING TO FOLLOW.

11. Arboricultural Method Statement for tree removal

CONDITION WORDING TO FOLLOW.

12. Arboricultural Supervision

CONDITION WORDING TO FOLLOW.

13. Sustainable Drainage Systems (SuDS)

The development hereby approved shall not commence until a Sustainable Drainage Strategy and associated detailed design, management and maintenance plan of surface water drainage for the site using SuDS methods has been submitted to and approved in writing by the Local Planning Authority. The approved drainage system shall be implemented in accordance with the approved Sustainable Drainage Strategy prior to the use of the building commencing and maintained thereafter for the lifetime of the development.

Reason: To prevent the increased risk of flooding by ensuring the provision of a satisfactory means of surface water disposal is incorporated into the design and the build and that the principles of sustainable drainage are incorporated into this proposal and maintained for the lifetime of the proposal.

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Pre-occupation conditions

14. Implementation/Installation of Extract/Ventilation System – Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the extract/ventilation systems shown on the approved plans have been installed in accordance with the approved plans, and thereafter shall be permanently retained.

Reason: To safeguard the amenity of nearby premises and the area generally

15. Implementation/Installation of Refuse Storage and Recycling Facilities – Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the refuse store, and area/facilities allocated for storing of recyclable materials, as shown on the approved plans have been completed in accordance with the approved plans. Thereafter, all refuse and recyclable materials associated with the development shall either be stored within this dedicated store/area, as shown on the approved plans, or internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the public highway or pavement, except on the day of collection.

Reason: To safeguard the amenity of the occupiers of adjoining premises, protect the general environment, and prevent obstruction to pedestrian movement, and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

16. Completion and Maintenance of Cycle Provision – Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the cycle parking provision shown on the approved plans has been completed, and thereafter, be kept free of obstruction and available for the parking of cycles only.

Reason: To ensure the provision and availability of adequate cycle parking.

17. Land affected by contamination - Reporting of Unexpected Contamination

In the event that contamination is found at any time when carrying out the approved development that was not previously identified it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with the requirements of Condition 6 and where remediation is necessary a remediation scheme must be prepared in accordance with the requirements of Condition 7, which is to be submitted to and be approved in writing by the Local Planning Authority.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority in accordance with condition 8.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

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18. Premises Management Plan

Prior to the occupation of the use hereby approved, a Premises Management Plan shall be submitted to and approved in writing by the Local Planning Authority. The plan shall accord with the Framework Student/Tenant Management Plan (received 28 February 2018) and include the following unless otherwise agreed in writing by the Local Planning Authority:

- The day to day management of tenants (including conduct, security arrangements and systems, complaint protocols, house rules)
- Details of tenancy conditions / agreement
- Overall maintenance and management of the site
- Management of waste and recycling

The Premises Management Plan shall be implemented prior to the first occupation of the development and maintained as such in perpetuity unless otherwise agreed in writing by the Local Planning Authority.

Reason: In the interests of protecting residential amenity for future and neighbouring occupiers.

Post occupation management

19. Limited Period Buildings

The building(s) hereby permitted shall be removed and the land restored to its former condition on or before 1 September 2029.

Reason: In accordance with the application and because the proposed building(s) would otherwise prove unacceptable in this location.

20. Tenancy limit

The building(s) hereby permitted shall not be occupied by an individual tenant for a period longer than 2 years consecutively from the start date of the individual's tenancy.

Reason: To ensure that the proposed development meets the needs of short-term tenants.

21. Obscured Windows

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and/or re-enacting that Order) the proposed windows to units 1.11, 1.12 and 1.13 as indicated on the approved plans shall be obscured with louvres to the specification agreed with the Local Planning Authority and shall be permanently maintained thereafter as such.

Reason: To safeguard the amenities of the adjoining premises from overlooking and loss of privacy.

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22. Restriction of Use of Roof

The roof area of the buildings hereby permitted shall not be used as a balcony, roof garden or similar amenity area without the grant of further specific planning permission from the Local Planning Authority.

Reason: To safeguard the amenities of the adjoining premises.

23. Landscape (Planting) Works - Shown

The planting proposals hereby approved shall be carried out no later than during the first planting season following the date when the development hereby permitted is ready for occupation or in accordance with a programme agreed in writing with the council. All planted materials shall be maintained for five years and any trees or plants removed, dying, being severely damaged or becoming seriously diseased within 5 years of planting shall be replaced with others of similar size and species to those originally required to be planted.

Reason: To ensure that the appearance of the development is satisfactory.

List of Approved Plans and Drawings

24. List of Approved Plans and Drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision:

AP-AFA-XX-ZZ-DR-A-1000 - REV 1 Site location plan, received 12 December 2018
 AP-AFA-XX-ZZ-DR-A-1102 - REV 1 Proposed site block plan, received 12 December 2018
 AP-AFA-XX-ZZ-DR-A-1102 – REV 1 Proposed Site Block Plan, received 12 December 2018
 AP-AFA-XX-GF-DR-A-1210 – REV 4 Ground Floor Proposed GA Plan received 26 February 2019
 AP-AFA-XX-01-DR-A-1211 REV 4 Level 01 Proposed GA Plan received 26 February 2019
 AP-AFA-XX-02-DR-A-1212 REV 4 Level 02 Proposed GA Plan received 26 February 2019
 AP-AFA-XX-05-DR-A-1230 REV 4 Proposed Roof Plan P4 received 26 February 2019
 AP-AFA-XX-ZZ-DR-A-1250 REV 3 Proposed Northwest Elevation received 26 February 2019
 AP-AFA-XX-ZZ-DR-A-1251 REV 5 Proposed Southeast Elevation received 26 February 2019
 AP-AFA-XX-ZZ-DR-A-1252 REV 5 Proposed Northeast Elevation received 26 February 2019
 AP-AFA-XX-ZZ-DR-A-1253 REV 3 Proposed Southwest Elevation received 26 February 2019
 AP-AFA-XX-ZZ-DR-A-1270 REV 5 Proposed Section A-A received 26 February 2019
 AP-AFA-XX-ZZ-DR-A-1271 REV 5 Proposed Section B-B P5 received 26 February 2019
 AP-AFA-XX-ZZ-DR-A-1272 REV 3 Proposed Section C-C P3 received 26 February 2019
 AP-AFA-XX-ZZ-DR-A-1273 REV 3 Proposed Section A-A (large scale) P3 received 26 February 2019
 AP-AFA-XX-ZZ-DR-A-1274 REV 3 Proposed Section B-B (large scale) P3 received 26 February 2019
 AP-AFA-XX-ZZ-DR-A-1275 REV 3 Proposed Section C-C (large scale) P3 received 26 February 2019
 5034 SK09C Typical Residential Unit REV C, received 1 March 2019
 5034 SK06D 3D View from Northeast REV D, received 26 February 2019
 Appendix C - Arboricultural survey, received 12 December 2018
 Appendix D - Ecological survey, received 12 December 2018
 Appendix E - Desk study - 1 of 2, received 12 December 2018
 Appendix E - Desk study - 2 of 2, received 12 December 2018
 Appendix F - Transport report V 2, received 12 December 2018
 Appendix G - Sustainability statement, received 12 December 2018

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Appendix H - Acoustics report, received 12 December 2018
 Appendix I - statement of community involvement, received 12 December 2018
 Supplementary information, received 12 December 2018
 Wessex water foul sewage plan, received 12 December 2018
 Design and access statement - Nov 2018, received 12 December 2018
 Addendum to Sustainability Statement, received 26 February 2019
 Communal Room Statement, received 26 February 2019
 Framework Student Management Plan, received 28 February 2019
 Arboricultural Impact Assessment and Arboricultural Method Statement, received 1 March 2019

Reason: For the avoidance of doubt.

Advices**1. Traffic Regulation Order (TRO)**

You are advised that a Traffic Regulation Order (TRO) is required. You must submit a plan to a scale of 1:1000 of an indicative scheme for a TRO, along with timescales for commencement and completion of the development. Please be aware that the statutory TRO process is not straightforward; involving the public advertisement of the proposal(s) and the resolution of any objections. You should expect a minimum of six months to elapse between the Highway Authority's TRO Team confirming that it has all the information necessary to enable it to proceed and the TRO being advertised. You will not be permitted to implement the TRO measures until the TRO has been sealed, and we cannot always guarantee the outcome of the process. We cannot begin the TRO process until the appropriate fee has been received. To arrange for a TRO to be processed contact the Highway Authority's Transport Development Management Team by emailing transportdm@bristol.gov.uk

N.B. The cost of implementing any lining and signing required by the TRO is separate to the TRO fees, which solely cover the administration required to prepare, consult, amend and seal the TRO.

2. Public Right of Way

The property boundary of the development hereby approved abuts a Public Right of Way PROW BCC/254/20 You are advised that before undertaking any work you must contact the Highway Authority's Public Rights Of Way Team by emailing rightsofway@bristol.gov.uk Whilst it may be unlikely that the Public Right Of Way will be affected by the proposed development PROW BCC/254/20:

- Should remain open, unobstructed and safe for public use at all times;
- No materials are to be stored or spilled on the surface of the PROW;
- There must be no encroachment onto the width of the PROW;
- No vehicles are to use the PROW without lawful authority of the landowner(s), unless a private right of way is shown on property deeds. It is the applicant's responsibility to ensure that the appropriate private right exists or has been acquired from the landowner.
- Any scaffolding and/or skips placed over or adjacent to the PROW must not obstruct public access or inconvenience the public in their use of the way and must be properly licensed. Licences can be found online at www.bristol.gov.uk/highwaylicences

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- Any interference of the Public right of way either whilst demolition/construction is in progress or on completion, may well constitute a criminal offence.

If construction works are likely to temporarily affect the right of way, a Temporary Traffic Regulation Order (TTRO) may be required to close or divert the PROW for the duration of the works on the grounds of safety of the public. To discuss and/or apply for a TTRO contact the Highway Authority's Network Management Team by emailing traffic@bristol.gov.uk

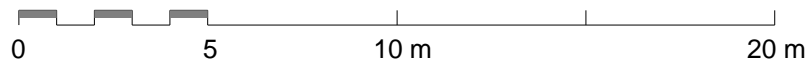
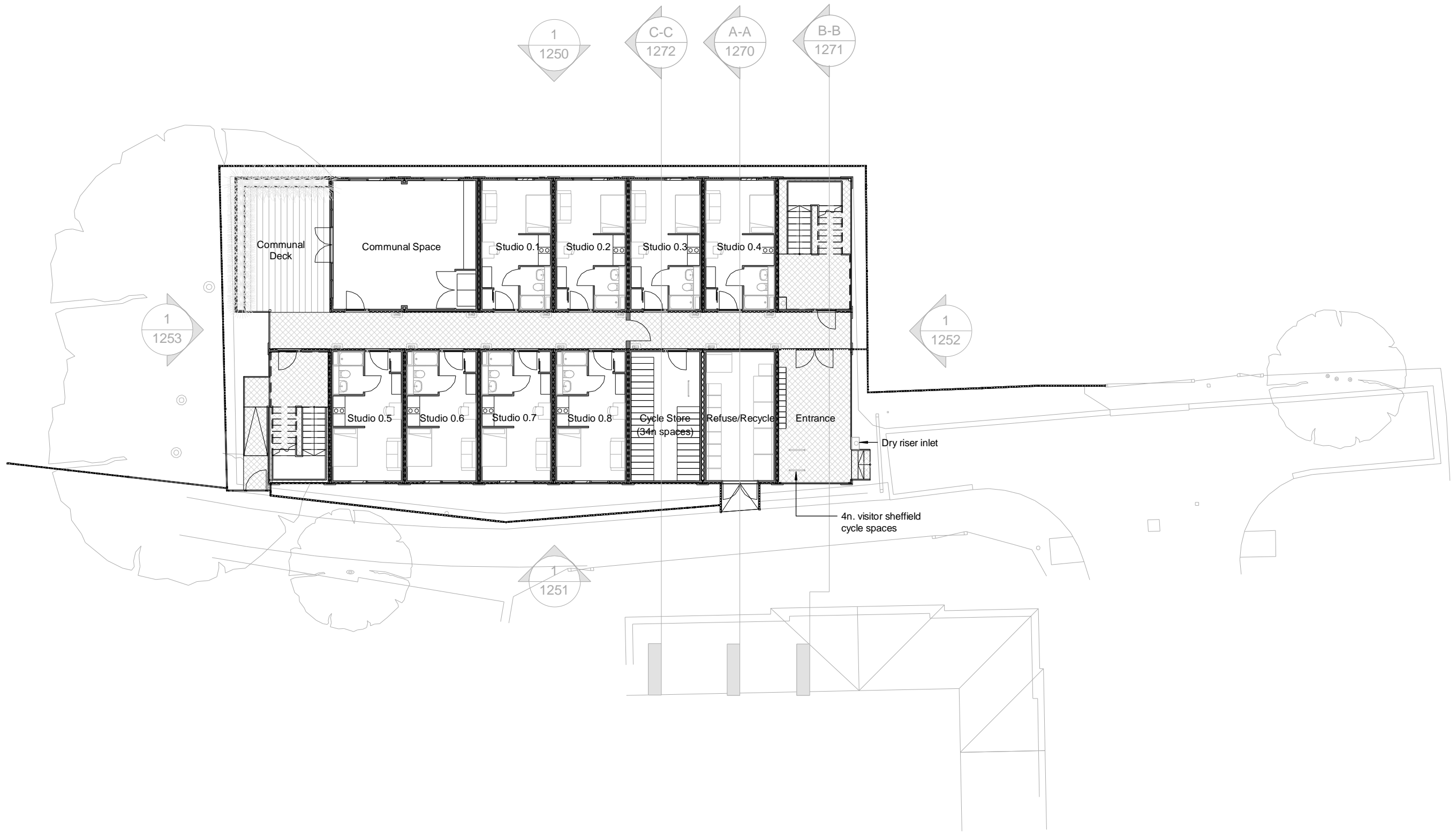
3. Nesting Birds

Anyone who takes, damages or destroys the nest of any wild bird whilst that nest is in use or being built is guilty of an offence under the Wildlife and Countryside Act 1981 and prior to commencing work you should ensure that no nesting birds will be affected.

Supporting Documents

3. Alexandra Park Public Car Park

1. Proposed GA plan level GF
2. Proposed GA plan level 02
3. Proposed Southeast elevation
4. 3D view from Northeast
5. Extract from Design and access statement



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REFER TO REGISTER FOR ALL HAZARD IDENTIFICATION

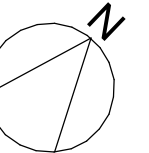
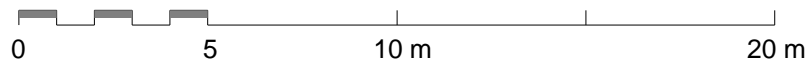
Rev	Description	Date	Drawn	Checked
1	Communal, refuse and cycle store location amended	04/10/18	MK	ND
2	Communal space relocated; communal deck added; laundry added; staircases replanned; fire door added to centre of access corridor; accessible unit 0.1 added	07/11/18	MK	ND
3	Communal space reduced; wheelchair accessible unit omitted; 1x unit added.	20/11/18	MK	ND
4	Fire doors added to staircases; Staircase layout amended	22/11/18	MK	ND

Status Codes
S0: WIP
S1: Coordination
S2: Information
S3: Iit. Review
S4: Construction
D1: Costing
D2: Tender
D3: Contractor Design
D4: Procurement
A: Construction
B: Partially Signed-off
AB: As Built

AlecFrenchArchitects
 27 Trenchard Street, Bristol, BS1 5AN, T. 01179293011 F. 01179221121
 E. mail@alecfrench.co.uk www.alecfrench.co.uk

Alexandra Park Launch Pad
 Alexandra Park, Fishponds
 Level GF Proposed GA Plan

Scale	Date	AFA Project No.	Drawn by	Checked by
1 : 200 @A3	28/09/18	5034	MK	ND
Drg.No.			Rev.	Status
AP - AFA - XX - GF - DR - A - 1210			P 4	S2



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REFER TO REGISTER FOR ALL HAZARD IDENTIFICATION

Rev	Description	Date	Drawn	Checked
1	Background plan amended	04/10/18	MK	ND
2	Staircases replanned, background updated	07/11/18	MK	ND
3	Fire doors added to staircases, Staircase layout amended	22/11/18	MK	ND
4	Units 2.11, 2.12 and 2.13 omitted	11/02/19	MK	ND

Status Codes
S0: WIP
S1: Coordination
S2: Information
S3: Iit. Review
S4: Construction
D1: Costing
D2: Tender
D3: Contractor Design
D4: Procurement
A: Construction
B: Partially Signed-off
AB: As Built

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 E. mail@alecfrench.co.uk www.alecfrench.co.uk

Alexandra Park Launch Pad
 Alexandra Park, Fishponds
 Level 02 Proposed GA Plan

Scale	Date	AFA Project No.	Drawn by	Checked by
1 : 200 @A3	28/09/18	5034	MK	ND
Drg.No.			Rev.	Status
AP - AFA - XX - 02 - DR - A - 1212			P 4	S2



Page 120

Materials

Walls:
 (01) Painted corrugated steel panels
 (02) PPC aluminium louvre vents
 (03) Painted blockwork

Roof:
 (04) Painted steel roof
 (05) Galvanised steel roof to walkways

Windows:
 (06) Plastic framed glazing system

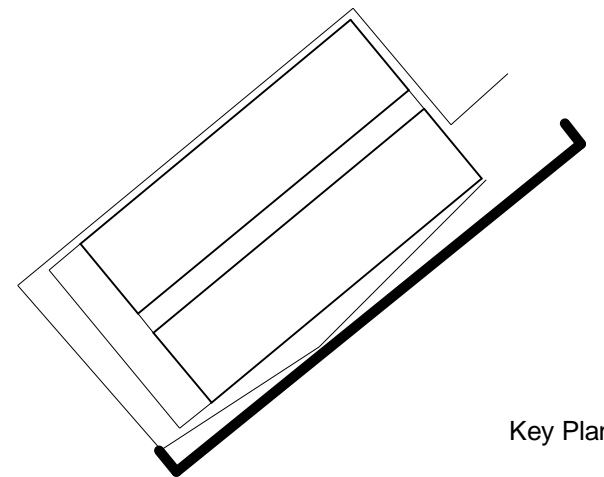
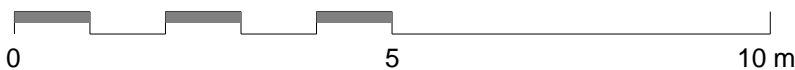
Doors:
 (07) Plastic framed solid doors with glazed side panel
 (08) Plastic framed solid doors
 (09) Plastic framed louvred doors

Stairs / Ramps:
 (10) Painted steel staircase / ramp

Railings and balustrades:
 (11) Painted steel railings and balustrades to terraced areas, external walkways and staircases.

Boundary treatments:
 (12) Galvanised steel palisade fencing

Other:
 (13) Planting
 (14) Painted metal planters
 (15) Angled painted steel privacy screens



Key Plan

RESPONSIBILITY IS NOT ACCEPTED FOR ERRORS MADE BY OTHERS IN SCALING FROM THIS DRAWING. ALL CONSTRUCTION INFORMATION SHOULD BE TAKEN FROM FIGURED DIMENSIONS ONLY DETAILS AND SIZES SHOWN ARE INDICATIVE ONLY AND ARE SUBJECT TO CONFIRMATION BY THE RELEVANT SUB CONTRACTOR.

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CDM HEALTH & SAFETY REGULATIONS

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Rev	Description	Date	Drawn	Checked
1	Communal, refuse and cycle store location amended	04/10/18	MK	ND
2	Staircases replanned; communal deck added; background updated	07/11/18	MK	ND
3	Staircases amended; Brown roof omitted	26/11/18	MK	ND
4	Units 2.11, 2.12 and 2.13 omitted	11/02/19	MK	ND
5	Painted steel angled privacy Screens added to units 1.11, 1.12 and 1.13	18/02/19	MK	ND

Status Codes	
S0: WIP	
S1: Coordination	
S2: Information	
S3: Int. Review	
S4: Construction	
D1: Costing	
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Alexandra Park Launch Pad
 Alexandra Park, Fishponds
 Proposed Southeast Elevation

Scale	Date	AFA Project No.	Drawn by	Checked by
1 : 100 @A3	28/09/18	5034	MK	LL
Dwg No.	Rev.	Status		
AP - AFA - XX - ZZ - DR - A - 1251	P 5	S2		



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Rev	Description
A	Communal, refuse and cycle store location amended
B	Staircases replanned; background updated
C	Units 2.11, 2.12 and 2.13 omitted
D	Painted steel angled privacy Screens added to units 2.11, 2.12 and 2.13

Date	Drawn	Checked
04/10/18	MK	ND
07/11/18	MK	ND
04/02/19	MK	ND
18/02/19	MK	ND

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Alexandra Park Launch Pad					
Alexandra Park, Fishponds					
3D View from Northeast					
Scale	@A3	Drawn by	Checked by	Job No.	Dirg.No.
Date	28/09/18	MK	ND	5034	SK06
Rev.	D				
Stage	RIBA Stage 3				



Existing aerial view of the site from south (image from www.google.com/maps)

2. Project Overview

The project is a partnership between United Communities HA (UC), University of Bristol's Student Union (UBSU), 1625 Independent People (1625ip) and Bristol City Council (BCC) with the support of the property charity LandAid. A number of the consultants involved have donated their time at cost or on a pro bono basis in support of the project.

The scheme will house young people with low-level support needs who have been homeless and are moving on from 1625ip's supported housing and students on low incomes and facing financial hardship. This will provide desperately needed move-on accommodation for young people progressing from 1625ip's supported housing, and support the University's widening participation aims.

The site is being provided by BCC on a temporary basis for ten years. The project is essentially temporary in nature.

The vision is to create a diverse, supportive and sustainable community of Bristol University students and young people moving on from 1625ip, linked within their local and Bristol University community. By breaking down some of the perceptions of barriers between Bristol University and the wider City, the aim is to promote a closer working relationship whereby the University is an active participant in affordable housing solutions, not just for its students but for other young people too.

Members, through self-management of the scheme will contribute to their community, become active citizens, gain valuable skills, make friendships and take ownership of decisions and their lives.

The scheme will be affordable and target young people and students in financial hardship. All young people will be able to access University facilities and support.

The project will be developed using prefabricated modular

units specifically fitted out to create good quality housing and be located on a relatively central site. The scheme will house 34 young people. The design team will work together to develop a low impact scheme with good thermal performance and good quality design.

The aim is that should this project prove successful it will act as a prototype for developing a number of other sites in Bristol to meet both a real need but also encourage the building of resilient communities.

4. Design Statement

4.01 Brief

As noted earlier in this report the project is about creating an innovative community of young people and students as well as meeting a need for affordable housing for both these uses groups. The project essentially is for residential accommodation. Residents are unlikely to stay less than a year and may stay up to two years. The current proposal is that the scheme will be located on this site for a maximum of ten years before being relocated. This offers a model that could be reproduced on other temporary sites.

Due to the innovative use proposed Bristol City Council Planning Officers have been consulted and whilst the proposed units are smaller than the 37m² (the studio units are 21.5m²) which would normally be required for dwellings, by providing communal space to accommodate shared facilities, the development can acceptably be classified as a Sui Generis residential use.

The communal facilities are an integral part of the project as they are seen as being fundamental to the ambition to create a vibrant community. The communal facilities provided are:

- Large communal room for meals and socialising
- Laundry
- Bike store
- External deck

Each residential unit has an en-suite with a shower and WC, a cooking area with 2 ring hob and microwave and sink, storage space, space to study, room for a single bed, electrical heating and an openable window for natural ventilation. The construction and fit is designed to meet current building regulations. The size of the units is restricted by the site and maximum load size for lorry transport for modular buildings without a police escort.

The client brief noted the scheme will need to:

- Be erected with limited foundation and sub-structure work on site.
- Be capable of being re-located to another site without compromising their structural integrity.
- Have a communal area containing a kitchen and a launderette.
- Include a communal refuse and recycling store
- Include a secure communal cycle store

As a result the proposed design was developed as a form of modular construction. Expressing the construction as part of the design solution was part of the brief to identify this project as being different from more conventional housing schemes because of its ability to be relocated to other temporary sites and speedy delivery.

A variety of modular units were considered, but the most cost effective and re-locatable option has proved to be prefabricated purpose built steel containers.

The design process has included the end user via a number of focus groups, which UBSU and 1625ip have convened.



Site Location Plan

Development Control Committee B – 13 March 2019

ITEM NO. 4

WARD: Stoke Bishop CONTACT OFFICER: David Macfadyen

SITE ADDRESS: Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ

APPLICATION NO: 18/04727/F Full Planning

DETERMINATION DEADLINE: 15 March 2019

Demolition of existing WCs. and construction of cafe, replacement WCs and education booth.

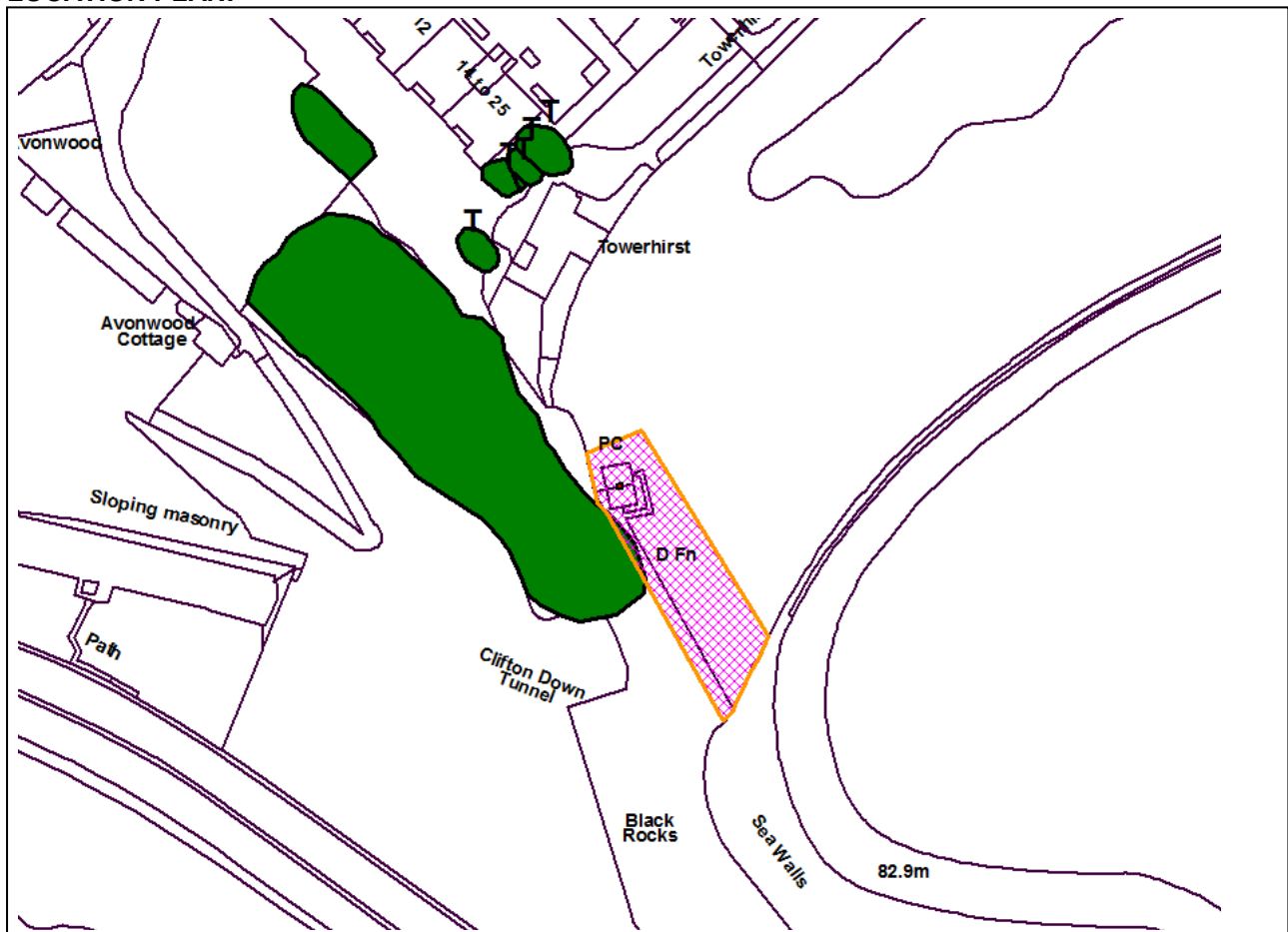
RECOMMENDATION: Grant subject to Condition(s)

AGENT: Chris Goodsall Architect
48 Birchall Road
Bristol
BS6 7TT

APPLICANT: The Downs Committee
151 Whiteladies Road
Bristol
BS8 2RA

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



Development Control Committee B – 13 March 2019**Application No. 18/04727/F : Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ****SUMMARY**

The proposed development involves demolition of the existing public toilet block at Sea Walls, north of Circular Road on Durdham Down open space, and construction of a replacement building including public toilets, café and education facilities. The site is within a sensitive context, both in terms of heritage and ecology. The proposed building would be of similar height to the existing toilet block but would be approximately double the overall size. The building would be of simple, modern design including a steel frame structure, predominantly glazed walls and flat green roof. It is noted that a first floor roof terrace area which previously formed part of the proposed building has now been removed from the application due to officer concerns regarding the increased height and massing this resulted in and resultant greater landscape impact.

Following public consultation, there have been a total of 96 responses received in objection and 37 responses received in support. Key concerns relate to the principle of building upon the open space, the appropriateness of the proposed design, the scale of the building, impact to wildlife and the post development management of impacts of the uses proposed (litter, car parking, noise etc.).

Following assessment, it is the conclusion of officer's that the proposed development would represent a very rare scenario where a minor degree of development on the open space would accord with national and local planning policy and would consequently be acceptable. This is due to the use of the proposed building supporting use of the wider open space for sports and recreational purposes and therefore ancillary to the designation as Important Open Space. Furthermore, the development would replace an existing building in the same location and it would be screened to some extent by an existing treeline, thus limiting the impact to open character of the area. The proposed design is found to be of a high quality and would provide a light weight and elegant appearance. The development would be of a scale, position and design which would cause negligible harm to adjacent buildings of historic significance. The proposals would also enhance the setting and significance of an adjacent historic drinking fountain. Subject to safeguarding conditions, it is found that the building would avoid detrimental impact to ecology, biodiversity, trees, green infrastructure, neighbouring amenity and living conditions, surrounding highways, public safety, climate change and flood risk. Specifically, conditions will be applied requiring a site management plan including detailed proposals for day-to-day management of the proposed uses as well as details of security measures.

In light of these conclusions, subject to the conditions outlined within the report, the development is found to be in accordance with relevant national and local planning policy. There are no material considerations which would warrant the refusal of planning permission. Consequently, it is the recommendation of officers that permission is granted subject to conditions.

SITE DESCRIPTION

The development site relates to an existing public toilet block which serves The Downs open space and adjacent land. The building is located to the far north western corner of the open space, sited approximately 40m north of Circular Road and the Sea Walls viewing point. The building is of single storey scale with a flat roof and stone walls. It is understood to date from the 1950s. The building comprises separate facilities for males, females and disabled users with level access from the front (east) and male facilities accessed from the rear (west). Durham Down open space surrounds the site to the north, south and east, with the Sea Walls clifftop and the Avon Gorge located to the west. The suburb of Sneyd Park is located further north of the site. This area is predominantly residential in use and character, commencing at Towerhirst (Grade II Listed) situated approximately 35m north of the site. A Listed (Grade II) drinking fountain is also located in the clifftop wall to the south west of the toilet block. The site is within The Downs Conservation Area, forms part of a Site of Nature Conservation Interest, is designated a Local Historic Park and Garden as well as Important Open Space. The Avon Gorge, adjacent to the west of the site is designated a Site of Special Scientific Interest as well as a Special Area of Conservation.

Development Control Committee B – 13 March 2019**Application No. 18/04727/F : Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ**

PLANNING HISTORY

Application ref:	Proposal:	Decision:
05/04487/FB	Forming new door opening in external wall to allow formation of accessible W.C and construction of concrete ramp	GRANTED - 06.01.2006
18/02629/F	Construction of Cafe and Replacement WCs and Education Booth. Demolition of Existing WCs	WITHDRAWN - 14.08.2018

Since withdrawal of the above application for a similar proposal in August 2018, the applicant made a pre application submission to enter discussions with the Local Planning Authority which have culminated in the current application.

APPLICATION

The application seeks full planning permission for the proposed demolition of the existing toilet block and construction of a replacement building which would include a café, public toilets and an educational booth.

The proposed building would be sited similarly to the existing toilet block, oriented parallel with the adjacent cliff top wall to the west. The building would be larger than the existing toilets however, measuring 17m in width by 6.6m depth. The building would be of a single storey, a maximum of 3.2m in height from adjacent ground level.

The buildings construction would comprise a rectangular frame of ten steel columns spaced equally within the eastern and western elevations supporting a flat roof structure. The roof would comprise steel beams, plywood construction topped by single ply membrane with the upper face planted but for a flat glazed roof light and a flat array of solar panels. The proposed roof structure would overrun the perimeter walls creating a canopy structure.

The proposed café would be sited to the southern end of the building and would include large glazed walls. The toilets would be sited off centre, to the northern side of the building with timber clad walls and a small glazed element above. The northern end of the building would comprise the educational booth which would also be constructed with glazed walls. Both the northern and southern ends of the building would include series of full height glazed sliding doors which when open, would provide the effect of the building becoming a pavilion.

The proposed café would include a service bar and small area of indoor seating. A decking area is proposed surrounding the base of the building which would provide a further area of proposed outdoor seating. The café would have proposed opening hours of 8am to 7pm Monday to Saturday and 10am to 4pm on Sundays. It is proposed the café will be staffed by 2 full time staff and 2 part time staff. Bin storage relating to the café is proposed to the rear (west) of the building. This would be housed within an enclosure finished with woven stainless steel mesh. It is outlined that bins would be collected privately, with collections every second day. Café staff would manoeuvre bins to Circular Road for collection and then back to the building. A Sheffield stand housed within an enclosure which would accommodate cycle parking for x2 staff bicycles is proposed to the northern end of the building.

The proposed toilets would replace the existing public toilets at the site. It is highlighted that the Local Authority has cut funding to maintain public toilets across the city and the toilets at the site have been maintained by The Downs Committee since early 2018. Separate male, female and disabled facilities

Development Control Committee B – 13 March 2019**Application No. 18/04727/F : Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ**

are proposed to operate in accordance with the opening hours of the café. Another single unisex toilet would also be available overnight. It is suggested that this may be offered subject to coin operated lock and will be fitted with more robust facilities to protect against the risk of vandalism. The disabled facilities would also be available overnight but via Radar (Royal Association for Disability Rights) key lock.

The proposed education booth is designed to provide a base for the Avon Gorge and Downs Wildlife Project. This is an organisation set up to protect the wildlife interest of the Avon Gorge and Downs and raise awareness and understanding about the importance of the area for people and wildlife. The booth would have a total internal area of 7m² and would be used to facilitate the operation of the wildlife project and its various events. This would be expandable by use of outdoor decked area including retractable blinds to provide shelter in adverse weather conditions. The proposed blinds would be incorporated within the roof structure.

It is also proposed to re-instate the adjacent Listed fountain to a functional state and replace the existing tarmac surface adjacent to the fountain with individual paving tiles.

COMMUNITY INVOLVEMENT

The proposed development is classed as 'Minor' development; therefore there is no requirement for the Applicant to demonstrate community engagement prior to submitting the application. Nevertheless, it is understood that the applicant has reviewed the content of public consultation run by the Local Planning Authority on the previous application and as part of the current application, and revised proposals to reflect some for the responses.

EQUALITIES ASSESSMENT

During the determination of this application due regard has been given to the impact of this scheme in relation to the Equalities Act 2010 in terms of its impact upon key equalities protected characteristics. These characteristics are age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation. There is no indication or evidence (including from consultation with relevant groups) that different groups have or would have different needs, experiences, issues and priorities in relation this particular proposed development. Overall, it is considered that the approval of this application would not have any significant adverse impact upon different groups or implications for the Equalities Act 2010.

RESPONSE FROM PUBLICITY AND CONSULTATION

186 neighbouring properties were directly notified of the application via neighbour notification letter. The proposed development was also advertised via site notice and local press advert.

The deadline for responses was 5th December 2018.

A total of 96 responses in objection were received, 37 responses in support and 1 response classified as neutral.

A summary of the grounds of objection is as follows:

- Unsuitable for The Downs
- Additional car parking on Circular Road and potential overspill onto the grass
- Design inappropriate for Victorian context
- No need or requirement for another café on The Downs given the existing one near the water tower

Development Control Committee B – 13 March 2019**Application No. 18/04727/F : Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ**

- Additional rubbish and potential for littering surrounding the site directly associated with the café which would harm the appearance of The Downs
- The proposal will break the legislation which prevents development on The Downs (The Downs Act) and set a precedent for further development
- Green spaces should be protected
- Cafe will bring too many visitors to a rare, unusual and treasured site
- Presence of café will deter birds from the adjacent cliffs
- Wrong to develop a beauty spot and bound to result in negative effects to the character and setting of the area
- Existing toilets should simply be retained and upgraded
- Nobody should be allowed to build on The Downs
- The proposed building is x3 the size of the existing toilet block and not in the style of local buildings
- Harm to the setting of Towerhirst by moving the building closer to this site
- Impact to habitat within buffer zone of adjacent SSNI and SAC designated land
- Lack of Habitat Regulations Assessment relating to adjacent designated sites
- Change to the peace and tranquillity of the area
- Objections in relation to the inclusion of a roof terrace which converts a one storey building to a two storey building, making it more visible and increasing impact to neighbours
- Existing sewers may not be able to cope with the increased capacity required
- Increased risk of accidents involving children and vehicles directly relating to the café
- Potential for damage to the building and associated anti-social behaviour which will stretch the resources of the police
- Concerns relating to the building attracting more people to the area during the evening which will encourage anti-social and potentially criminal behaviour
- The design fails to blend in with the setting in a sensitive location
- The viability of a café at this location has not been demonstrated and no business plan presented. This causes concern the café may fail and become dilapidated
- The glass design will not appear as transparent as suggested
- Impact upon trees in the adjacent SSSI, potentially destabilising these
- The planted roof adds bulk to the form, increasing the impact upon the area
- Potential for the proposed bins to be left at the road side for a period following collection and detracting from the appearance of the area as well as encouraging foraging wildlife (sea gulls/rats etc.)
- There is no public transport nearby
- Food sold in the café may not be healthy resulting in increased obesity
- Potential overlooking and loss of privacy to neighbouring sites as a result of the proposed roof terrace

A summary of the grounds of support is as follows:

- The existing toilet building is an eyesore and provides little to the area
- Media reports of growth in 'coffee shops' suggest demand is there
- A good way to fund upkeep of the public toilets and The Downs as a whole
- The toilets need updating
- Support subject to enforcement of the opening hours as proposed
- Café is fine as long as it doesn't cause noise or block views

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- o Education is important to enhance visitor experience of The Downs and educate users which will help protect the area

ELECTED MEMEBERS & AMENITY GROUP RESPONSES

Darren Jones, MP for Bristol North West has commented on the application as follows:

"I have had mixed views from local residents. Some wish to express their view that no structural developments should be permitted on the Downs whilst others are more open to the proposals, so long as it is done in the right way.

Specific concerns raised with me include:

- The impact of having a "destination" on the Downs in respect of the number of visitors arriving by car and the subsequent impact on car parking in the neighbouring area;

- Concerns about the impact on the neighbouring environment, specifically in respect of rubbish and the impact of artificial lighting that might be kept on overnight in an area that is naturally dark; and

- The impact of the design on the landscape, including the feasibility of the proposed "terrace" for additional seating.

There was a general concern about this proposal being the "thin edge of the wedge" in respect of further commercialisation and development on the Downs, which would not be welcomed by local residents.

I trust that you will take my constituents views into regard when taking decisions on this issue".

Councillor John Goulandris, Ward Councillor for Stoke Bishop has commented on the application as follows:

"There are mixed views amongst residents about the need and advisability of yet another cafe on the Downs, but almost universal objection to the design of proposed new cafe. It fails totally to blend with its verdant setting. It does serious harm to the visual amenity and detracts from what is a truly unique location with iconic views across the Avon Gorge. The second storey and viewing platform compromise the privacy of nearby residential properties. They also add to the massing and make the building very prominent in what is meant to be a green open space.

Concerns have also been raised about likely increased traffic, litter, noise and potential anti social behaviour at night.

There is a strong body of thought that, if a new cafe is definitely thought to be a good thing, it could and should be located in a much less sensitive location.

In summary, a more sympathetic, less dominant design in a less sensitive location might be acceptable. This proposal, however, is unacceptable and I have to agree with residents, who have called the proposal 'a veritable blot on the landscape'."

Friends of The Downs & Avon Gorge Group:

"The Friends of the Downs supports the Downs Committee initiative for the construction of a cafe and public convenience but with some reservations.

Development Control Committee B – 13 March 2019

Application No. 18/04727/F : Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ

The Friends group take the pragmatic view that in the current financial atmosphere without the development the toilet facility will be lost. Severe budget cuts means the Downs has little choice but to explore all possible income streams.

We acknowledge that the Downs needs to be financially stable.

However there are concerns about creeping commercialisation of the Downs and the very thing people come to the Downs for is lost.

There also concerns that sewerage may be a problem, along with light pollution, security and litter.

FOD+AG have concerns of how the building will be managed particularly the proposed upper deck.

As an organisation we are not fully convinced that the design is an exemplar of a sustainable building and seems at odds with its surroundings”.

Clifton & Hotwells Improvement Society:

“The Clifton & Hotwells Improvement Society supports the retention and enhancement of these public conveniences, which are needed in this location. No doubt the café will help to fund their upkeep.

The creation of refreshment facilities in this popular spot is welcomed and the planned viewing platform is an excellent idea.

Concerns from some commentators regarding refuse storage and vandalism will need to be addressed”.

Conservation Advisory Panel:

“The Panel felt it should clarify that it regretted the fact an application had been lodged to build a cafe in order to fund the public lavatories. The sublime atmosphere of the Seawalls should remain free of urban commercial gentility and continue to be the site of nothing more than the occasional visit of an ice cream van.

To turn to the design of the proposed cafe, the Panel noted that the building would be nearer to Towerhirst than had been intended previously and that a semi-circular section of stone paving was now proposed in front of the listed fountain. The Design and Access Statement referred to both Mies's Farnsworth House and Philip Johnson's Glass House as precedents to be emulated in some ways, yet both have significantly greater transparency than that which is proposed, no first floor viewing platform and no balustraded access ramps and rubbish bins. It is unfortunate that the proposed 3D views of the proposed cafe show neither the access ramps nor the rubbish bins; the impression given is idealised.

It seems inevitable that secure, vandal resistant shutters would be required and these must be included in the design as, if added as an afterthought, they would degrade the proposed building.

The present view from the Seawalls at ground level is marvellous. There is no need for a first floor viewing platform”.

Development Control Committee B – 13 March 2019**Application No. 18/04727/F : Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ**

OFFICER RESPONSES TO PUBLIC CONSULTATION

It is highlighted that during the application process, a proposed first floor roof terrace area and external staircase were removed from the proposed building following concerns raised by both consultation and officers.

A number of comments refer to the Clifton and Durdham Downs (Bristol) Act 1861. It is highlighted that this is non-planning legislation and is not a material consideration in the determination of a planning application. The grant of planning permission would not override any other legislative process and legal approval under other legislative frameworks will be required irrespective of the outcome of this planning application. The determination of an application for planning permission holds no power over any other legislative process.

All other comments have been noted and are accounted for in the assessment beneath.

INTERNAL & EXTERNAL CONSULTEES

Avon Gardens Trust:

Avon Gardens Trust have looked at the revised submitted plans (November 2018), 18/04727/F and note that the proposed development is intended to be built on the enlarged site of the existing WC facilities. The AGT have reservations about the size of this proposed development and also the lack of night-time security from vandals. We consider that if these two reservations could be addressed satisfactorily, then we would consider the development to have less harm on the significance of this important Local Historic Park and Garden.

City Design Group, Bristol City Council:

Previously concerns were raised relating to the inclusion of the proposed roof terrace and the increased massing this caused. Following its removal, the proposals are supported subject to conditions.

Transport Development Management, Bristol City Council:

Clarifications were requested regarding the size of the bin store, waste management proposals and cycle parking for staff. These matters have all been clarified and are acceptable. Subject to conditions, the proposed development is acceptable in transport and highways terms.

Environmental Protection, Pollution Control, Bristol City Council:

No objection subject to the following conditions if the application is approved – conditions regarding no installation of equipment for the extraction and dispersal of cooking smells/fumes without the approval of the local planning authority, limits on noise from plant and equipment, limits on use of refuse and recycling hours as well as limiting the proposed opening hours.

Arboricultural Officer, Bristol City Council:

No major concerns regarding impact to trees. The boundary wall is likely to prevent the majority of the roots from entering the construction zone. Light pruning works have been specified to avoid damage to the branching during construction – this is reasonable management. The trees do not require protective fencing during construction as the boundary wall will create a sturdy shield. A condition is recommended to ensure works are carried out in accordance with all recommendations within the supplied arboricultural report.

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Nature Conservation Officer, Bristol City Council:

This proposal is located close to but outside the Avon Gorge Woodlands Special Area of Conservation (SAC). I consider that the proposed relatively small scale demolition and construction works will not cause a likely significant effect on the interest features of the SAC. As such, a Habitats Regulations Assessment (HRA) under The Conservation of Habitats and Species Regulations 2017 (as amended) is not required in this instance.

This proposal directly impacts upon part of a designated Site of Nature Conservation Interest (SNCI), Clifton and Durdham Downs. However, with best practice construction methods incorporating pollution prevention methods, including the minimisation of dust pollution, significant ecological impacts are not anticipated. This can adequately be controlled by means of conditions attached to any consent.

Flood Risk Officer, Bristol City Council:

No objection subject to a drainage condition. Drainage proposals should follow guidance within the document titled "Bristol City Council Sustainable Drainage Standing Advice".

Crime Prevention Design Advisor, Avon & Somerset Constabulary:

After reading through the Design and Access Statement I have no concerns, along with the local neighbourhood policing team that patrol the area for this proposed application.

However I could not find any mention for the safety and security measures to be in place for this building, it only makes reference to surveillance, whilst this is important, it should not be relied upon as the sole strategy for tackling crime and disorder. Therefore consideration for the following:

- Access control to enter the building
- CCTV
- Lighting
- Additionally to consider the security of the building when closed
- The daily takings of cash, will this be taken daily off site or left in a Safe within a secure room

Reading through some of the residence objections and including the Neighbourhood Policing Team (concerns regarding the additional litter that may occur from the Café if providing a take away service), I suggest bins should be provided and emptied when full and when clearing the tables outside should also be responsible and mindful to pick up additional rubbish that has been purchased from the Café. Additionally the Bin store area to be secured.

Wessex Water:

Wessex Water has no objections to this application providing items shown in red are resolved prior to determination of the application. Specifically, detail is required relating to the proposed foundation design due to proximity to a 3400mm tunnel sewer at 10m below ground level. The drainage strategy is acceptable to Wessex Water.

Network Rail:

Network Rail has no objection in principle to the above proposal but due to the proposal being next to Network Rail land and our infrastructure and to ensure that no part of the development adversely impacts the safety, operation and integrity of the operational railway we have

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included asset protection comments which the applicant is strongly recommended to action should the proposal be granted planning permission. The local authority should include these requirements as planning conditions if these matters have not been addressed in the supporting documentation submitted with this application.

RELEVANT LEGISLATION & POLICIES

The Planning (Listed Buildings and Conservation Areas) Act 1990
 National Planning Policy Framework – February 2019
 Bristol Local Plan comprising Core Strategy (Adopted June 2011) and Site Allocations and Development Management Policies (Adopted July 2014)
 Climate Change and Sustainability Practice Note (2012)
 The Downs Conservation Area Enhancement Statement (1993)

In determining this application, the Local Planning Authority has had regard to all relevant policies of the Bristol Local Plan and relevant guidance.

ASSESSMENT**DEVELOPMENT OF OPEN SPACE**

Paragraph 96 of the National Planning Policy Framework (NPPF) states “Access to a network of high quality open spaces and opportunities for sport and physical activity is important for the health and well-being of communities. Planning policies should be based on robust and up-to-date assessments of the need for open space, sport and recreation facilities (including quantitative or qualitative deficits or surpluses) and opportunities for new provision. Information gained from the assessments should be used to determine what open space, sport and recreational provision is needed, which plans should then seek to accommodate”.

Paragraph 97 then states “Existing open space, sports and recreational buildings and land, including playing fields, should not be built on unless:

- a) an assessment has been undertaken which has clearly shown the open space, buildings or land to be surplus to requirements; or
- b) the loss resulting from the proposed development would be replaced by equivalent or better provision in terms of quantity and quality in a suitable location; or
- c) the development is for alternative sports and recreational provision, the benefits of which clearly outweigh the loss of the current or former use”.

The local policy in relation to open space can be found at Policy BCS9 of the Bristol Core Strategy which states: “Open spaces which are important for recreation, leisure and community use, townscape and landscape quality and visual amenity will be protected.

Some areas of open space may be released, through the development plan process, for appropriate development where:

- They are no longer important for recreation, leisure and community use, townscape and landscape quality and visual amenity;
- Development of all or part of an open space would result in improved urban form or an enhancement to existing open space areas.

The supporting text for the policy proceeds to expand on the above by stating: “Whilst the Core

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Strategy aims to retain the vast majority of green open space within the city, not all such land is important for recreation, leisure and community use, townscape and landscape quality and visual amenity. Existing open spaces may also offer opportunities to contribute to the delivery of necessary development, improvements to urban form or enhancements to existing open space areas. In determining whether open space should be released community involvement will be central to decision making. Account will also be taken of open space standards, levels of provision in the locality and the importance of the open space for recreation, leisure and community use, its contribution to townscape and landscape quality and its visual amenity”.

Policy DM17 of the Site Allocations and Development Management Policies Local Plan (SADMP) provides more detailed advice regarding development involving existing green infrastructure assets. This policy confirms the designation of The Downs as “Important Open Space”.

In relation to Important Open Space, Policy DM17 states “Development on part, or all, of an Important Open Space as designated on the Policies Map will not be permitted unless the development is ancillary to the open space use”. Essentially this prohibits all by very limited types of development upon such land.

In this case, the proposed development involves demolition of the existing toilet block and construction of a replacement building in place. Whilst the existing toilet block has a footprint of 42m², the proposed building would have a footprint of 90m². As such, it is recognised that the proposals involve development of some previously undeveloped land adjacent to the existing building.

Paragraph 97 of the NPPF indicates open space should only be developed in the limited circumstances outlined above. In this case, in relation to criteria a) it is noted that the open space concerned is not deemed to be surplus to requirements, and in relation to criteria b) no replacement of open space is proposed in this instance. As such, the development does not qualify with either of these criteria. In relation to criteria c) the proposed development comprises construction of a replacement building for use as café, public toilets and educational purposes. The Local Planning Authority is satisfied that all three of these uses can be attributed to sports and recreational provision and would encourage and support the use of the open space for sports and recreational purposes. The proposal is therefore found to accord with paragraph 97 of the NPPF.

In relation to Core Strategy Policy BCS9, it is recognised that The Downs remain highly important for recreation, leisure and community use, townscape and landscape quality and visual amenity. As such, permitting development on the Important Open Space is subject to delivering an improved urban form or an enhancement of the open space area. The requirement for development on Important Open Space at Policy DM17 to be ancillary to the open space use is also noted.

As highlighted above, all three proposed uses are found to support use of the open space. Specifically, the proposed toilets would replace the existing public toilets, offering an upgrade upon the existing facilities which are recognisable dated. The provision of public toilets on The Downs is of paramount importance to facilitate equal use and access to the open space by all sections of society. The presence of publicly accessible toilets will enable prolonged use of the open space for its various sports and recreational purposes by all sections of the community. As facilities essential to use of the open space, the proposed public toilets would achieve an ancillary relationship to the open space.

The proposed café will serve and support visitors to The Downs by provision of hot/cold drinks and a range of food which (subject to proper management) would encourage, extend and enhance public use of the open space. It is not unusual for public parks or open spaces to include some form of provision for sale of food and drink. This would support the experiences of existing visitors to the open space. The café would be sited 1.2km from the existing café adjacent to the water tower, with no similar existing facilities available in this part of the open space. Whilst it is recognised that the proposed café is less essential than public toilets to the function of the open space, there would

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remain a direct link between the provision of food and drink supply and enjoyment of the public open space. Furthermore, the proposed café would also be directly linked to supporting the maintenance of the toilets with café staff taking responsibility for their management and maintenance including cleaning.

The educational booth is proposed for use by the Avon Gorge and Downs Wildlife Project. This is an organisation set up to protect the wildlife interest of the Avon Gorge and Downs and raise awareness and understanding about the importance of the area for people and wildlife. This use would promote access, understanding and knowledge of important characteristics of the area and therefore contribute to supporting the preservation and function of the area for open space use. As such, this use would also support the primary activities and role of the area as public open space.

There would be a direct link and relationship between all three of the proposed uses and the function of the open space for sports and recreational purposes. In this regard, the proposed use of the building would be ancillary to the main function of area as an open space. In light of the aforementioned factors, the proposed uses are found to be sufficiently ancillary to the use of The Downs as Important Open Space.

Overall the proposed uses are all found to stand to support and enhance user experience of the area as open space in accordance with Policy BCS9. It is highlighted that only a very small degree of currently developed open land is proposed for development when considering the scale of the open space as a whole. As such, the area would remain well served in terms of open space. Furthermore, the contribution of this land, directly adjacent to the existing toilet block and its contribution to enjoyment and use of the open space is found to be relatively limited. Users would generally likely utilise other parts of the open space which are not in close proximity of the toilet block. The current proposals will likely enhance experience of this part of the open space through upgrade and modernisation of the facilities as well as more regular cleaning due to the association with the café.

The impact of the building upon the character of the area will be discussed in greater detail beneath, however the proposed scale, form and design are not found to harm important characteristics of the area as open space. Consequently the development would not prejudice user experience of the open space by means of its position, scale, form or general design. Overall, the proposed uses are found to be suitably ancillary to the use of The Downs as Important Open Space and would avoid prejudicing this function. The development is therefore in accordance with the requirements of national and local planning policy and is acceptable in this regard.

APPEARANCE, CHARACTER & HERITAGE ASSETS

The Authority is required (under Sections 66 and 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990) to pay special attention to the desirability of preserving or enhancing the character or appearance of the conservation area as well as the setting and significance of Listed buildings or structures. The case of *R (Forge Field Society) v Sevenoaks DC* [2014] EWHC 1895 (Admin) ("Forge Field") has made it clear where there is harm to a listed building or a conservation area the decision maker "must give that harm considerable importance and weight".

Section 12 (Achieving well-designed places) of the NPPF outlines that "The creation of high quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities". Planning policies and decisions should aim to ensure that developments:

- a) will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- b) are visually attractive as a result of good architecture, layout and appropriate and

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effective landscaping;

- c) are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d) establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;
- e) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
- f) create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

Section 12 of the NPPF also states that "Permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions, taking into account any local design standards or style guides in plans or supplementary planning documents".

Section 16 (Conserving and enhancing the historic environment) of the NPPF outlines that heritage assets "are an irreplaceable resource, and should be conserved in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of existing and future generations.

Paragraph 189 of the NPPF states that "In determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary".

Paragraph 195 of the NPPF states that "Where a proposed development will lead to substantial harm to (or total loss of significance of) a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or total loss is necessary to achieve substantial public benefits that outweigh that harm or loss, or all of the following apply:

- a) the nature of the heritage asset prevents all reasonable uses of the site; and
- b) no viable use of the heritage asset itself can be found in the medium term through appropriate marketing that will enable its conservation; and
- c) conservation by grant-funding or some form of not for profit, charitable or public ownership is demonstrably not possible; and
- d) the harm or loss is outweighed by the benefit of bringing the site back into use.

Paragraph 196 of the NPPF states that "Where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable

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use".

Policy BCS21 (Quality Urban Design) of the Core Strategy advocates that new development should deliver high quality urban design that contributes positively to an area's character and identity, whilst safeguarding the amenity of existing development.

Policy BCS22 (Conservation and the Historic Environment) of the Core Strategy states that new development will safeguard or enhance heritage assets and the character and setting of areas of acknowledged importance including:

- Scheduled ancient monuments;
- Historic buildings both nationally and locally listed;
- Historic parks and gardens both nationally and locally listed;
- Conservation areas;
- Archaeological remains

Policy DM26 (Local Character & Distinctiveness) of the Site Allocations & Development Management Policies (SADMP) Local Plan outlines that all development is expected to contribute positively to an area's character and identity. The policy builds on policy BCS21 (above) by stipulating the characteristics which development should seek to respond to. General principles include:

- i. Responding appropriately to and incorporating existing land forms, green infrastructure assets and historic assets and features; and
- i. Respecting, building upon or restoring the local pattern and grain of development, including the historical development of the area; and
- ii. Responding appropriately to local patterns of movement and the scale, character and function of streets and public spaces; and
- iii. Retaining, enhancing and creating important views into, out of and through the site; and
- iv. Making appropriate use of landmarks and focal features, and preserving or enhancing the setting of existing landmarks and focal features; and
- v. Responding appropriately to the height, scale, massing, shape, form and proportion of existing buildings, building lines and set-backs from the street, skylines and roofscapes; and
- vi. Reflecting locally characteristic architectural styles, rhythms, patterns, features and themes taking account of their scale and proportion; and
- vii. Reflecting the predominant materials, colours, textures, landscape treatments and boundary treatments in the area.

The policy states that "development will not be permitted where it would be harmful to local character and distinctiveness or where it would fail to take the opportunities available to improve the character and quality of the area and the way it functions."

Policy DM27 (Layout and Form) of the SADMP outlines that the layout, form, pattern and arrangement of streets, open spaces, development blocks, buildings and landscapes should contribute to the creation of quality urban design and healthy, safe and sustainable places. It should make efficient use of land, provide inclusive access and take account of local climatic conditions.

Policy DM29 (Design of New Buildings) of the SADMP states the design of new buildings should be of

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high quality. Proposals for new buildings will be expected to:

- i. Be clearly organised in terms of their form and internal layout and circulation to reflect the hierarchy of function they will accommodate, the uses they will serve and the context they will address; and
- ii. Incorporate active frontages and clearly defined main entrances facing the public realm that emphasise corners and reinforce the most prominent frontages; and
- iii. Respond to the solar orientation of the building to support energy efficient design while ensuring as far as possible that active rooms face the public realm; and
- iv. Provide appropriate natural surveillance of all external spaces; and
- v. Ensure that existing and proposed development achieves appropriate levels of privacy, outlook and daylight; and
- vi. Allow for future adaptation or extension to accommodate alternative uses or to respond to the changing future needs or circumstances of occupiers by means of their internal arrangement, internal height, detailed design and construction; and
- vii. Provide appropriately for inclusive access and circulation; and
- viii. Incorporate opportunities for green infrastructure such as green roofs, green walls and green decks that may be accessed and used where appropriate; and
- ix. Incorporate exteriors and elevations that provide visual interest from a range of viewing distances and are visually organised and well-proportioned; and
- x. Incorporate high quality detail of an appropriate scale and proportion, arranged in a coherent way that contributes positively to the overall design approach of the building; and
- xi. Employ high quality, durable and sustainable materials of an appropriate texture, colour, pattern and appearance that contribute positively to the character of the area.

Policy DM31 (Heritage Assets) of the SADMP outlines that where development has an impact upon a heritage asset, it will be expected to conserve and, where appropriate, enhance the asset or its setting.

The site is located within The Downs Conservation Area and is also within the setting of Towerhirst and an adjacent drinking fountain which are both Grade II Listed structures for their historic significance. The Downs are also designated a Historic Park and Garden of local significance which represents a heritage asset under Policies BCS22 and DM31.

The important characteristics of the area which have warranted designation as a conservation area are described within The Downs Conservation Area Enhancement Statement. This document states: "The Conservation Area is dominated by the Downs, an expansive plateau of open parkland, defined by the Avon Gorge and Westbury Road to the west and east with the slopes of Clifton and Stoke Bishop to the south and north. This was bought up mainly by the Wills family and laid out for the people of Bristol in the Victorian period. It is now maintained by the City of Bristol. It divides into six distinct areas. The second, the main plateau is short grass land with some low shrubs edged along principal roads by avenue trees. This generous open area is enclosed by substantial buildings predominantly villas and institutional buildings dating from the Victorian and Edwardian period". General enhancement objectives for the conservation area are stated to include:

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- A traffic management scheme
- A study into restrictions on use of main roads at certain times
- The effects of Park & Ride
- Resisting loss of gardens for car parking where visible from The Downs
- Resist conversion of large dwellings to flats where excessive car parking will result
- Audit of significant street furniture of historic interest
- Landscape management scheme required to ensure maintenance of open space
- Groups of significant buildings will require strengthening in terms of character created by traditional materials and original features

The existing public toilets building and surrounding area are not specifically referenced within the Enhancement Statement. However the general characteristics of the wider area, the expansive plateau of open parkland comprising short grass land with some low shrubs edged along principal roads by avenue trees, does apply to the site. The existing building is relatively non-descript but does utilise stone cladding which is reflective of buildings within the local area. It is positioned sensitively, roughly equidistant between the Sea Walls viewing point and the grand dwelling of Towerhirst. This area is at the far north western corner of the open space, before the transition to the residential area of Sneyd Park. A row of low trees line the clifftop to the west of the site. The trees screen the existing toilet block in views from the west, including the Avon Gorge and Leigh Woods beyond. The flat roof form of the existing building with a maximum height of approximately 3m provides a relatively low profile to the structure. In longer views across the open space, the building does not appear prominently on the horizon given the backdrop created by the adjacent treeline. Whilst neutral and effective, the existing building would not however warrant retention or protection.

The proposed building would be built in a similar position, replacing the existing toilet block. It is recognised the proposed building would be larger than the existing building in terms of width, depth and overall footprint. The existing toilet block has a footprint of 42m² and the proposed building would total 90m². As such, the proposed building would be more than double the overall footprint. This increase would largely be in terms of total width, with the proposed building extending 1.8m further north and 6m further south than the existing. The building would have a similar rectilinear footprint and shape created by proposed flat roof form. The total height of the proposed building would be approximately 20cm taller than the existing toilet block.

In terms of proposed siting, the location of the proposed building on the site of the existing toilet block is supported. The presence of the existing structure at this location would help to offset some of the perceived impact of the proposed building upon the openness of the area. Furthermore, the position is roughly equidistant between the Sea Walls viewing point and Towerhirst, reducing the impact to both, as well as providing sufficient separation to avoid impact views of the Listed drinking fountain. Similar to the existing toilet block, the position would also benefit from the screening and backdrop provided by the clifftop trees to the west. This would further limit the impact of the building upon the openness of the area in longer views. As such, the proposed siting is supported.

With regards to the proposed scale, it is recognised that this would be significantly larger than the existing building. This would predominantly be to the south however, away from Towerhirst and therefore limiting the impact upon the setting of this adjacent Listed building. The increase in height proposed would be very minor which would work to limit perception of the increased scale. The majority of the additional mass would be positioned to the south and would benefit from the screening provided to the rear by the clifftop trees.

In relation to the detailed form and design, the building would utilise a free standing steel frame, comprising ten steel columns spaced equally within the eastern and western elevations supporting a flat roof structure. But for the proposed centrally positioned toilet block, the walls would be glazed. Whilst it is noted that glazed will not appear completely transparent in all settings and conditions, this

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will provide the building a light weight appearance and minimal structural form. This would further offset the increased scale of the building in comparison to the existing building. The structural steel columns will measure 15cm in width and the roof slab will measure 30cm in depth. The equal spacing of the structural columns will provide a coherent elevational layout. The minimal scale of these solid features is found to contribute to an overall elegant built form. The roof of the building would include an upper surface which would be planted with species native to the adjacent Avon Gorge. This would not be highly visible from ground level however would offset some of the impact of development of green land. The design includes a flat solar panel module to meet sustainability objectives whilst ensuring the low profile and elegant flat roof form are retained.

The proposed toilet block would be located off centre to the northern side. This would be finished with timber cladding. Whilst timber is not a commonplace cladding material utilised on adjacent buildings, it would provide a natural appearance which would not be at odds with the park land setting. A detailed specification for the cladding would be secured by condition prior to its installation to ensure the layout, detail and finish is of a high quality. A glazed infill section is proposed to the upper part of the toilet block, providing natural light as well as an important visual separation between the structure and the roof. This acts to emphasise the freestanding nature of the structure.

It is recognised that the design of the building does not seek to mimic the Victorian architecture found locally. This would likely result in a building of greater bulk and mass by incorporation of pitched roof forms, greater extent of solid forms and resultantly greater landscape impact. No objection is held to this approach given the design is found to be coherent and of a high quality modern approach. The proposed design is reflective of the differing function and role of the building in relation to the public realm in comparison to adjacent buildings.

Details of proposed materials including the detailed finish of proposed steel work, glazing manifestations, timber cladding, woven steel mesh bin enclosure and rain water goods will be secured be condition to ensure these are of high quality.

Taking account of the preceding assessment, the proposed development is found to result in a negligible impact to the setting and historic significance of Towerhirst. The proposals would result in a very low degree of harm to the status of The Downs as a Local Historic Park and Garden. The proposed development would minimise conflict with important characteristics of The Downs Conservation Area and would result in a low degree of harm to the conservation area. The development would avoid harm to the setting of the adjacent Listed drinking fountain. The proposals involve restoration of this feature to working state as well as improved surfacing (individual paving stones) surrounding the fountain which will highlight this feature of historic significance within the public realm. This would enhance the setting of the heritage asset and offer public benefit in this regard. Further public benefits associated with the proposed development lie within the upgrade of public toilets, community benefit relating to educational use, a minimum of 3 full time jobs and more broad economic benefits both relating to construction of the building as well as the local economy through the café.

Officers have undertaken the assessment required under the Sections 66 and 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990 as well as that required by Section 16 of the NPPF and have given special regard to the requirement to preserve the setting and significance of heritage assets. There is a low degree of less than substantial harm caused as a result of the proposals, this has been given considerable importance and weight. However, overall, the development would preserve and in some cases enhance the status and significance of adjacent heritage assets. Sufficient public benefits are attributed to the proposed development which would outweigh the low level of less than substantial harm identified. The design of the proposed building is found to accord with the objectives of national and local planning policy and resultantly the development is acceptable in this regard.

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ECOLOGY & BIODIVERSITY

Paragraph 175 of the NPPF states: “When determining planning applications, local planning authorities should apply the following principles:

- a) if significant harm to biodiversity resulting from a development cannot be avoided (through locating on an alternative site with less harmful impacts), adequately mitigated, or, as a last resort, compensated for, then planning permission should be refused;
- b) development on land within or outside a Site of Special Scientific Interest, and which is likely to have an adverse effect on it (either individually or in combination with other developments), should not normally be permitted. The only exception is where the benefits of the development in the location proposed clearly outweigh both its likely impact on the features of the site that make it of special scientific interest, and any broader impacts on the national network of Sites of Special Scientific Interest;
- c) development resulting in the loss or deterioration of irreplaceable habitats (such as ancient woodland and ancient or veteran trees) should be refused, unless there are wholly exceptional reasons and a suitable compensation strategy exists; and
- d) development whose primary objective is to conserve or enhance biodiversity should be supported; while opportunities to incorporate biodiversity improvements in and around developments should be encouraged, especially where this can secure measurable net gains for biodiversity.

Paragraph 177 of the NPPF states: “The presumption in favour of sustainable development does not apply where the plan or project is likely to have a significant effect on a habitats site (either alone or in combination with other plans or projects), unless an appropriate assessment has concluded that the plan or project will not adversely affect the integrity of the habitats site”.

Policy BCS9 of the Core Strategy states: “The integrity and connectivity of the strategic green infrastructure network will be maintained, protected and enhanced. Opportunities to extend the coverage and connectivity of the existing strategic green infrastructure network should be taken.

Individual green assets should be retained wherever possible and integrated into new development. Loss of green infrastructure will only be acceptable where it is allowed for as part of an adopted Development Plan Document or is necessary, on balance, to achieve the policy aims of the Core Strategy. Appropriate mitigation of the lost green infrastructure assets will be required.

Development should incorporate new and/or enhanced green infrastructure of an appropriate type, standard and size. Where on-site provision of green infrastructure is not possible, contributions will be sought to make appropriate provision for green infrastructure off site”.

In relation to Biological Conservation the policy states: “Internationally important nature conservation sites are subject to statutory protection.

National and local sites of biological and geological conservation importance will be protected having regard to the hierarchy of designations and the potential for appropriate mitigation. The extent to which a development would contribute to the achievement of wider objectives of the Core Strategy will be carefully considered when assessing their impact on biological and geological conservation.

Where development would have an impact on the Bristol Wildlife Network it should ensure that the integrity of the network is maintained or strengthened”.

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Policy DM19 of the SADMP states: “Development which would be likely to have any impact upon habitat, species or features, which contribute to nature conservation in Bristol will be expected to:

- i. Be informed by an appropriate survey and assessment of impacts; and
- ii. Be designed and sited, in so far as practicably and viably possible, to avoid any harm to identified habitats, species and features of importance; and
- iii. Take opportunities to connect any identified on-site habitats, species or features to nearby corridors in the Wildlife Network.

Where loss of nature conservation value would arise development will be expected to provide mitigation on-site and where this is not possible provide mitigation off-site.

Development on or adjacent to sites of nature conservation value will be expected to enhance the site’s nature conservation value through the design and placement of any green infrastructure provided”.

In relation to Sites of Nature Conservation Importance (SNCI), the policy states: “Development which would have a harmful impact on the nature conservation value of a Site of Nature Conservation Interest will not be permitted”.

The site and the wider open space form part of a Sites of Nature Conservation Importance. The Avon Gorge which is situated 3-4m west of the site is designated a Site of Special Scientific Interest (SSSI) as well as Special Area of Conservation (SAC). The Local Authority Nature Conservation officer has therefore been consulted upon the potential impacts of the proposals with regards to biodiversity. The requirements of the Conservation of Habitats and Species Regulations 2017 (as amended) with regards to Habitats Regulations Assessment (HRA) have been reviewed and it has been established that whilst this proposal is located close to the Avon Gorge Woodlands SAC and SSSI, it is found that the proposed development will not cause a significant effect on the interest features of the SAC. This is due to the relatively small scale demolition and construction works proposed.

The development will however directly impact upon part of the Clifton and Durdham Downs SNCI. However, following review it is found that with best practice construction methods incorporating pollution prevention methods, including the minimisation of dust pollution, significant negative ecological impacts are not anticipated. The Nature Conservation officer has confirmed that conditions attached to any eventual consent would be sufficient to safeguard biodiversity. These would require checks for nesting birds prior to commencement of development, the provision of green roofs (which are included within proposals), details of glazing manifestations to prevent risk posed to birds through striking the glazed walls as well as an assessment of predicted light levels experienced surrounding the site. Subject to these measures, the development would avoid harmfully impacting upon habitat, species or features, which contribute to nature conservation in Bristol. The development is therefore found to accord with the objectives of national and local planning policy and resultantly the development is acceptable in this regard.

TREES & GREEN INFRASTRUCTURE

Section 15 (Conserving and enhancing the natural environment) of the NPPF states that planning policies and decisions should contribute to and enhance the natural and local environment by: “recognising the intrinsic character and beauty of the countryside, and the wider benefits from natural capital and ecosystem services – including the economic and other benefits of the best and most versatile agricultural land, and of trees and woodland”.

Paragraph 175 of the NPPF proceeds to outline that when determining planning applications, local planning authorities should apply the following principles: “development resulting in the loss or

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deterioration of irreplaceable habitats (such as ancient woodland and ancient or veteran trees) should be refused, unless there are wholly exceptional reasons and a suitable compensation strategy exists". Exception reasons exist in scenarios such as infrastructure projects (including nationally significant infrastructure projects, orders under the Transport and Works Act and hybrid bills), where the public benefit would clearly outweigh the loss or deterioration of habitat.

Policy BCS9 (Green Infrastructure) of the Bristol Cores Strategy outlines that the integrity and connectivity of the strategic green infrastructure network will be maintained, protected and enhanced. Opportunities to extend the coverage and connectivity of the existing strategic green infrastructure network should be taken. Individual green assets should be retained wherever possible and integrated into new development. Loss of green infrastructure will only be acceptable where it is allowed for as part of an adopted Development Plan Document or is necessary, on balance, to achieve the policy aims of the Core Strategy. Appropriate mitigation of the lost green infrastructure assets will be required. Development should incorporate new and/or enhanced green infrastructure of an appropriate type, standard and size. Where on-site provision of green infrastructure is not possible, contributions will be sought to make appropriate provision for green infrastructure off site.

Policy DM17 (Development Involving Existing Green Infrastructure) of the Site Allocations and Development Management Policies Local Plan (SADMP) outlines that development should integrate important existing trees. Development which would result in the loss of Ancient Woodland, Aged trees or Veteran trees will not be permitted. It is suggested that where tree loss or damage is essential to allow for appropriate development, replacement trees of an appropriate species should be provided in accordance with the standard set out within Policy DM17 known as the Bristol Tree Replacement Standard (BTRS).

In relation to green infrastructure provision, it is recognised that the development would result in development of some previously undeveloped open, grassland. The development has however incorporated a green, planted roof covering the majority of the building. This is proposed to be planted with species native to the adjacent Avon Gorge. It is noted that the existing building at the site does not offer such a feature. As such, whilst the development involves the loss of a degree of existing green infrastructure, this would be mitigated at roof level to offer an enhancement upon existing green infrastructure levels. Details of proposed planting will be secured by condition attached to any eventual consent to ensure planted materials are appropriate to the context and are maintained through to establishment.

The site is located adjacent to a row of trees that grow beyond the adjacent clifftop wall which runs to the west of the site. These are relatively small specimens, most likely due to the limited rooting environment created by the cliff face and adjacent wall, noted to be of low to moderate quality however offering some landscape value.

An arboricultural report including tree survey, impact assessment, method statement and protection plan has been submitted accompanying the application. This finds that the trees will require will require both above and below ground protection. Above ground, minor pruning works are recommended as well as installation of temporary ground protection during construction. This will be subject to a watching brief maintained by the Project Arboriculturalist. It is anticipated that the existing boundary wall will have prevented tree root penetration below ground towards the development site. These measures have been reviewed and are accepted by the Local Authority Arboricultural officer. It has also been specified that a post construction tree survey will be undertaken and submitted to the Local Authority, 1 year following substantial completion of the building. This will ensure any unlikely impact to trees is identified and can be mitigated following development. These details would be secured by condition attached to any eventual permission. Subject to these measures, the development would avoid harmfully impacting upon green infrastructure assets including trees. The development is therefore found to accord with the objectives of national and local planning policy and resultantly the development is acceptable in this regard.

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NEIGHBOURING AMENITY

Section 12 (Achieving well-designed places), paragraph 127 of the NPPF outlines that planning policies and decisions should ensure that developments create places with a high standard of amenity for existing and future users.

Policy BCS21 (Quality Urban Design) of the Core Strategy outlines that all new development within Bristol will be expected to strive to achieve high standards of urban design. With regards to amenity it is outlined, that new development is expected to safeguard the amenity of existing development.

Policy BCS23 (Pollution) of the Core Strategy outlines that development should be sited and designed in a way as to avoid adversely impacting upon environmental amenity or biodiversity of the surrounding area by reason of fumes, dust, noise, vibration, smell, light or other forms of air, land, water pollution, or creating exposure to contaminated land. Further to this, in locating and designing development, account should also be taken of the impact of existing sources of noise or other pollution on the new development and the impact of the new development on the viability of existing uses by reason of its sensitivity to noise or other pollution.

Policy DM35 (Noise Mitigation) of the SADMP outlines that development which would have an unacceptable impact on environmental amenity or biodiversity by reason of noise will be expected to provide an appropriate scheme of mitigation. In assessing such a scheme of mitigation, account will be taken of:

- i. The location, design and layout of the proposed development; and
- ii. Existing levels of background noise; and
- iii. Measures to reduce or contain generated noise; and
- iv. Hours of operation and servicing.

Development will not be permitted if mitigation cannot be provided to an appropriate standard with an acceptable design, particularly in proximity to sensitive existing uses or sites.

In relation to neighbouring residential properties, the proposed building would be in closest proximity of Towerhirst which is sited approximately 35m north of the site. Seawalls, a post war flatted development is located approximately 60m from the site, beyond Towerhirst. The next closest property is 27 Sea Walls Road, separated by some 90m of the site. Given the scale of the proposed building and the aforementioned separations, no harmful loss of light, overshadowing, impact to outlook or loss of privacy would result to any adjacent residential property.

In relation to noise and potential disturbance, it is noted that given the surrounding layout, the only property which would stand to experience any potential change in conditions would be Towerhirst. The garden of this property extends to within 15m of the site. There is however a substantial wall between the garden and the neighbouring property. It is also understood that the garden of this property is tiered with other parts located at lower level further from the site. This neighbouring site shares its boundary with the public open space and therefore some level of noise and disturbance resulting from this public/private relationship is inevitable. In closest proximity of the neighbouring property would be the educational booth and public toilets. The public toilets would be sited similarly to the existing facilities and it is unlikely these would cause any more noise or disturbance than the present facilities. High levels of noise would also not be anticipated to be associated with the educational booth given the proposed use and scale of this facility.

Some noise and activity would be generated by patrons relating to the proposed café and its outdoor seating area. This facility would be sited at the southern end of the proposed building, thus furthest from the neighbouring property. Under normal use this would only relate to noise from conversation

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and general human activity which could be commonly expected at any point of a public open space.

The opening hours for the café are proposed to be restricted to Monday to Saturday 08.00 to 19.00 and 10.00 to 16.00 on Sundays. These are found to be reasonable to safeguard against noise creating activity at quieter times of day. Restrictions also secured via conditions will also be applied to hours of collection for waste to prevent noise relating to this servicing at quieter times of day. It is noted that no plant or extraction equipment is proposed for the café and therefore noise emissions relating to such equipment would not be an issue. A condition has been recommended by the Pollution Control officer to prevent installation of such equipment without the prior approval of the Local Authority. Further conditions are also suggested to ensure that any noise generated by plant or equipment which is installed at any point shall be at least 5 dB below the background level at any time at residential premises.

Information submitted with the planning application with regard to how the café, toilets and educational booth will be managed by the occupiers is basic in nature at this point as an operator for the café has not been identified. As such, a condition securing a detailed site management plan will be attached to any eventual consent. This will set out details of various operations of the café, public toilets and education booth, including waste management, security, servicing and deliveries, communications and complaints procedures, out of hours procedures, health and safety and cleaning and maintenance.

Subject to these measures, the proposed development is not found to cause undue harm to the amenity, living conditions and enjoyment of adjacent residential properties. The development is therefore found to accord with the objectives of national and local planning policy and resultantly the development is acceptable in this regard.

TRANSPORT & HIGHWAYS

Section 9 (Promoting Sustainable Transport) of the NPPF outlines that significant development should be focused on locations which are or can be made sustainable, through limiting the need to travel and offering a genuine choice of transport modes. This can help to reduce congestion and emissions, and improve air quality and public health. However, opportunities to maximise sustainable transport solutions will vary between urban and rural areas, and this should be taken into account in both plan-making and decision-making.

In relation to sustainable transport, the NPPF states that development should:

- a) give priority first to pedestrian and cycle movements, both within the scheme and with neighbouring areas; and second – so far as possible – to facilitating access to high quality public transport, with layouts that maximise the catchment area for bus or other public transport services, and appropriate facilities that encourage public transport use;
- b) address the needs of people with disabilities and reduced mobility in relation to all modes of transport;
- c) create places that are safe, secure and attractive – which minimise the scope for conflicts between pedestrians, cyclists and vehicles, avoid unnecessary street clutter, and respond to local character and design standards;
- d) allow for the efficient delivery of goods, and access by service and emergency vehicles; and
- e) be designed to enable charging of plug-in and other ultra-low emission vehicles in safe, accessible and convenient locations.

Policy BCS10 (Transport & Access Improvements) of the Core Strategy states that development

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proposals should be located where sustainable travel patterns can be achieved, with more intensive, higher density mixed use development at accessible centres and along or close to main public transport routes. Proposals should minimise the need to travel, especially by private car, and maximise opportunities for the use of walking, cycling and public transport.

Developments should be designed and located to ensure the provision of safe streets and reduce as far as possible the negative impacts of vehicles such as excessive volumes, fumes and noise. Proposals should create places and streets where traffic and other activities are integrated and where buildings, spaces and the needs of people shape the area.

The following hierarchy for transport user priorities is set out:

- a) The pedestrian;
- b) The cyclist;
- c) Public transport;
- d) Access for commercial vehicles;
- e) Short stay visitors by car;
- f) The private car.

Policy DM23 (Transport Development Management) of the SADMP outlines that new development should not give rise to unacceptable traffic conditions and will be expected to provide safe access to the highway network. The policy also outlines that new development should be accessible by sustainable transport methods such as walking, cycling and public transport. Furthermore, the policy sets standards for parking provision. These can be found at Appendix 2 of the SADMP document. The parking standards are maximum levels for car parking and minimum levels for cycle parking.

Policy DM32 (Recycling & Refuse Provision in New Development) of the SADMP outlines that all new development should provide bin and recycling storage facilities fit for the nature of development, with adequate capacity for the proposed development, in a location which is safe and accessible for all users and does not harm the visual amenity of the area or neighbouring amenity.

The site is located at the far north western corner of The Downs open space which the proposed uses are designed to serve. By virtue of the lack of development bordering the site to the south, east and west, the site is recognisably not best served by public transport. There is however bus stops located at Rockleaze and Downleaze between 600-800m or 8-10 minutes' walk (dependent on inbound or outbound) east of the site. These are presently served by the route 4 service running between the city centre and Cribbs Causeway via Shirehampton. This would provide a reasonable means of public transport access to the site.

Given the position within a public open space where people are likely to attend for sports and recreational purposes, the distances users are likely willing to walk or cycle to reach their destination are likely greater. As such, it is not foreseen that the scale of distances involved to reach the site would discourage walking or cycling as in other contexts. As such, the development can be considered to promote such active and sustainable means of transit. Within this context, the 1.3km walk from other bus stops at the top of Whiteladies Road or even from Clifton Down Railway Station (2.1km) would not be unreasonable for some. It is consequently concluded that the site would be accessible by public transport and sustainable means.

It is noted that the parking standards do not require the provision of any car parking or cycle parking within cafes under 250m². It is recognised some users will inevitably access the site via cars however there is found to be sufficient parking on Circular Road that this would not cause a significant highways issue. Any illegal parking which is presently an issue on The Downs and may also be attributed to the proposed development can be dealt with through other highways legislation.

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There is existing publicly accessible visitor cycle parking in the form of Sheffield stands available at the Sea Walls viewing point. A staff cycle parking stand is proposed for x2 bikes within the development.

As highlighted above, the implications of the Equalities Act 2010 have been considered both procedurally and in relation to the proposed development. The development would include level access from the surrounding public realm and would be required to accord with all necessary aspects of Part M of Building Regulations. It is noted that there is an existing on street disabled parking bay adjacent to the site on Circular Road at Sea Walls. This is deemed to accord with policy requirements in relation to equal access.

In relation to waste storage and management, a bin store is proposed to the rear of the building. This would be sited to have the lowest possible public perception. The bin store is deemed to be of sufficient scale to accommodate bins required to serve the development. It is outlined that staff will transport the bins to Circular Road for collection. This will take place every second day. To ensure full management of the waste collection process, a detailed waste management strategy will be required prior to commencement of use of the café. This would be secured via condition in the event of permission being granted. Subject to this measure, the proposals would accommodate acceptable facilities for storage of refuse and recycling.

Subject to conditions, the proposed development is therefore found compatible with national and local policy objectives for sustainable development through encouraging active and low carbon travel. The development would also avoid any detrimental impact upon the safe and free flow of the surrounding highway network. On this basis the proposed development is therefore acceptable in terms of transport and access.

SITE MANAGEMENT & CRIME PREVENTION

Policy DM10 (Food and Drink Uses and the Evening Economy) of the SADMP states that food and drink uses will be acceptable provided that they would not harm the character of the area, residential amenity and/or public safety, either individually or cumulatively; and that proposals resulting in a harmful concentration will not be permitted.

Policy DM28 (Public Realm) of the SADMP states that: “development will be expected to “reduce crime and fear of crime by creating a well-surveilled public realm that is well managed and cared for”.

As concluded within the preceding sections, the proposed café is not found to harm the character of the area or harm residential amenity. A harmful concentration of such uses would also not be created in this instance. There are however some implications for public safety and crime prevention. Due to the isolated nature of the site and public accessibility of the open space, this area is understood to attract crime. It is noted that the proposed café would offer benefits over the existing public toilet block in terms of increased public surveillance over this part of the open space.

Avon and Somerset Police have been consulted on the development and the local policing team responsible for patrol of the area have raised no specific concerns. The Crime Prevention officer has however recommended that further details of access control, CCTV, lighting and cash handling are submitted. The proposals do include an extensive CCTV system as shown on plans. Further details of security aspects will be sought via condition prior to first use of the building. This will include details of general site management plan relating to management of the café, public toilets and educational booth. This will include details of public waste bins (which are shown on plans) and how these will be managed to prevent an increase in littering relating to the development. Subject to this measure, the development would not be directly associated with increased crime or detriment to public safety.

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Themes of sustainability, carbon reduction and climate change underpin national planning policy. Policies BCS13-15 of the Core Strategy relate to the Council's expectations with regard to sustainable construction of new buildings and emissions in respect of climate change. These policies must be addressed and the guidance within the Council's Climate Change and Sustainability Practice Note followed. New dwellings are expected to minimise energy requirements. This will be achieved by high standards of energy efficiency including optimal levels of thermal insulation, passive ventilation and cooling, passive solar design, and the efficient use of natural resources in new buildings. Core Strategy Policy requires new dwellings are also incorporate an element of renewable energy to reduce carbon emissions by a further 20% above energy saving measures.

The applicant has supplied a sustainability statement in support of the proposed development. This outlines a range of measures the development would take to achieve high levels of energy efficiency and reduce energy consumption. These include specification of thermal elements above and beyond the building regulations requirements. In turn, this will decrease the overall heating demand of the development. Due to the location of the site the heating fuel will be electricity. Incorporating an air source heat pump and a heat recovery ventilation system will reduce the electricity demand, and therefore the CO₂ emissions. The building has been designed with a large overhanging roof to create shading. This design feature mitigates the risk of overheating which would otherwise be likely in the summer months due to excess solar gain.

The sustainability statement includes an energy table which indicates these measures would make a 10% reduction in carbon emissions beyond Building Regulations Part L levels. The energy table also indicates a further 29% reduction in carbon emissions (beyond the improved Part L level) can be achieved via on site renewable energy generation, specifically via installation of photovoltaic panels. These are proposed for installation to the flat roof and will be installed flat. These are shown on the proposed drawings. Further detail would be sought via condition regarding the exact positions and specifications proposed prior to the installation of this element. A condition would also be applied to any eventual permission ensuring full compliance with the supplied a sustainability statement and energy statement. Subject to this, the proposed development would therefore make an acceptable contribution to policy objectives of sustainability and climate change.

DRAINAGE & FLOOD RISK

Policy BCS16 (Flood Risk and Water Management) of the Core Strategy states that development in Bristol will follow a sequential approach to flood risk management, giving priority to the development of sites with the lowest risk of flooding. The development of sites with a sequentially greater risk of flooding will be considered where essential for regeneration or where necessary to meet the development requirements of the city.

Development in areas at risk of flooding will be expected to:

- i. be resilient to flooding through design and layout, and/or
- ii. incorporate sensitively designed mitigation measures, which may take the form of on-site flood defence works and/or a contribution towards or a commitment to undertake such off-site measures as may be necessary, in order to ensure that the development remains safe from flooding over its lifetime.

All development will also be expected to incorporate water management measures to reduce surface water run-off and ensure that it does not increase flood risks elsewhere. This should include the use of sustainable drainage systems (SUDS).

Policy DM33 (Pollution Control, Air Quality and Water Quality) of the SADMP states that Development

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which has the potential, either individually or cumulatively, for an unacceptable impact on environmental amenity, biodiversity or water quality by reason of pollution as set out in the Core Strategy but is considered desirable for reasons of economic or wider social need will be expected to provide an appropriate scheme of mitigation.

The policy proceeds to outline that “Development adjacent to underground or surface water bodies covered by the Water Framework Directive and Severn River Basin Management Plan should contribute towards those water bodies maintaining or achieving Good Ecological Status. This may take the form of on-site measures or a financial contribution to off-site measures.”

“In terms of water quality, the River Frome, Brislington Brook, Malago, River Trym and Colliter’s Brook do not currently achieve Good Ecological Status due to impacts from flood protection / land drainage schemes and urbanisation. To comply with the Water Framework Directive water bodies should reach good ecological potential by 2027. Measures will therefore be sought from development adjacent to waterways covered by the Water Framework Directive, where feasible and viable, either through measures in the Severn River Basin Management Plan or other good practice such as naturalised river habitats, deculverting and appropriate vegetation management plans. The River Avon is at good ecological status and this should not be allowed to deteriorate through development.”

The site is not located within a flood zone and is not at risk of flooding. The proposed development is located within surface drainage discharge zone where the priority is to use infiltration techniques where possible, or reduce the discharge rate and provide water quality improvements. The proposed development of undeveloped green land would likely result in increased discharge rates. It is noted that the development includes proposals for a large green roof which would mitigate some of the effects in terms of increased discharge rates. Wessex Water has confirmed proposals to discharge surface water via the main sewer as acceptable. Measures to reduce surface water run of into the existing combined sewer, to improve water quality and reduce flood risk are however encouraged. The applicant is referred to local flood risk and drainage standing advice. The application does not detail proposals to meet these objectives. As such, a condition will be applied to any eventual consent requiring a detailed sustainable urban drainage strategy prior to commencement. Subject to this measure, the development would avoid causing any significant increase in flood risk locally.

COMMUNITY INFRASTRUCTURE LEVY

Some new developments granted planning permission will be liable to pay Community Infrastructure Levy (CIL) to Bristol City Council.

CIL is payable where development comprises 100m² or more of new build floorspace or results in the creation of one or more dwellings.

The proposed development does not qualify to pay CIL.

CONCLUSION

The proposed development has been assessed against all relevant national and local planning policy as well as relevant adopted supplementary guidance. The proposed development of open space is found to be acceptable in this instance on the basis the proposed use would be for sports and recreational purposes and ancillary to the designation as Important Open Space. The development is found to preserve and in some cases enhance the status and significance of adjacent heritage assets. The design of the building presents an acceptable response to the context. Following assessment and subject to safeguarding conditions, it has been confirmed that the proposed development would avoid detrimental impact to ecology, biodiversity, trees, green infrastructure, neighbouring amenity and living conditions, surrounding highways, public safety, climate change and flood risk. The development is therefore found to be in accordance with relevant national and local planning policy.

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There are no material considerations which would warrant the refusal of planning permission. It is therefore recommended permission is granted subject to conditions.

RECOMMENDED GRANT subject to condition(s)**Time limit for commencement of development**

1. Full Planning Permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

Pre commencement condition(s)

2. Construction management plan

No development shall take place including any works of demolition (with the exception of erection of the single storey environmental classroom and enabling works) until a construction management plan or construction method statement has been submitted to and been approved in writing by the Local Planning Authority. The approved plan/statement shall be adhered to throughout the construction period. The statement shall provide for:

- o Parking of vehicles of site operatives and visitors
- o Routes for construction traffic
- o Method of preventing mud being carried onto the highway
- o Pedestrian and cyclist protection
- o Proposed temporary traffic arrangements including hoardings and/or footway closures
- o Arrangements for turning vehicles
- o Arrangements to receive abnormal loads or unusually large vehicles
- o How the delivery of construction materials and the collection of waste will be managed
- o Where construction materials and waste will be stored
- o Methods of communicating the Construction Management Plan to staff, visitors and neighbouring residents and businesses

Reason: In the interests of safe operation of the highway in the lead into development both during the demolition and construction phase of the development.

3. Site Specific Construction Environmental Management Plan

No development shall take place including any works of demolition (with the exception of erection of the single storey environmental classroom and enabling works) until a site specific Construction Environmental Management Plan has been submitted to and been approved in writing by the Council. The plan must demonstrate the adoption and use of the best practicable means to reduce the effects of noise, vibration, dust and site lighting on the surrounding area and all surrounding premises and infrastructure.

Reason: In the interests of the amenities of surrounding occupiers during the construction of the development.

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4. Vegetation Clearance

No clearance of vegetation or structures suitable for nesting birds, shall take place between 1st March and 30th September inclusive in any year without the prior written approval of the Local Planning Authority. The authority will require evidence provided by a suitably qualified ecologist that no breeding birds would be adversely affected including by disturbance before giving any approval under this condition. Where checks for nesting birds by a qualified ecological consultant are required they shall be undertaken no more than 48 hours prior to the removal of vegetation or the demolition of, or works to buildings.

Reason: To ensure that wild birds, building or using their nests are protected.

5. Sustainable Drainage System (SuDS)

The development hereby approved shall not commence until a Sustainable Drainage Strategy and associated detailed design, management and maintenance plan of surface water drainage for the site using SuDS methods has been submitted to and approved in writing by the Local Planning Authority. This shall demonstrate use of infiltration techniques where possible, or reduce the discharge rate and provide water quality improvements. The approved drainage system shall be implemented in accordance with the approved Sustainable Drainage Strategy prior to the use of the building commencing and maintained thereafter for the lifetime of the development.

Reason: To prevent the increased risk of flooding by ensuring the provision of a satisfactory means of surface water disposal is incorporated into the design and the build and that the principles of sustainable drainage are incorporated into this proposal and maintained for the lifetime of the proposal.

6. Artificial Light

Prior to the commencement of development a detailed lighting scheme and predicted light levels by a suitably qualified Lighting Engineer shall be submitted and approved in writing by the Local Planning Authority. The report should include details of all internal and external lighting (including any decorative lighting and security lighting within external amenity/access areas) and associated light spill plans unless otherwise agreed in writing by the Local Planning Authority.

Artificial lighting to the development must conform to requirements to meet the Obtrusive Light Limitations for Exterior Lighting Installations for Environmental Zone - E2 contained within Table 1 of the Institute of Light Engineers Guidance Notes for the Reduction of Obtrusive Lighting, GN01, dated 2005.

Reason: In order to safeguard the amenities of adjoining residential occupiers, the appearance and character of the area as well as local ecology.

7. Further details: Drinking Fountain Restoration

Prior to commencement of development a detailed method statement and phasing strategy of the proposed restoration works for the historic drinking fountain, including large scale details and samples (where necessary) as well as details of the future management and maintenance of the fountain shall be submitted to and approved in writing by the Local Planning Authority. The restoration works shall be completed in accordance with the agreed strategy, prior to first

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use of the building hereby approved unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the proposed works do not result in harm to the historic fabric of the Listed drinking fountain and deliver the public benefit associated with the development.

8. Detailed External Area Landscape Scheme

Notwithstanding notations on any approved plans or documents, a full detailed design of hard landscaping and structures within the external areas and access paths to the development hereby approved shall be submitted to and approved by the Local Planning Authority prior to the commencement of development. This scheme shall include large scale details of the approved paved area surrounding the drinking fountain with proposed levels in relation to the existing footway, details of the proposed access ramp including levels and details of the proposed decking including levels. The scheme shall also provide specifications and where necessary samples of all proposed surfacing materials. The development shall be completed in accordance with the approved scheme and implemented prior to the first occupation of the building(s) hereby approved and maintained as such in perpetuity.

Reason: To deliver a high quality and inclusive external space and to make the most efficient use of the public space and to ensure acceptable means of access to the building from the existing public realm and protect and enhance the character of the site and the area and to ensure its appearance is satisfactory.

9. Further details: Materials

Notwithstanding any materials noted on any approved plans, sample panels and/or detailed specifications (as appropriate) of all proposed external finishing materials (including exposed steelwork, timber cladding, timber doors, glazing specifications including manifestation details, steel mesh, timber decking and rainwater goods) are to be submitted and approved in writing by the Local Planning Authority before the relevant parts of the work are commenced or items installed.

The development shall be completed in accordance with the approved details before commencement of use of the building, unless otherwise agreed in writing by the Local Planning Authority.

Reason: In order that the external appearance of the building is of a high quality and would be appropriate to the setting.

10. Further details: Education Booth Blinds

Prior to installation of the external blinds proposed for installation within the roof fabric surrounding the educational booth, detailed specifications demonstrating proposed appearance, material, colour and method of fixing at ground level shall be submitted to and approved in writing by the Local Planning Authority before the relevant parts of the work are commenced. The development shall then be completed in accordance with the approved details prior to the first commencement of the use hereby approved and maintained thereafter for the lifetime of the development as such unless otherwise agreed in writing by the Local Planning Authority.

Reason: In order that the external appearance of the building is of a high quality and would be appropriate to the setting.

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11. Further details: Green Roof Specification

Prior to installation of the green roof hereby approved, a detailed planting specification comprising locally appropriate species as well as a maintenance schedule through to establishment shall be submitted to and approved in writing by the Local Planning Authority. The development shall then be completed in accordance with the approved details prior to the first commencement of the use hereby approved and maintained thereafter for the lifetime of the development unless otherwise agreed in writing by the Local Planning Authority. If the roof planting fails, it shall be replaced in full to a specification as agreed in writing by the Local Planning Authority and maintained to establishment.

Reason: To ensure the proposed planting is compatible with local biodiversity, will support local ecosystems and provide a satisfactory appearance appropriate to the surroundings.

Pre occupation condition(s)

12. Site Management Plan

Prior to first use of the café, toilets and educational facility hereby approved, a detailed site management plan shall be submitted to and approved in writing by the Local Planning Authority. This shall include detailed management proposals for each use including proposed operating arrangements for each use including waste management, security, servicing and deliveries, communications and complaints procedures, out of hours operational details, management of all external outdoor areas, health and safety information and cleaning and maintenance proposals. The uses shall then operate in direct accordance with the details indicated within the site management plan in perpetuity, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the development protects the amenity of the area, neighbouring residents and public safety.

13. Waste Management Plan

Prior to first use of the building hereby approved, a detailed waste management strategy shall be submitted to and approved in writing by the Local Planning Authority. This shall outline detailed proposals for the management of waste relating to the uses hereby approved including:

- o Management of waste/bins at the site
- o Details of recycling provisions for the site
- o Details of manoeuvring of receptacles to and from the site
- o Detailed collection proposals for waste from the site

The development shall then operate in accordance with the approved waste management strategy thereafter unless otherwise agreed in writing by the Local Planning Authority.

Reason: To safeguard the amenity of the area including that of neighbouring occupiers as well as manage highways impacts relating to waste management.

14. Site Security Strategy

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Prior to first use of the building hereby approved, a detailed security strategy for the site shall be submitted to and approved in writing by the Local Planning Authority. This shall outline detailed security measures for the site hereby approved including:

- o Access control for the building
- o CCTV proposals
- o Lighting proposals
- o Out of hour's security strategy
- o Details relating to management of cash

The development shall then operate in accordance with the approved security strategy in perpetuity unless otherwise agreed in writing by the Local Planning Authority.

Reason: To safeguard public safety and minimise the likelihood of crime.

15. No Installation of Plant and Equipment

No equipment for the extraction and dispersal of cooking smells/fumes shall be installed until details including detailed plans, method of construction, any odour control measures and predicted noise levels have been submitted to and been approved in writing by the Local Planning Authority. Such equipment shall then be installed directly in accordance with the approved details prior to the first commencement of the use hereby approved and permanently retained thereafter unless otherwise agreed in writing by the Local Planning Authority.

Reason: To safeguard the amenity of the local area including appearance, character and neighbouring living conditions as well as to prevent airborne odours

16. Renewable Energy Generation (Solar Roofing Membrane)

The renewable energy generation technology hereby approved (solar roofing membrane - Alwitra Evalon Solar) shall be installed and fully operational prior to first use of the building hereby approved and maintained in perpetuity.

Reason: To ensure that the development would meet sustainability and climate change policy objectives.

17. Refuse and Recycling

No building or use hereby permitted shall be occupied or the use commenced until the refuse store, and area/facilities allocated for storing of recyclable materials, as shown on the approved plans have been completed in accordance with the approved plans. Thereafter, all refuse and recyclable materials associated with the development shall either be stored within the dedicated store/area, as shown on the approved plans, or internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the public highway or pavement, except on the day of collection.

Reason: To safeguard the amenity of the occupiers of adjoining premises, protect the general environment, and prevent obstruction to pedestrian movement, and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

18. Cycle Parking

No building or use hereby permitted shall be occupied or the use commenced until the staff cycle parking provision shown on the approved plans has been completed, and thereafter, be

Development Control Committee B – 13 March 2019**Application No. 18/04727/F : Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ**

kept free of obstruction and available for the parking of cycles only.

Reason: To ensure the provision and availability of adequate cycle parking.

19. Completion of Pedestrians/Cyclists Access - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the means of access for pedestrians and/or cyclists have been constructed in accordance with the approved plans and shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety

Post occupation management

20. Opening Hours

All patrons, customers and members of the public shall vacate the building and external seating areas outside the hours of 08:00 to 19:00 Monday to Saturday and 10:00 to 16:00 on Sundays and Bank Holidays.

Reason: To safeguard the amenity of neighbouring occupiers.

21. Seating area extent

The seating area hereby permitted shall only take place on the area and in strict accordance with the extent set out on the approved plan ?. All tables, chairs and associated furniture and receptacles will furthermore be removed from the area and stored internally outside of the stated opening hours.

Reason: To ensure that pedestrian and bicycle movement would not be obstructed and to safeguard safety and security as well as residential and visual amenity

22. Hours of Waste Collections

Activities relating to the collection of refuse and recyclables and the tipping of empty bottles into external receptacles shall only take place between 08:00 and 19:00 Monday to Saturday and not at all on Sundays or Bank Holidays.

Reason: To safeguard the amenity of neighbouring occupiers

23. Deliveries

Activities relating to deliveries shall only take place between 08.00am and 19:00pm Monday to Saturday and not at all on Sundays or Bank Holidays.

Reason: In order to safeguard the amenities of future and adjoining occupiers.

24. Restriction of noise from plant and equipment

The rating level of any noise generated by plant and equipment as part of the development shall be at least 5 dB below the background level as determined by BS4142: 2014 Methods for rating and assessing industrial and commercial sound.

Development Control Committee B – 13 March 2019**Application No. 18/04727/F : Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ**

Reason: To safeguard the amenity of nearby premises and the area generally.

25. In accordance with Arboricultural Report

The proposed development shall be implemented in accordance with all findings and recommendations (including arboricultural watching brief) as outlined within the supplied Arboricultural Impact Assessment (Arboricultural Impact Assessment, Arboricultural Method Statement and Tree Protection Plan, Hillside Trees Ltd, February 2019). The project arboricultural consultant must be present to oversee works within root protection areas in accordance with the arboricultural supervision recommendations in the arboricultural method statement. Copies of written site notes and/or reports detailing the results of arboricultural site supervision shall be submitted to the Local Planning Authority following the operations detailed within the arboricultural method statement. In accordance with the Tree Protection Plan, a BS5837:2012 tree survey shall be undertaken 1 year following substantial completion of the building.

Reason: To protect green infrastructure and the character and appearance of the area in line with Policy BCS9 of the Core Strategy and Policy DM17 of the Site Allocations & Development Management Policies Local Plan.

26. In accordance with Sustainability and Energy Statement

The development hereby approved shall incorporate all energy efficiency measures, renewable energy, sustainable design principles and climate change adaptation measures into the design and construction of the development in full accordance with the energy and sustainability statements (Sustainability Statement and Energy Strategy (Iteration 2), Chris Goodsall Architect, 26th February 2019) prior to first occupation. A total 10% reduction in carbon dioxide emissions beyond Part L 2013 Building Regulations in line with the energy hierarchy shall be achieved through improved building fabric, and a 29% reduction in carbon dioxide emissions below residual emissions through renewable technologies (solar roofing membrane) shall be achieved.

Reason: To ensure the development incorporates measures to minimise the effects of, and can adapt to a changing climate in accordance with Policies BCS13 (Climate Change), BCS14 (Sustainable Energy), BCS15 (Sustainable Design and Construction) and DM29 (Design of New Buildings).

27. Restriction on Use of Roof

The flat roof of the building hereby approved shall not be used as a roof terrace, roof garden, balcony or for any other form of amenity area without further specific consent by the Local Planning Authority.

Reason: To safeguard to the special characteristics of the surrounding conservation area as well as neighbouring amenity.

Development Control Committee B – 13 March 2019**Application No. 18/04727/F : Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ****List of approved plans**

28. List of approved plans and drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

DC2-ED01 B Location and block plan, received 31 October 2018
DC2-ED02 Site analysis, received 31 October 2018
DC2-ED10 B Site plan as existing, received 31 October 2018
DC2-ED70 A Existing wc's - plans and elevations, received 31 October 2018
DC2-PD11 C Proposed site plan (no map), received 6 February 2019
DC2-PD80 A Proposed visualisations, received 6 February 2019
DC2-PD100 B Proposed volumetric model, received 18 February 2019
DC2-PD61 B Proposed section A-A, received 18 February 2019
DC2-PD62 Proposed section B-B (retractable blinds), received 18 February 2019
DC2-PD10 D Site plan as proposed (with map), received 6 February 2019
DC2-PD20 F Proposed plans, received 18 February 2019
DC2-PD70 F Proposed elevations, received 18 February 2019
Sustainability statement and energy strategy (Iteration 2), received 26 February 2019
Arboricultural Report (inc. Impact Assessment, Method Statement & Tree Protection Plan) (REV A), received 26 February 2019
Design and access statement and heritage statement, received 31 October 2018
Method statement for ecological protection, received 31 October 2018
Waste management strategy, received 18 February 2019

Reason: For the avoidance of doubt.

Development Control Committee B – 13 March 2019**Application No. 18/04727/F : Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ****Advices**

- 1 Nesting birds: Anyone who takes, damages or destroys the nest of any wild bird whilst that nest is in use or being built is guilty of an offence under the Wildlife and Countryside Act 1981 and prior to commencing work you should ensure that no nesting birds will be affected.
- 2 Construction site noise: Due to the proximity of existing noise sensitive development and the potential for disturbance arising from contractors' operations, the developers' attention is drawn to Section 60 and 61 of the Control of Pollution Act 1974, to BS 5528: Parts 1 and 2: 2009 Noise and Vibration Control on Construction and Open Sites code of practice for basic information and procedures for noise and vibration control" and the code of practice adopted by Bristol City Council with regard to "Construction Noise Control". Information in this respect can be obtained from Pollution Control, City Hall, Bristol City Council, PO Box 3176, Bristol BS3 9FS.
- 3 Application for listed building consent potentially needed: This permission does not act as listed building consent and you are accordingly advised of the potential need to submit a separate listed building consent application in respect of the works shown on the drawings hereby approved (restoration of the drinking fountain).
- 4 BS Standard - tree work: Any works should be completed in accordance with British Standard 3998: Recommendations for tree work, you are advised that the work should be undertaken by a competent and suitably qualified tree contractor.
- 5 Tree Protection: You are advised to refer to BS5837 : 2012 Trees in relation to construction for detailed information on types of tree protection, protection zones and other relevant matters.
- 6 Bats and bat roosts: Anyone who kills, injures or disturbs bats, obstructs access to bat roosts or damages or disturbs bat roosts, even when unoccupied by bats, is guilty of an offence under the Wildlife and Countryside Act 1981, the Countryside and Rights of Way Act 2000 and the Conservation (Natural Habitats, &c.) Regulations Act. Prior to commencing work you should ensure that no bats or bat roosts would be affected. If it is suspected that a bat or bat roost is likely to be affected by the proposed works, you should consult English Nature (Taunton office 01823 283211).
- 7 Wessex Water requirements: It will be necessary to comply with Wessex Water's main drainage requirements and advice and further information can be obtained from <http://www.wessexwater.co.uk>.
- 8 Are existing public sewers affected by the proposals?

According to our records the proposed site will be over a 3400mm tunnel sewer. Whilst this sewer is approximately 10 metres deep, the applicant will need to submit details to our Buildover Team so that we can assess what is required when building over this critical asset.

In the circumstances it is recommended that further details are submitted to the LPA prior to planning decision showing details of the proposed foundations including depth and type. Wessex Water can then assess the feasibility of building over this critical sewer without disruption of normal services. 9 Network Rail

Network Rail has no objection in principle to the above proposal but due to the proposal being next to Network Rail land and our infrastructure and to ensure that no part of the development adversely impacts the safety, operation and integrity of the operational railway we have included asset protection comments which the applicant is strongly recommended to action

Development Control Committee B – 13 March 2019**Application No. 18/04727/F : Public Conveniences Circular Road Sneyd Park Bristol BS9 1ZZ**

should the proposal be granted planning permission. The local authority should include these requirements as planning conditions if these matters have not been addressed in the supporting documentation submitted with this application.

This structure is within the zone of influence of Clifton Down Tunnel, as the application does not contain any foundation plans and any foundation would most likely require piling into rock, therefore, our Structures Engineers require foundation plans which will need to be approved by Network Rail's AssetProtectionWestern@NetworkRail.co.uk

TUNNELS

Network Rail's Engineer is to approve details of any development works within 15m, measure horizontally, from the outside face of the tunnel extrados with special reference to:

The type and method of construction of foundations

Any increase/decrease of loading on the tunnel both temporary and permanent.

Certified proof that the proposals shall have no detrimental effect upon the tunnel will be necessary.

Any proposal must not interfere with Network Rail's operational railway not jeopardise the structural integrity of the tunnel. The above details should be submitted to the Council and only approved in conjunction with Network Rail.

Network Rail will not accept any liability for any settlement, disturbance or damage caused to any development by failure of the tunnel structures nor for any noise or vibration arising from the normal use and/or maintenance of the tunnel. No right of support is given or can be claimed from Network Rails tunnels or railway land.

10 Network Rail

SAFETY

Any works on this land will need to be undertaken following engagement with Asset Protection to determine the interface with Network Rail assets, buried or otherwise and by entering into a Basis Asset Protection Agreement, if required, with a minimum of 3months notice before works start. assetprotectionwestern@networkrail.co.uk

PILING

Where vibro-compaction/displacement piling plant is to be used in development, details of the use of such machinery and a method statement should be submitted for the approval of Network Rail's Asset Protection Engineer prior to the commencement of works and the works shall only be carried out in accordance with the approved method statement.

EXCAVATIONS/EARTHWORKS

All excavations / earthworks carried out in the vicinity of Network Rail's property / structures must be designed and executed such that no interference with the integrity of that property / structure can occur. If temporary compounds are to be located adjacent to the operational railway, these should be included in a method statement for approval by Network Rail. Prior to commencement of works, full details of excavations and earthworks to be carried out near the railway undertaker's boundary fence should be submitted for approval of the Local Planning Authority acting in consultation with the railway undertaker and the works shall only be carried out in accordance with the approved details.

Where development may affect the railway, consultation with the Asset Protection Engineer should be undertaken.

DRAINAGE

Soakaways / attenuation ponds / septic tanks etc, as a means of storm/surface water disposal must not be constructed near/within 5 metres of Network Rail's boundary or at any point which could adversely affect the stability of Network Rail's property/infrastructure. Storm/surface water must not be discharged onto Network Rail's property or into Network Rail's culverts or drains. Network Rail's drainage system(s) are not to be compromised by any work(s). Suitable drainage or other works must be provided and maintained by the Developer to prevent surface water flows or run-off onto Network Rail's property / infrastructure.

Proper provision must be made to accept and continue drainage discharging from Network Rail's property. (The Land Drainage Act) is to be complied with. Suitable foul drainage must be provided separate from Network Rail's existing drainage. Once water enters a pipe it becomes a controlled source and as such no water should be discharged in the direction of the railway.

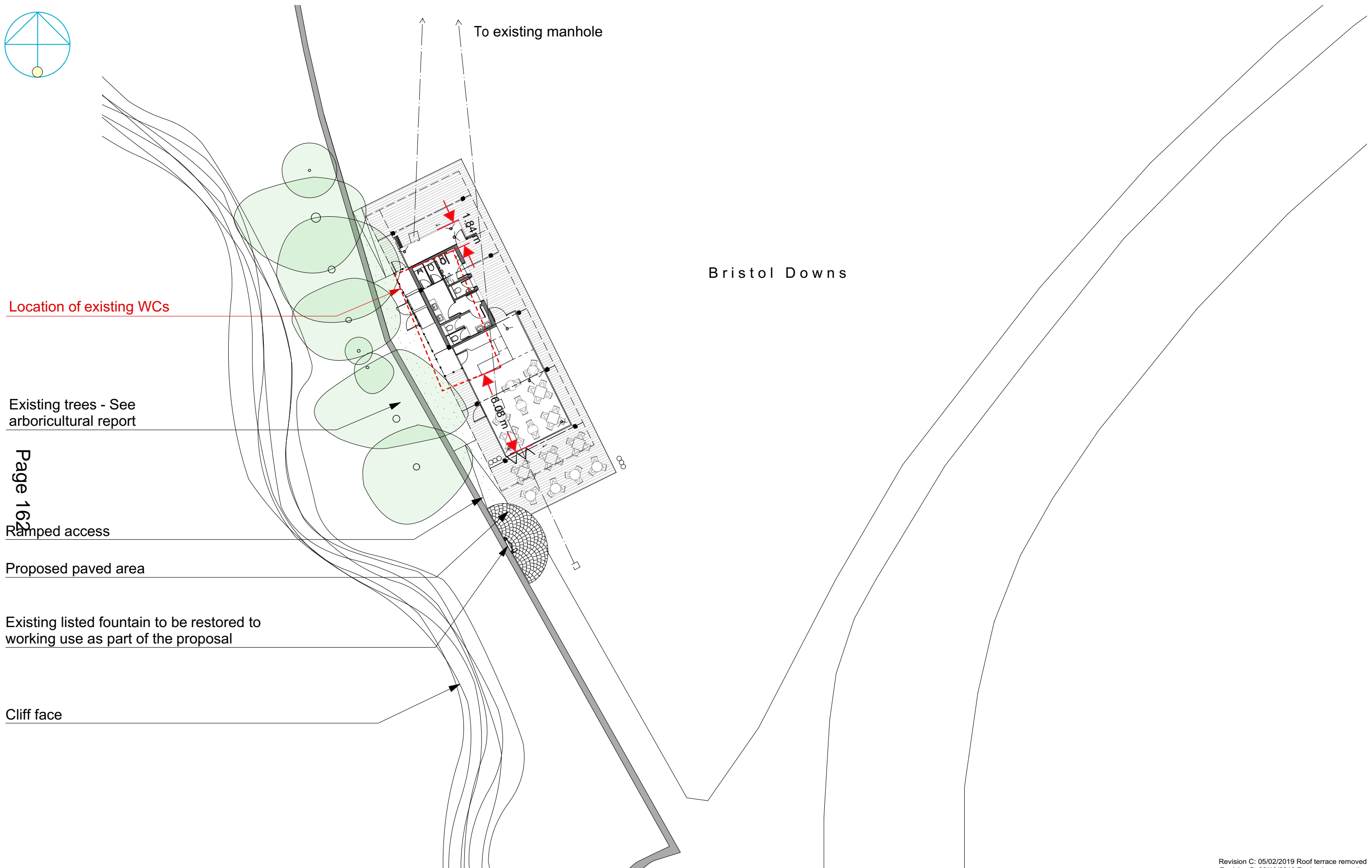
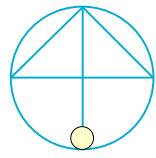
Full details of the drainage plans are to be submitted for acceptance to the Network Rail Asset Protection Engineer. No works are to commence on site on any drainage plans without the acceptance of the Network Rail Asset Protection Engineers: Network Rail has various drainage standards that can be provided Free of Charge should the applicant/developer engage with Network Rail's Asset Protection Engineers.

- 11 Application for advertisement consent needed: You are reminded of the need to obtain separate consent under the Town and Country Planning (Control of Advertisements) Regulations 1992 for any advertisements requiring express consent which you may wish to display on these premises.

Supporting Documents

4. Public Conveniences, Circular Road, Sneyd Park, Bristol, BS9 1ZZ

1. Proposed site plan
2. Proposed plans
3. Proposed elevations
4. Proposed section
5. Proposed visualisation
6. Site Photographs



Location of existing WCs

Existing trees - See arboricultural report

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Ramped access

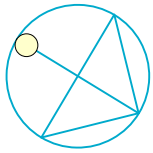
Proposed paved area

Existing listed fountain to be restored to working use as part of the proposal

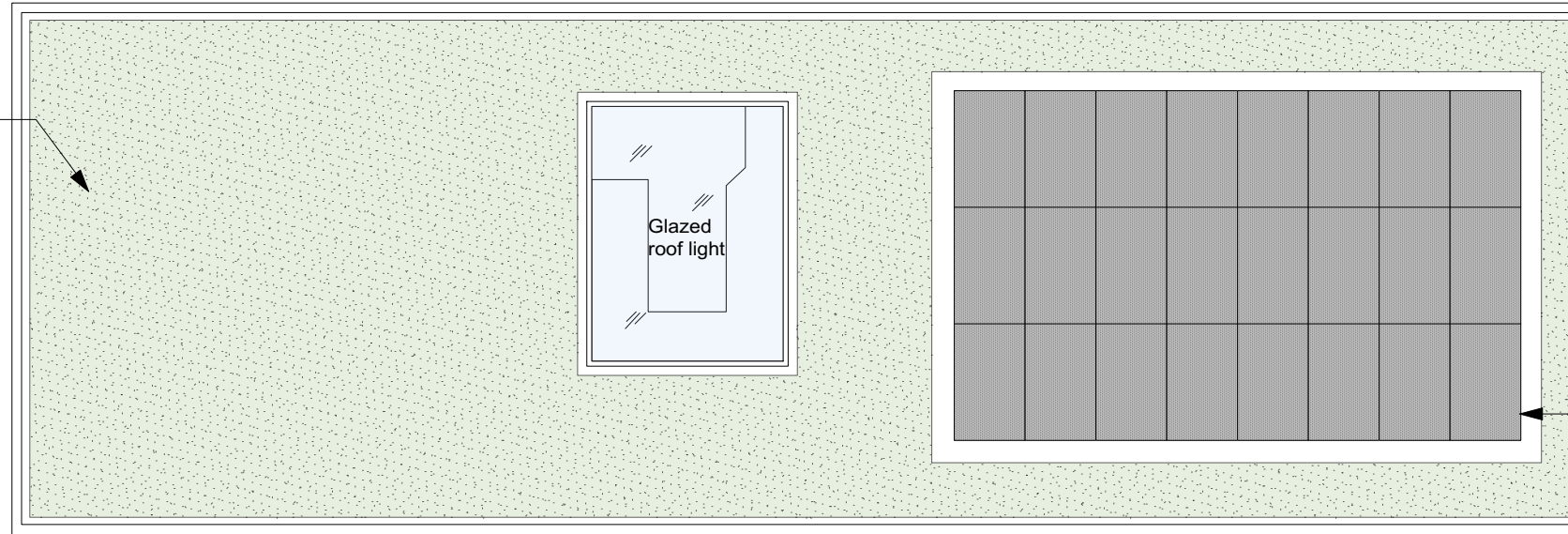
Cliff face

Bristol Downs

Revision C: 05/02/2019 Roof terrace removed
Revision B: 22/10/2018 Revised post meeting
Revision A: 12/09/2018 Site layout adjusted



Extensive roof planted with local native species, interpreted for educational purposes



Roof Plan

Area Schedule

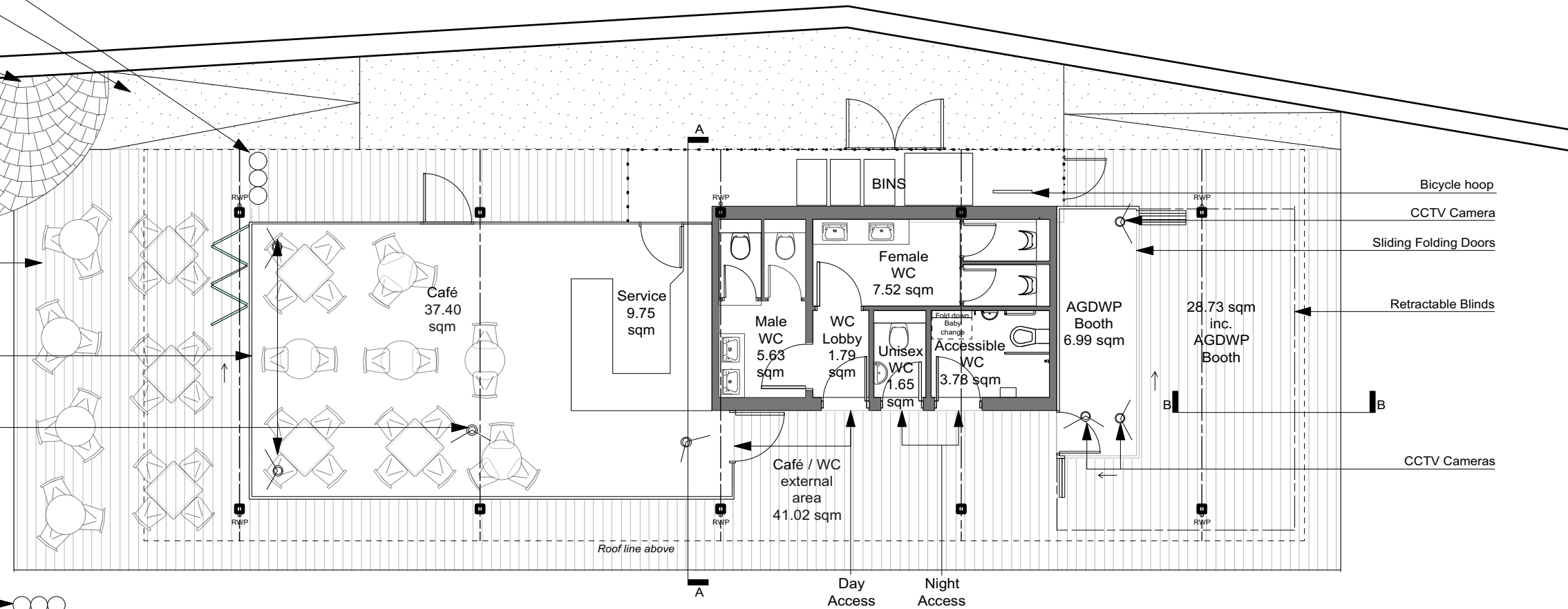
Ground:	74.51 sqm
External:	62.76 sqm
Total	137.27 sqm

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Waste/Recycling Bins
Access Ramp
Listed Fountain

Existing Pavement
Paved Area
Decking
Sliding Folding Doors
CCTV Cameras

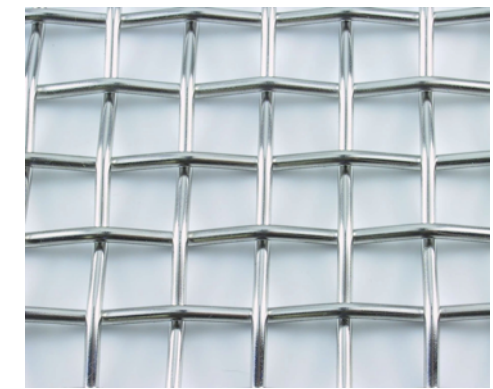
Waste/Recycling Bins
Bicycle racks



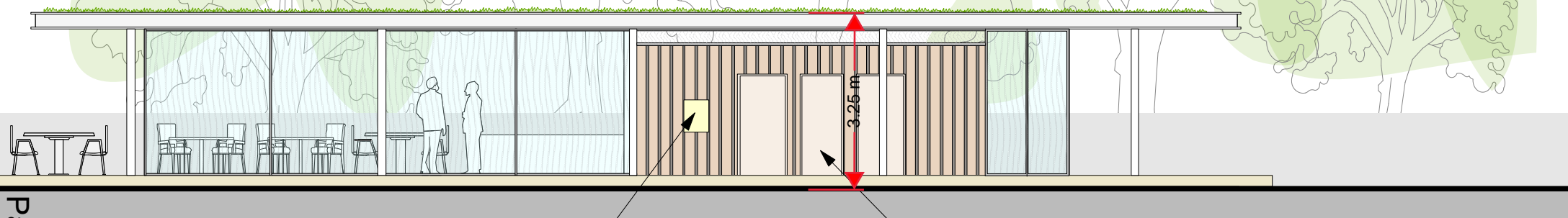
Ground Floor Plan

Bicycle hoop
CCTV Camera
Sliding Folding Doors
Retractable Blinds
CCTV Cameras

Revision F: Updated RWP's, roof, cycle hoop, retractable blinds 14/02/2019
Revision E: Roof terrace removed, 04/02/2019
Revision D: Revised following planners comments, 08/01/2019
Revision C: Revised post meeting, 22/10/2018
Revision B: Bins and CCTV cameras added, 05/09/2018
Revision A: Location adjusted, ramp relocated, 03/09/2018



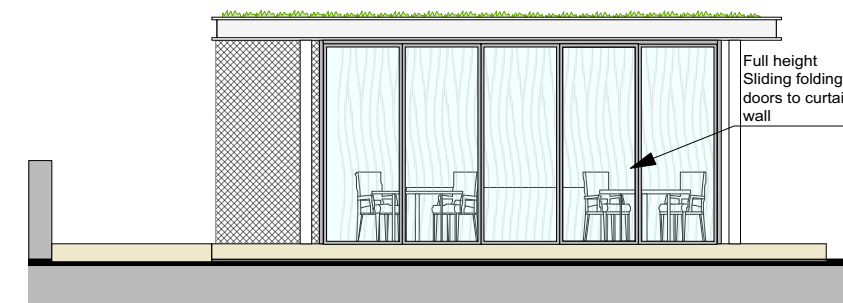
Example of woven stainless steel mesh to be used to enclose bin store



Front Elevation

Defibrillator

Toilet doors to be timber clad



Side Elevation

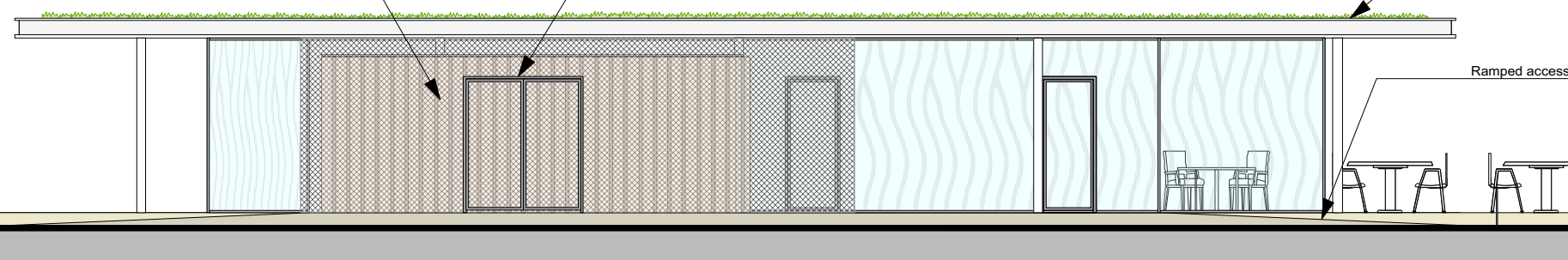
Full height Sliding folding doors to curtain wall

Stainless Steel decorative mesh to secure enclosure to BOH

Mesh gate on metal frame

Extensive roof planted with local native species, interpreted for educational purposes

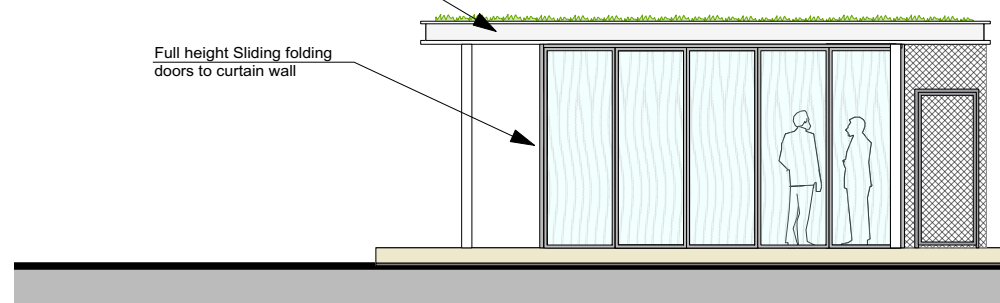
Ramped access



Rear Elevation

Steelwork to be painted

Full height Sliding folding doors to curtain wall



Side Elevation

Rev. F: Notes added, 14/02/2019
Rev. E: Roof terrace removed, 04/02/2019
Rev. D: Revised following planning comments, 08/01/2019
Rev. C: Revised post meeting, 22/10/2018
Rev. B: Ramp adjusted, 03/09/2018
Rev. A: 31/08/2018

Notes

Roof construction:

Green roof to overhang planted with species native to the Avon gorge
22mm plywood roofdeck on
Firrings commencing at a thickness of 75mm to allow 50mm continuous ventilation on
Roof joist size tbc by SE (195x75mm @400mm ctrs shown) with 200mm rockwool between and
Min 25mm rigid insulation on
Vapour barrier on
12.5mm plasterboard
Supported by Steel Beams to SE spec supported on
Columns and steel details to SE spec
Perimeter insulation as indicated

Steel structure:

Beams, columns, bracing, foundations and details to SE spec

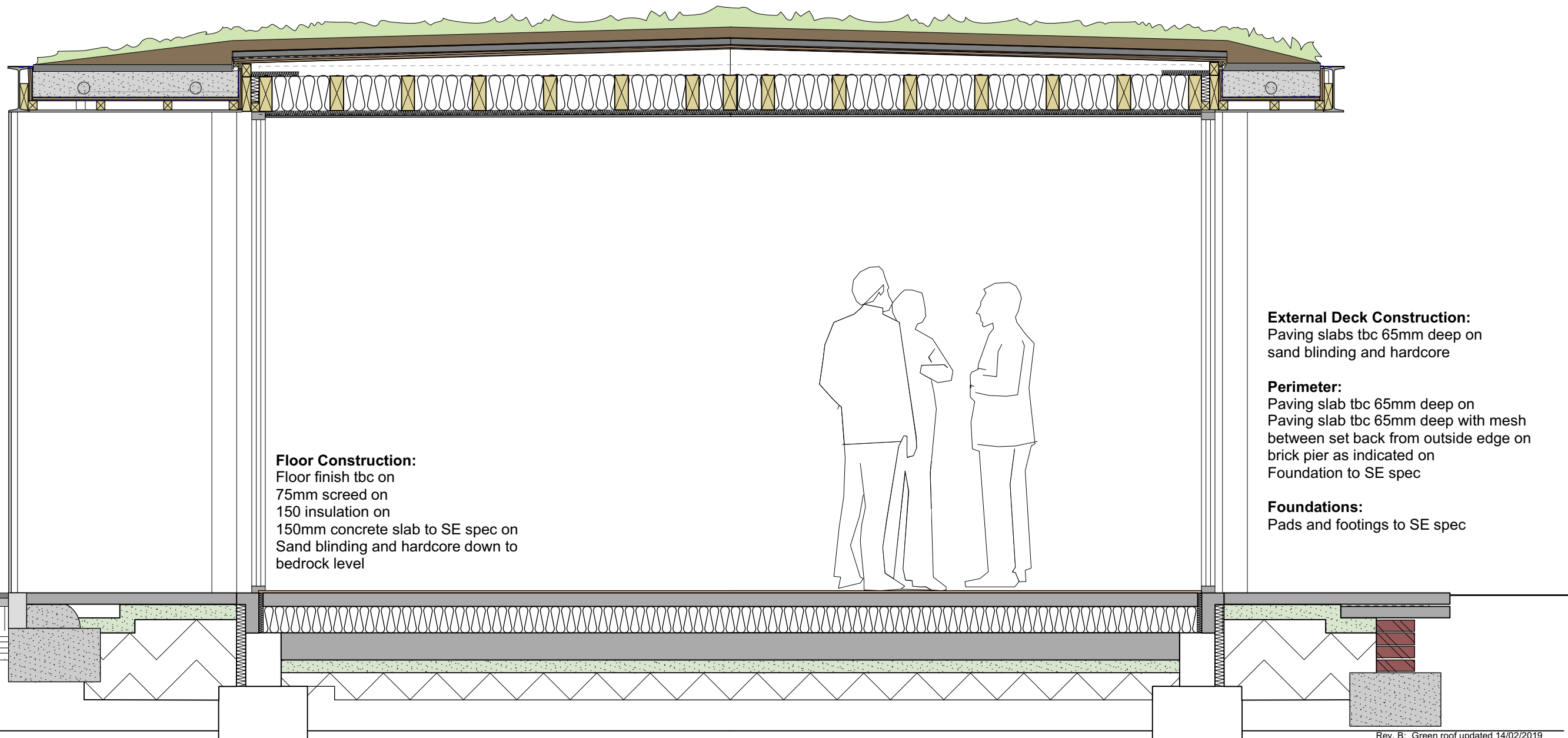
Glazing:

System tbc
Frameless glazing fixed back to structural timbers at base

Overhang to be filled with gravel for drainage.

Land drain within the gravel connected to rainwater downpipes

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Floor Construction:

Floor finish tbc on
75mm screed on
150 insulation on
150mm concrete slab to SE spec on
Sand blinding and hardcore down to bedrock level

External Deck Construction:

Paving slabs tbc 65mm deep on
sand blinding and hardcore

Perimeter:

Paving slab tbc 65mm deep on
Paving slab tbc 65mm deep with mesh
between set back from outside edge on
brick pier as indicated on
Foundation to SE spec

Foundations:

Pads and footings to SE spec

Rev. B: Green roof updated 14/02/2019
Rev. A: Roof terrace removed 04/02/2019



Rev. A: Roof terrace removed, 05/02/2019

Seawalls viewing point

Listed drinking fountain

Existing toilet block



Listed drinking fountain

Existing toilet block





Towerhirst

Existing toilet block



Existing toilet block

Towerhirst





MEN

Person sitting on a bench in the background.

Development Control Committee B – 13 March 2019

ITEM NO. 5

WARD: Lawrence Hill **CONTACT OFFICER:** Conrad Rodzaj
SITE ADDRESS: St Gabriels Court St Gabriels Road Bristol BS5 0RT

APPLICATION NO: 18/04108/F Full Planning

DETERMINATION DEADLINE: 8 March 2019

Demolition of all buildings, altered access and construction of a mixed use development comprising 'flexible' commercial uses (Use Classes: A1, A2 and/or B1(a-c) along with 10 dwellinghouses with associated car parking, refuse and cycle storage.

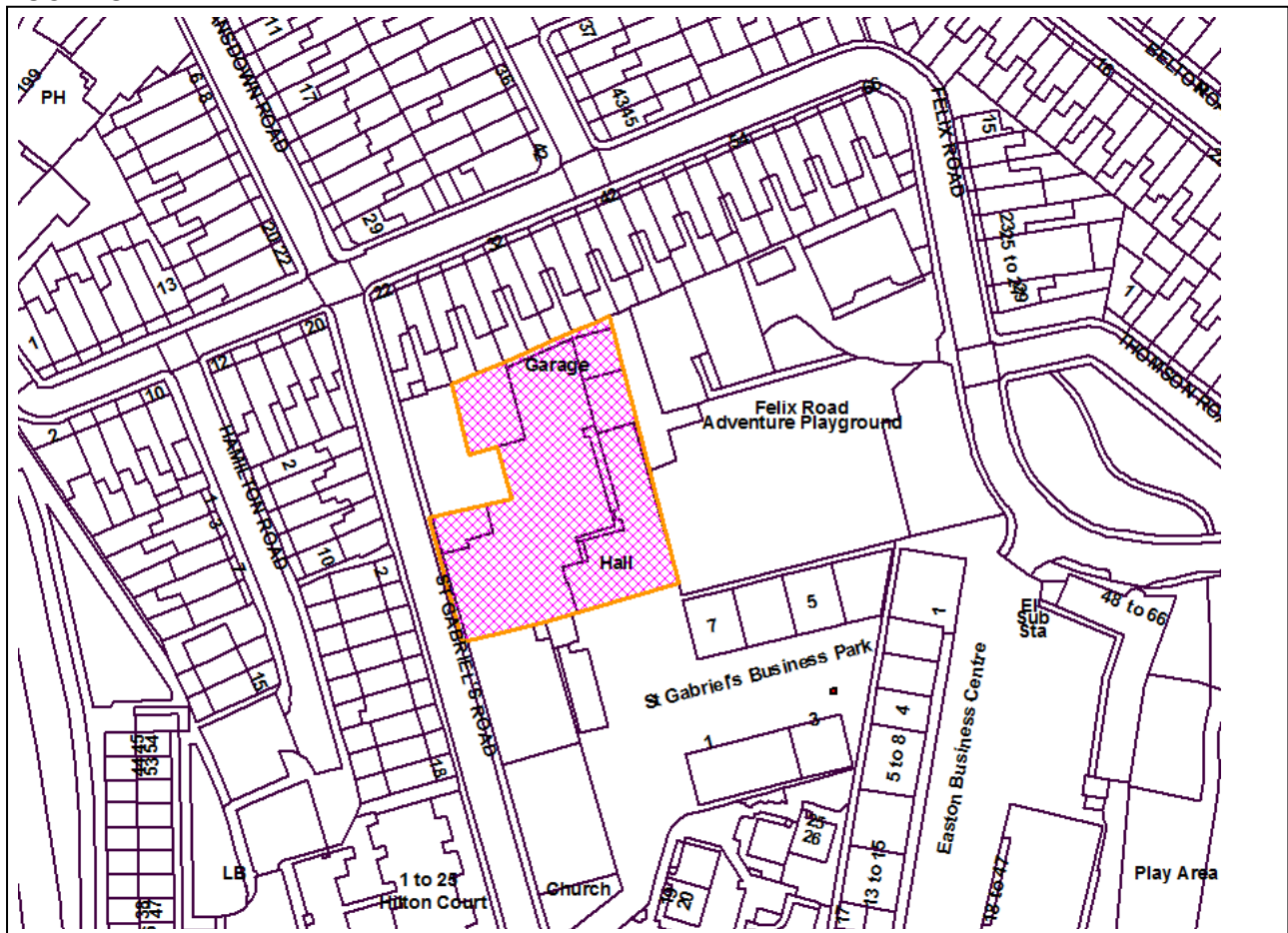
RECOMMENDATION: Grant subject to Condition(s)

AGENT: Aspect360 Ltd
 45 Oakfield Road
 Clifton
 Bristol
 BS8 2AX

APPLICANT: Cabot Trustees Ltd
 c/o agent

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



Development Control Committee B – 13 March 2019**Application No. 18/04108/F : St Gabriels Court St Gabriels Road Bristol BS5 0RT****BACKGROUND**

St Gabriel's Court consists of a number of units containing retail, storage, office, food and drink, business and general industrial uses (garage) approved under permission 13/01989/F. It is understood that these operate during typical daytime hours, that is, 09:00 to 18:00. Residential uses exist to the north, including a recent permission for change of use from a D1 use (approved under 17/06437/F). To the east is the Felix Road adventure playground. To the southeast lies St Gabriel's Business Park which comprises commercial units. To the south lies a site which has permission for a hand car wash, approved under 14/04343/F.

The site lies within a Coal Authority High Risk area.

See application documents for further details.

HISTORY

18/01953/COU Units 13-15 - 'Class MA' conversion of retail units to 2no. residential dwellings. - Prior Approval GIVEN

18/01954/COU Units 16 and 21 - 'Class O' conversion of offices to 3no. residential dwellings. - Prior Approval GIVEN

17/06437/F - Proposed conversion of building from D1 after school club to 2no. residential dwellings (Use Class C3) with associated demolition and external works (Approved 08.03.2018). This is located to the north of the site.

14/04343/F - Change of use to a hand car wash and valeting facility with associated container store and wash bay enclosure (Approved 25.06.2015). This is located to the south of the site.

13/01989/F - Retrospective application for change of use of the buildings within the site to include; up to 300sqm of A1 floor space, up to 200sqm of A3 floor space, up to 200sqm of B1 floor space, up to 150sqm of B2 floor space, up to 350sqm B8 floor space up to 330sqm of D1 floor space. Alterations and remodelling to the layout of the site to include; 14 car parking spaces, cycle parking, erection of 900mm high pedestrian guard rails, resurfacing, erection of bollards, planting and refuse/recycling stores (Approved 27.09.2013).

This permission sought to regularize a planning consent following enforcement history on the site.

PROPOSAL

Demolition of all buildings, altered access and construction of a mixed use development comprising 'flexible' commercial uses (Use Classes: A1, A2 and/or B1 (a-c) along with 10 dwellinghouses with associated car parking, refuse and cycle storage.

RESPONSE TO CONSULTATION

Neighbouring properties have been consulted with 2 letters of support received and 11 objections have been raised with the following planning concerns:

Amenity:

Concerns of overlooking to the rear of dwellings on Felix Road which may be increased by the lowering of the existing boundary wall.

Concern of overlooking to the playground at the rear.

Concern of overlooking from the commercial building to 10 Hamilton Road.

Unable to see a revised sunlight study. Revised shadow study is still inadequate not showing enough periods.

Development Control Committee B – 13 March 2019

Application No. 18/04108/F : St Gabriels Court St Gabriels Road Bristol BS5 0RT

Suggestions of a screen on the balcony of properties number 7, 8 and 9 to eliminate residents being able to look across and down into the bedrooms on Felix Road.

To align properties 10, 9, 8 and 7 on a slight angle in line with the brick wall, which runs along the back of 34, 32 and 30 Felix Road. This will stop any overlooking and breach of privacy into the bedrooms of the residents of Felix Road.

Overbearing impact due to height and proximity of proposed nearest dwelling to rear of Felix Road. Loss of light to rear of dwellings at Felix Road given change in levels of the site and the proposed height of the dwellings.

Proposed tree planting would result in loss of light to rear of Felix Road.

Design:

Concerns over the height of the proposed buildings being out of character with the area.

Highways:

Concerns that parking provision is insufficient resulting in a highway safety impact.

Traffic impact to and surrounding roads.

Loss of commercial:

The site currently caters for 10+ local small scale BME businesses. Concern that there will be a loss of business & employment opportunities in the community and the current proposal does not address this issue.

Concerns raised there is a huge shortage of affordable commercial units within inner city. Many local entrepreneurs cannot afford large B1 complex 3,000+ Sqf. For example there is a long waiting list for St Gabriel's industrial units next to this site; there is also long waiting list in the Easton Business workshops.

Other:

Concerns of building works causing disruption.

The site has been contaminated by previous occupants.

Concerns that the consultation did not include all the affected neighbours. *Officer comment: All statutory neighbours were consulted as part of the consultation, a site notice and press advert were also included as part of the consultation.*

Suggestion that the proposal should have included greater community engagement.

Lack of provision made for affordable housing.

WARD MEMBER

The local ward member has called in the application to committee following concerns about neighbour amenity.

HIGHWAYS

TDM have no objection to the proposals subject to conditions.

As the internal layout is not adoptable, we would require the signing of an exemption certificate (by the owners of the land). This would then act as a landcharge against the property. We would require this to be signed prior to any approval.

BRISTOL WASTE

Concerns raised over the size of the bin store and the distance of the collection point to the public highway.

CITY DESIGN GROUP

No objections subject to conditions.

Development Control Committee B – 13 March 2019

Application No. 18/04108/F : St Gabriels Court St Gabriels Road Bristol BS5 0RT

POLLUTION CONTROL

No objections subject to condition.

CONTAMINATED LAND

No objection subject to conditions.

ARCHAEOLOGY

No objections subject to standard watching brief conditions: B28 and C18.

FLOODRISK MANAGER

No objection.

NATURE CONSERVATION

No objection subject to informative.

ARBORICULTURAL CONSULTANT

No objections subject to conditions.

AIR QUALITY

No objections.

PROJECT MANAGER SUSTAINABILITY

No objection subject to conditions.

AVON FIRE AND RESCUE

No objection with the installation of fire 1 hydrant.

RELEVANT POLICIES

National Planning Policy Framework – February 2019

Bristol Local Plan comprising Core Strategy (Adopted June 2011), Site Allocations and Development Management Policies (Adopted July 2014) and (as appropriate) the Bristol Central Area Plan (Adopted March 2015). In determining this application, the Local Planning Authority has had regard to all relevant policies of the Bristol Local Plan and relevant guidance.

KEY ISSUES

A) PRINCIPLE OF DEVELOPMENT - IS THE LOSS OF SOME EXISTING COMMERCIAL FLOORSPACE AND PROPOSED RESIDENTIAL DEVELOPMENT ACCEPTABLE IN PRINCIPLE?

The existing site is mixed commercial site comprising of A1, A3, B2, B8 floor and D1 floor space. Policy DM 12 of the Sites Allocation and Development Management Local Plan seeks to retain employment premises where they make a valuable contribution to economy and employment floor spaces.

The site is not within a Principle Industrial and Warehousing Area. The site is not within a highstreet and does not provide a local shopping service that policy BCS7 would seek to protect. The existing mixed use commercial totals approximately 1001 m2. Following redevelopment of the site the proposed mixed commercial use would total 537 m2.

Policy DM12 relates to retaining employment sites unless it can be demonstrated that the criteria listed under this policy can be met, including that there is no demand for employment uses; or a net reduction in floorspace is necessary to improve the existing premises. It is considered that the existing buildings are dated with the facilities in need of improvement. The site is currently badly managed and many of the units are sub-let. The existing premises have a history of enforcement action with subsequent retrospective planning applications. The proposed mixed use element of the proposal

Development Control Committee B – 13 March 2019**Application No. 18/04108/F : St Gabriels Court St Gabriels Road Bristol BS5 0RT**

would result in much improved commercial premises while freeing up space for the creation of housing.

The site currently caters for 10+ local small scale BME businesses. Concern have been raised that there will be a loss of business & employment opportunities in the community and the current proposal does not address this issue. It is suggested there is a huge shortage of affordable commercial units within inner city. Many local entrepreneurs cannot afford large B1 complex 3,000+ Sqf. For example there is a long waiting list for St Gabriel's industrial units next to this site and the nearby Easton Business workshops.

The site will still retain mixed use commercial floorspace, with smaller units envisaged. The marketing report states that rents are likely to be at the lower end of the market. Therefore the proposed new commercial premises is likely to still be able to cater for smaller local BME businesses. It has been conditioned that the proposed commercial floorspace is constructed and marketed prior to the occupation of the residential units.

It is considered that in principle the proposed loss of some of employment space complies policies DM7, DM12 and the NPPF. The benefits of the improved facilities and addition of housing supply on the site are considered to outweigh the harm.

Section 2 of the National Planning Policy Framework (NPPF) outlines that housing applications should be considered in the context of the presumption in favour of sustainable development. With focus on a social objective to support strong, vibrant and healthy communities, by ensuring that a sufficient number and range of homes can be provided to meet the needs of present and future generations; and by fostering a well -designed and safe built environment, with accessible services and open spaces that reflect current and future needs and support communities' health, social and cultural well being.

Policy BCS5 (Housing Provision) of the Core Strategy outlines that delivery of housing to meet the Council's housing targets will primarily be focused on previously developed sites however some open space will be utilised for housing development. The strategy by which the Council will allow development of open space is set out within the Site Allocations & Development Management Policies (SADMP) Local Plan.

Policy DM1 (Presumption in Favour of Sustainable Development) of the SADMP outlines that the city's approach to development proposals will generally be positive and reflective of the presumption in favour of sustainable development as referenced throughout the NPPF.

The site is located within an area of existing residential development, adjacent to a commercial site. The site lies within the existing urban area within easy walking distance of a local centre on Stapleton Road. It is located on a main bus route into and out of the city centre. It is in a very sustainable location where national and local plan policies encourage the more efficient use of land, subsequently the redevelopment of the site for residential use is acceptable in principle.

B) IS THE MIX OF HOUSING, ITS TENURE AND DENSITY ACCEPTABLE?

Policy BCS18 (Housing Type) of the Core Strategy states that all new residential development should maintain, provide or contribute to a mix of housing tenures, types and sizes to help support the creation of mixed, balanced and inclusive communities.

The proposed residential accommodation to be provided, would be x10 3 bedroom dwellings. The area consists of a mixture of 2 and 3 bedroom dwellings which consist mainly of Victorian terraces and modern terraces and 1 and 2 bedroom flats many within nearby highrise blocks. The proposed dwellings would therefore represent an appropriate mix and would therefore be welcomed in this location especially given there is a need for family housing in the city in inner urban areas.

Concerns have been raised over the lack of provision made for affordable housing. Under policy DM3: Affordable Housing Provision Smaller Sites affordable housing contributions will only be sought from schemes of 11 to 14 units. Contributions from schemes of 10 units will only be sought where the maximum combined gross floorspace is above 1,000 square metres (gross internal area). The site proposes 10 units with gross floorspace of 930 m2 therefore does not trigger the need to provide

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affordable housing.

C) KEY ISSUES

i) ECOLOGY

Policy DM19 is relevant which requires any loss of nature conservation value to be either mitigated on site or off site.

The council's ecologist has suggested that informatives have been added to the permission to protect wild birds in the trees and hedges to be removed. Also an informative to protect bats during the demolition of the existing buildings.

With the suggested informatives the proposed impact on ecology is not considered detrimental.

ii) CONTAMINATION

Concerns have been raised that the site has been contaminated by previous occupants. The site has been subject to assessment and a number of reports have been submitted on the results of soil and soil gas testing. Conditions are recommended to require this additional information.

iii) GROUND STABILITY

The site is in High Risk area of former Coal Mining. A full coal mining assessment and intrusive site investigations have already been undertaken on the site. The report concludes that there is no risk to the site from ground instability given that the nearest former shafts have been filled and capped.

iv) IS THE IMPACT ON TREES ACCEPTABLE?

An arboricultural survey has been submitted to inform consideration of the application. Only three trees grow at the site, two Norway Maples and a Sycamore. If the site is to be developed then it is inevitable that the trees will be lost as a direct result of building. The arboricultural survey confirms that the existing trees on site are low grade and not worthy of retention, none are protected. The proposed amount of trees to be planted exceeds that of the amount required in the Tree Replacement Standards which would be 8 for this site which includes sufficient planting within the public domain. Some of the trees proposed are sited in private back gardens; however it would not be reasonable to condition these to be retained.

Given the limited number of trees on site which are of low quality the proposed trees and landscaping is considered an improvement.

D) IS THE DESIGN OF THE PROPOSAL ACCEPTABLE?

Section 12 of the NPPF outlines the creation of high quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. Being clear about design expectations, and how these will be tested is essential for achieving this. So too is effective engagement between applicants, communities, local planning authorities and other interests throughout the process.

Policy BCS21 (Quality Urban Design) of the Core Strategy advocates that new development should deliver high quality urban design that contributes positively to an area's character and identity, whilst safeguarding the amenity of existing development.

Policy DM26 (Local Character & Distinctiveness) of the SADMP expands upon Core Strategy Policy BCS21 by outlining the criteria against which a development's response to local character and distinctiveness will be assessed. Development will not be permitted where it would be harmful to local character and distinctiveness or where it would fail to take the opportunities available to improve the

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character and quality of the area and the way it functions.

Policy DM27 (Layout and Form) of the SADMP outlines that the layout, form, pattern and arrangement of streets, open spaces, development blocks, buildings and landscapes should contribute to the creation of quality urban design and healthy, safe and sustainable places. It should make efficient use of land, provide inclusive access and take account of local climatic conditions.

Policy DM29 (Design of New Buildings) of the SADMP states the design of new buildings should be of high quality. Buildings should reflect their function and role appropriately within the public realm and be capable of adaptation to accommodate alternate uses and future needs. New buildings should incorporate active frontages, have clearly defined main entrances fronting the public realm, incorporate exteriors and elevations which provide visual interest from a range of viewing distances, be visually well organised and well proportioned.

Concerns have been raised over the height of the proposed buildings being out of character with the area. Adjacent to the site fronting St.Gabriels Road lies the former Easton Colliery Yard building. The former industrial building has strong presence and its elongated form sits along the kerbside. The proposed new commercial unit fronting St.Gabriels Road will form a contemporary take on the mass and form of the adjacent building. The eaves and ridge heights will match that of the adjacent building and rise at the opposite end. The dwellings behind will be set at a similar and set below the height of adjacent industrial buildings in the adjacent commercial site. The proposed height and mass is considered inkeeping with the character and appearance of the wider streetscene.

Given the levels of the site and the width of the road and separation distances from adjacent developments the proposed height of the proposed development is considered acceptable.

The proposed scale mass and design is considered inkeeping with the character and appearance of the wider area. The proposal is considered an improvement over the existing commercial buildings and will provide a positive contribution to the streetscene. The proposed design is considered acceptable.

E) WILL THE DEVELOPMENT PROVIDE SATISFACTORY ACCOMMODATION?

All dwellings comply with the Nationally Described Space Standards and maintain satisfactory privacy distances. The houses all have access to private amenity space and all dwellings will have access to the communal areas of open space.

F) WILL THE PROPOSAL HAVE AN ADVERSE IMPACT ON NEIGHBOUR AMENITY?

Section 8 of the NPPF outlines decisions should ensure that developments create places that are safe, inclusive and accessible and which promote health and well -being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

Suggestion were raised that the proposal should have included greater community engagement. A site meeting by was made with local residents and the Ward member. The consultation responses were fed back to the developer's agent and reasonable amendments and additional information was requested.

IMPACT ON PRIVACY

Policy BCS21 (Quality Urban Design) of the Bristol Core Strategy advocates that new development should give consideration to matters of neighbouring privacy, outlook and natural lighting.

Policy DM29 (Design of New Buildings) of the SADMP states the design of new buildings should be of high quality. To achieve this, new buildings are expected to ensure that existing and proposed development achieves appropriate levels of privacy, outlook and daylight.

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Concern of overlooking to the playground at the rear have been raised. The proposed properties to the rear of the site will gain views of the playground. However it would be unreasonable to protect the amenity of the playground as a material consideration. The police crime prevention design officer has welcomed the additional natural surveillance.

Balcony screens have been added to the dwellings and conditioned which will prevent overlooking between residents. The existing Felix Road boundary wall is not proposed to be lowered.

It is considered that the submitted plans show sufficient detail in order to gauge any potential impact on neighbour amenity; it is considered that there are adequate separation distances between neighbouring properties and those within the development. Concerns have been raised of overlooking to Felix Road. The proposed front windows of the adjacent dwelling are at an acute angle to the rear gardens of Felix Road, the proposed views would be limited to the end of the gardens to an extent that would not be considered unacceptable in an urban setting. Concern of overlooking from the commercial building to 10 Hamilton Road has been raised. The proposed building will be across the road with a distance of around 9.5 metres to the rear garden boundary and around 19.5 metres to the dwellinghouse. This distance is considered acceptable and similar minimum distances would apply to other dwellings in Hamilton Road. Similar distances apply to the former collier building which has permission for residential use. The distances from the commercial building to the front gardens of the terraces across the road are around 9.5 metres and the distance to the dwellings is around 13.5 metres. This distance is close but not uncommon in an urban setting and given that the commercial building will not be in use at night the impact of overlooking is reduced.

A suggestion has been made to align properties 10, 9, 8 and 7 on a slight angle in line with the brick wall, which runs along the back of 34, 32 and 30 Felix Road. It is suggested this will stop any overlooking and breach of privacy into the bedrooms of the residents of Felix Road.

The above suggestion is considered unreasonable given that the proposal is not considered to result in overlooking to neighbour amenity to a level that would be detrimental to living conditions.

IMPACT ON LIGHT

Concerns have been raised that the proposed tree planting would result in loss of light to rear of Felix Road. The proposed landscaping plan has been conditioned and will need to be reviewed that any species planted near the boundary will not grow to an excessive height that may result in an overbearing impact to neighbouring properties.

Concerns have been raised of an overbearing impact and a loss of light to rear of dwellings at Felix Road given change in levels of the site and the proposed height of the nearest dwelling. The amended plans have reduced the height of the nearest dwelling to two storey. The proposed dwelling is higher than the existing industrial building it replaces however it is set further from the boundary. The proposed impact is similar to that of the existing building, it will clear a 25 degree vertical angle from the nearest rear facing groundfloor windows in Felix Road. The proposed gable end is sited around 12 metres away from the nearest rear elevation which is considered acceptable. Sunlight studies have been submitted for the key times of the year and they demonstrate the impact of the proposal would not cause detrimental shading to the rear gardens of the properties at Felix Road. Given the layout of the plot of land and surrounding townscape, and the subsequent design, massing and form of the proposed dwelling, it is considered that no unacceptable amenity issues will arise. It is not considered that the proposal would not result in an overbearing impact or loss of light that would be detrimental to neighbour amenity.

NOISE IMPACT

Environmental protection has raised no concerns regarding noise from the proposed and adjacent commercial units to the proposed and existing dwellings. However given that air source heat pumps are proposed an acoustic report has been conditioned to assess the impact on existing adjacent dwellings.

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Concerns of building works causing disruption have been raised. A construction management plan has been conditioned and this is covered by other legislation.

G) IS THE DEVELOPMENT ACCEPTABLE ON HIGHWAY GROUNDS?

Section 4 of the NPPF outlines that transport policies have an important role to play in facilitating sustainable development but also in contributing to wider sustainability and health objectives. Smarter use of technologies can reduce the need to travel. The transport system needs to be balanced in favour of sustainable transport modes, giving people a real choice about how they travel. However, the Government recognises that different policies and measures will be required in different communities and opportunities to maximise sustainable transport solutions will vary from urban to rural areas.

Policy DM23 (Transport Development Management) of the SADMP outlines that new development should not give rise to unacceptable traffic conditions and will be expected to provide safe access to the highway network. The policy also outlines that new development should be accessible by sustainable transport methods such as walking, cycling and public transport. Furthermore, the policy sets standards for parking provision.

TDM deems this acceptable due to the sustainable location for residential and commercial development. This is based upon the location within an established residential area and within walking distance of bus routes, shops and facilities.

PARKING

The Bristol Local Plan: Site Allocations and Development Management Policies (adopted July 2014) (SA&DMP) details the following parking standard for the proposed use of the site: Three or more bed/house flat: an average of 1.5 spaces per dwelling below that 1.25 spaces per dwelling. The vehicular parking provision is a maximum. The site proposes 1 parking space per dwelling and 9 visitor spaces from the dwellings and commercial unit. This is considered acceptable. The proposed parking arrangements are considered acceptable subject to conditions.

ACCESS AND LAYOUT

TDM have confirmed the proposed visibility splay can be secured through a pre-commencement condition. It needs to be the width of a car - around 2m over and above the footway width. This will protect the parking bay and allow visibility past parked vehicles.

The proposal is not proposed to result in a detrimental impact to the parking of the adjacent roads or result in a detrimental impact to Highway Safety.

As the internal layout is not adoptable as the site is on private land and the proposed internal road is minor, TDM would require the signing of an exemption certificate (by the owners of the land). This would then act as a landcharge against the property. TDM would require this to be signed prior to any approval and have no objections subject to conditions.

CYCLE PARKING

In relation to cycle parking, Policy DM23 requires a provision of 2 cycle parking spaces per 2 or 3 bed dwelling. TDM are happy that the development provides sufficient cycle storage provision and this has been conditioned.

REFUSE

Policy DM32 (Recycling & Refuse Provision in New Development) of the SADMP outlines that all new development should provide bin and recycling storage facilities fit for the nature of development, with adequate capacity for the proposed development, in a location which is safe and accessible for all users and does not harm the visual amenity of the area or neighbouring amenity.

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The refuse store proposed is suitable and accessible, amended plans have moved a collection point a suitable distance from the highway and increased the bin store capacity.

H) CLIMATE CHANGE

i) ENERGY STRATEGY

Themes of sustainability, carbon reduction and climate change underpin national planning policy. Policies BCS13-15 of the Core Strategy relates to the Council's expectations with regard to sustainable construction of new buildings and emissions in respect of climate change. These policies must be addressed and the guidance within the Council's Climate Change and Sustainability Practice Note followed. New dwellings are expected to minimise energy requirements. This will be achieved by high standards of energy efficiency including optimal levels of thermal insulation, passive ventilation and cooling, passive solar design, and the efficient use of natural resources in new buildings. Core Strategy Policy requires new dwellings are also incorporate an element of renewable energy to reduce carbon emissions by a further 20% above energy saving measures.

A sustainability statement and energy table have been submitted accompanying the application including a range of proposed energy efficiency measures and further on site renewable energy generation measures. Suggested on site renewable energy measures include solar photovoltaic panels and air source heatpumps. Sustainable cities accept the targets can be met and have requested that further details of the proposed renewable energy sources are conditioned prior to occupation.

The proposed development is therefore considered acceptable and would make a positive contribution towards reducing energy consumption and carbon emissions.

ii) DRAINAGE

A sustainable drainage strategy has been conditioned prior to commencement.

iii) OTHER SUSTAINABILITY ISSUES

It is important that superfast Broadband is available to all new development. There is a commitment in the submission to the provision of a connectivity assessment, but this has not been received to date. This can be conditioned.

I) ARE THERE ANY EQUALITY ISSUES?

It is considered that the scheme is inclusive of the wider community and supports equality and diversity within its design.

During the determination of this application due regard has been given to the impact of this scheme in relation to the Equalities Act 2010 in terms of its impact upon key equalities protected characteristics. These characteristics are age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation. There is no indication or evidence (including from consultation with relevant groups) that different groups have or would have different needs, experiences, issues and priorities in relation this particular proposed development. Overall, it is considered that the determination of this application would not have any significant adverse impact upon different groups or implications for the Equalities Act 2010.

J) OTHER ISSUES

No other issues.

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The residential development and mixed use commercial development on this site is welcomed and the proposal achieves a satisfactory density of development within the existing site.

APPROVE WITH CONDITIONS.

Subject to an exception certificate being signed confirming the internal road is not adopted highway (Prior to the decision being issued).

The following development types will be liable for CIL:

- i. Development comprising 100m² or more of new build floorspace
- ii. Development of less than 100m² of new build floorspace that results in the creation of one or more dwellings
- iii. The conversion of a building that is no longer in lawful use

In this case, the proposed development would comprise of new build residential floor space creating a new dwelling. The development is therefore CIL liable £ 8778.00

RECOMMENDED GRANT subject to condition(s)

Time limit for commencement of development

1. Full Planning Permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

Pre commencement condition(s)

2. No works shall take place, including any works of clearance or demolition, until a construction management plan or construction method statement has been submitted to and been approved in writing by the Local Planning Authority. The approved plan/statement shall be adhered to throughout the construction period. The statement shall provide for:

Parking of vehicle of site operatives and visitors;

Routes for construction traffic;

Method of prevention of mud being carried onto highway;

Pedestrian and cyclist protection;

Proposed temporary traffic restrictions;

Arrangements for turning vehicles

Procedures for maintaining good public relations including complaint management, public consultation and liaison

Arrangements for liaison with the council's Pollution Control Team

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All works and ancillary operations which are audible at the site boundary, or at such other places as may be agreed with the Local Planning Authority, shall be carried out only between the following hours: 0800 hours and 1800 hours on Monday to Fridays and 0800 and 1300 hours on Saturdays and at no time on Sundays and Bank Holiday

Deliveries to and removal of plant, equipment, machinery and waste from the site must only take place within the permitted hours detailed above

Mitigation measures as defined in BS 5528: Parts 1 and 2: Noise and Vibration Control on Construction and Open Sites shall be used to minimise noise disturbance from construction works

Procedures for emergency deviation of the agreed working hours

Bristol City Council encourages all contractors to be 'Considerate Contractors' when working in the city by being aware of the needs of neighbours and the environment

Control measures for dust and other air borne pollutants. This must also take into account the need to protect any local resident who may have a particular susceptibility to air borne pollutants

Measures for controlling the use of site lighting whether required for safe working or for security purposes

Reason: In the interests of safe operation of the highway in the lead into development both during the demolition and construction phase of the development. CMP and to safeguard the amenities of surrounding occupiers

3. A site specific risk assessment and intrusive investigation shall be carried out to assess the nature and extent of the site contamination and whether or not it originates from the site. The investigation and risk assessment must be undertaken by competent persons and a written report of the findings must be produced. The results of this investigation shall be considered along with the Preliminary Ground Investigation report submitted with the application. The written report of the findings shall be submitted to an approved in writing by the Local Planning Authority prior to any works (except demolition) in connection with the development, hereby approved, commencing on site. This investigation and report must be conducted and produced in accordance with DEFRA and the Environment Agency's Model Procedures for the Management of Land Contamination, CLR 11.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other off site receptors.

4. Following demolition no construction shall take place until a detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property and the natural and historical environment must be submitted to and approved in writing by the Local Planning Authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and

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ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

5. Sustainable Drainage System (SuDS)

No construction shall commence until a Sustainable Drainage Strategy and associated detailed design, management and maintenance plan of surface water drainage for the site using SuDS methods has been submitted to and approved in writing by the Local Planning Authority. The approved drainage system shall be implemented in accordance with the approved Sustainable Drainage Strategy prior to the use of the building commencing and maintained thereafter for the lifetime of the development.

Reason: To prevent the increased risk of flooding by ensuring the provision of a satisfactory means of surface water disposal is incorporated into the design and the build and that the principles of sustainable drainage are incorporated into this proposal and maintained for the lifetime of the proposal.

6. Submissions of samples before specified elements started

Notwithstanding the approved plans samples of all facing materials, wall materials as appropriate, plus roof tiles and paving materials shall be submitted to and be approved in writing by the Local Planning Authority before the relevant parts of the work are commenced. The development shall be completed in accordance with the approved samples before the building is occupied.

Reason: In order that the external appearance of the development is satisfactory.

7. Notwithstanding the approved plans prior to the commencement of the of the development hereby approved a sample panel of the proposed finish for the external walls shall be erected on site and made available to the local authority and approved in writing. This shall show all proposed materials and jointing. The panel shall be retained on site for the duration of the works to act as a reference.
The works shall be completed in accordance with the approved materials.

Reason: In order that the external appearance of the development is satisfactory.

8. Prior to commencement of the relevant element, section drawings of all proposed window reveals, doors, sill and lintels, balustrade, coping; junction detail between render and bricks and the proposed privacy screens (to a scale of 1:5) shall be submitted to and approved in writing by the Local planning Authority. The development will be completed in accordance with the approved drawings:

Reason: In order that the external appearance of the development is satisfactory.

9. Highway Works - General Arrangement Plan

No development shall take place until general arrangement plan(s) indicating the following works to the adopted highway have been submitted and approved in writing by the Local Planning Authority

- o Footway refurbishment across frontage of site
- o Buildout in St Gabriels Road at access

Indicating proposals for:

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- o Levels of the highway tying into building threshold levels, taking into consideration 125mm kerb height and drainage crossfalls
- o Alterations to waiting restrictions or other Traffic Regulation Orders to enable the works
- o Locations of lighting, signing, street furniture, street trees and pits
- o Structures on or adjacent to the highway
- o Extent of any stopping up or dedication of new highway (including all public rights of way shown on the definitive map and statement)

These works shall then be completed prior to first occupation of the development to the satisfaction of the Highway Authority and as approved in writing by the Local Planning Authority. For further information contact the Highway Authority's Transport Development Management Team by emailing transportdm@bristol.gov.uk

NB: Planning permission is not permission to work in the highway. A Highway Agreement under Section 278 of the Highways Act 1980 must be completed, the bond secured and the Highway Authority's technical approval and inspection fees paid before any drawings are considered and approved and formal technical approval must be obtained prior to any works being permitted.

Reason: In the interests of public safety and to ensure that all road works associated with the proposed development are planned and approved in good time to include any statutory processes, are undertaken to a standard approved by the Local Planning Authority, and are completed before occupation.

10. Highway Condition Survey

No development shall take place (including investigation work, demolition, siting of site compound/welfare facilities) until a survey of the condition of the adopted highway has been submitted to and been approved in writing by the Local Planning Authority. To agree the extent of the area to be surveyed contact the Highway Authority's Transport Development Management Team by emailing transportdm@bristol.gov.uk The survey must consist of:

A plan to a scale of 1:1000 showing the location of all defects identified;
A written and photographic record of all defects with corresponding location references accompanied by a description of the extent of the assessed area and a record of the date, time and weather conditions at the time of the survey.

N.B. No building or use hereby permitted shall be occupied or the use commenced until any damage to the adopted highway has been made good to the satisfaction of the Highway Authority.

Reason: To ensure that any damage to the adopted highway sustained throughout the development process can be identified and subsequently remedied at the expense of the developer.

11. Renewable energy

Prior to commencement of development, details of the PV panels and Air Source Heat Pumps (including the exact location, dimensions, design/ technical specification) together with calculation of energy generation and associated CO₂ emissions to achieve 20% reduction on residual emissions from renewable energy in line with the approved energy statement should be submitted to the Local Planning Authority and approved in writing. Prior to occupation of the dwellings photographic evidence of the installed renewable energy technologies shall be submitted and approved in writing by the local planning authority and the technologies shall be thereafter retained.

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Reason: To ensure that the development contributes to mitigating and adapting to climate change and to meeting targets to reduce carbon dioxide emissions

Pre occupation condition(s)

12. Completion and Maintenance of Car/Vehicle Parking - Shown on approved plans
No building or use hereby permitted shall be occupied or the use commenced until the car/vehicle parking area (and turning space) shown on the approved plans has been completed, and thereafter, the area shall be kept free of obstruction and available for the parking of vehicles associated with the development. Driveways/vehicle parking areas accessed from the adopted highway must be properly consolidated and surfaced (not loose stone or gravel) and subsequently maintained in good working order.

Reason: To ensure that there are adequate parking facilities to serve the development constructed to an acceptable standard.

13. Completion and Maintenance of Cycle Provision - Shown on approved plans
No building or use hereby permitted shall be occupied or the use commenced until the cycle parking provision shown on the approved plans has been completed, and thereafter, be kept free of obstruction and available for the parking of cycles only.

Reason: To ensure the provision and availability of adequate cycle parking.

14. Implementation/Installation of Refuse Storage and Recycling Facilities - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the refuse store, and area/facilities allocated for storing of recyclable materials, as shown on the approved plans have been completed in accordance with the approved plans.

Thereafter, all refuse and recyclable materials associated with the development shall either be stored within this dedicated store/area, as shown on the approved plans, or internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the public highway or pavement, except on the day of collection. All storage facilities must meet the standards set out within Bristol Waste's - Waste and Recycling: Collection and Storage Facilities - Guidance for Developers, Owners and Occupiers.

Reason: To safeguard the amenity of the occupiers of adjoining premises, protect the general environment, and prevent obstruction to pedestrian movement, and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

15. Noise Assessment

No commencement of use shall take place until an acoustic report has been submitted to and approved in writing by the Council.

The report shall include details of the existing noise climate at the development site, a scheme of noise insulation measures for all residential accommodation and an assessment of noise generated by any heating, refrigeration or ventilation plant as part of this development.

The noise assessment shall be carried out by a suitably qualified acoustic consultant/engineer and shall take into account the provisions of BS4142: 2014 Methods for rating and assessing

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industrial and commercial sound and BS 8233: 2014 Guidance on sound insulation and noise reduction for buildings.

The approved scheme shall be implemented prior to the commencement of the use and be permanently maintained thereafter.

16. Prior to the occupation of the residential units hereby permitted, the proposed flexible commercial units shall be completed in accordance with the approved plans.

Reason- In the interests of providing employment floorspace

17. Prior to the occupation of the residential units hereby permitted, details of the proposed marketing of the commercial units, (to include method and timing), shall be submitted and approved in writing by the Local Planning Authority and the units shall be marketed in accordance with this approval.

Reason- In the interests of providing employment floor space

18. Prior to occupation of the dwellings hereby approved there should be submitted and approved in writing by the Local Planning Authority a detailed layout of the PV panels to the roofs. This should be approved by an MCS, (Microgeneration Certification Scheme), accredited installer to ensure analysis of and shading is taken into account. The analysis shall include calculation of energy generation and associated CO₂ emissions to achieve 21% reduction on residual emissions from renewable energy in line with the approved energy statement. The PV panels shall be installed in prior to occupation of the dwellings and thereafter retained.

Reason- To address the need to reduce CO₂ emissions through the use of renewable energy.

19. Landscaping Scheme

Prior to first occupation of the development hereby permitted there shall be submitted to the local planning authority and approved in writing a phasing plan for the approved landscaping. The approved landscaping scheme shall be implemented in accordance with the approved phasing. All planted materials shall be maintained for five years and any trees or plants removed, dying, being damaged or becoming diseased within that period shall be replaced in the next planting season with others of similar size and species to those originally required to be planted unless the council gives written consent to any variation.

Reason: To protect and enhance the character of the site and the area, and to ensure its appearance is satisfactory.

20. High Speed Broadband provision

Prior to first occupation, evidence should be provided to the local planning authority that the development has been registered with a high speed broadband provider and a copy of the connectivity assessment provided.

Reason- To ensure the development is in accordance with policy BCS15, (Sustainable Design and Construction)

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21. Parking within the development site is to be restricted to the areas allocated on the approved plans shall not encroach onto areas allocated on the plans for other uses.

Reason: To control the level of parking on the site and to safeguard the uses of other areas.

22. Protection of parking and servicing provision

The areas allocated for vehicle parking, circulation and manoeuvring on the approved plans shall only be used for the said purpose and not for any other purposes.

Reason: To ensure the provision and availability of satisfactory off-street parking and servicing/loading/unloading facilities for the development.

23. Hours of Use

The proposed commercial building fronting onto St Gabriels Road shall not be occupied or used/open; other than between the hours 8.00am to 8.00pm Mondays to Fridays and between the hours of 9.00am and 4.00pm on Saturdays and Sundays and Bank holidays unless otherwise agreed in writing with the Local Planning Authority.

Reason: In the interests of protecting the amenity of nearby occupiers from noise and disturbance.

24. Privacy Screen

Prior to occupation, the proposed balcony privacy screens as shown on the approved plans (Drawing no. 1647(L)128 E) shall be erected in the position and to the height shown. The privacy screens shall be retained in the position and height shown in perpetuity. If damaged, they shall be repaired or replaced with a screen offering equivalent privacy unless otherwise agreed in writing by the Local Planning Authority.

Reason: To safeguard amenity and living conditions of future occupiers in accordance with Policy BCS21 of the Core Strategy and Policy DM30 of the Site Allocations and Development Management Policies Local Plan.

List of approved plans

25. List of approved plans and drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

105/1647 drawing list, received 14 February 2019

Reason: For the avoidance of doubt.

Advices

- 1 All species of wild birds, their eggs, nests and chicks are legally protected until the young have fledged. If site clearance or tree or hedge removal is undertaken on site whilst birds are nesting, which is typically between 1st March and 30th September inclusive, then a check is recommended beforehand by a qualified ecological consultant. Where checks for nesting

Development Control Committee B – 13 March 2019**Application No. 18/04108/F : St Gabriels Court St Gabriels Road Bristol BS5 0RT**

birds are required they should be undertaken by a qualified ecological consultant no more than 48 hours prior to the removal of vegetation or the demolition of, or works to buildings.

This proposal includes the demolition of structures. The following advisory note is recommended. All species of bats and their roosts are legally protected. If bats are encountered all demolition or construction work should cease and the Bat Conservation Trust (Tel 0845 1300 228) should be consulted for advice.

2 Works on the Public Highway

The development hereby approved includes the carrying out of work on the adopted highway. You are advised that before undertaking work on the highway you must enter into a highway agreement under Section 278 of the Highways Act 1980 with the Council, which would specify the works and the terms and conditions under which they are to be carried out. Contact the Highway Authority's Transport Development Management Team by emailing transportDM@bristol.gov.uk allowing sufficient time for the preparation and signing of the Agreement. You will be required to pay fees to cover the Councils cost's in undertaking the following actions:

- o Drafting the Agreement
- o A Monitoring Fee equivalent to 15% of the planning application fee
- o Approving the highway details
- o Inspecting the highway works

Traffic Regulation Order (TRO)

You are advised that a Traffic Regulation Order (TRO) is required. You must submit a plan to a scale of 1:1000 of an indicative scheme for a TRO, along with timescales for commencement and completion of the development. Please be aware that the statutory TRO process is not straightforward; involving the public advertisement of the proposal(s) and the resolution of any objections. You should expect a minimum of six months to elapse between the Highway Authority's TRO Team confirming that it has all the information necessary to enable it to proceed and the TRO being advertised. You will not be permitted to implement the TRO measures until the TRP has been sealed, and we cannot always guarantee the outcome of the process. We cannot begin the TRO process until the appropriate fee has been received. To arrange for a TRO to be processed contact the Highway Authority's Transport Development Management Team by emailing transportdm@bristol.gov.uk

N.B. The cost of implementing any lining and signing required by the TRO is separate to the TRO fees, which solely cover the administration required to prepare, consult, amend and seal the TRO.

Impact on the highway network during construction

The development hereby approved and any associated highway works required, is likely to impact on the operation of the highway network during its construction (and any demolition required). You are advised that before undertaking any work you must contact the Highway Authorities Network Management Team by emailing traffic@bristol.gov.uk to discuss any temporary traffic management measures required, such as footway, Public Right of Way or carriageway closures or temporary parking restrictions a minimum of eight weeks prior to any activity on site to enable Temporary Traffic Regulation Orders to be prepared and a programme of Temporary Traffic Management measures to be agreed.

Private Road

You are advised that as a result of the proposed layout and construction of the internal access

Development Control Committee B – 13 March 2019**Application No. 18/04108/F : St Gabriels Court St Gabriels Road Bristol BS5 0RT**

road, the internal access road will not be accepted for adoption by the Highway Authority under Section 38 of the Highways Act 1980. The development will be bound by Sections 219 to 225 (the Advance Payments Code) of the Highways Act 1980, unless and until you agree to exempt the access road. The exemption from adoption will be held as a Land Charge against all properties within the application boundary. Contact the Highway Authorities Transport Development Management Team by emailing DMengineering@bristol.gov.uk

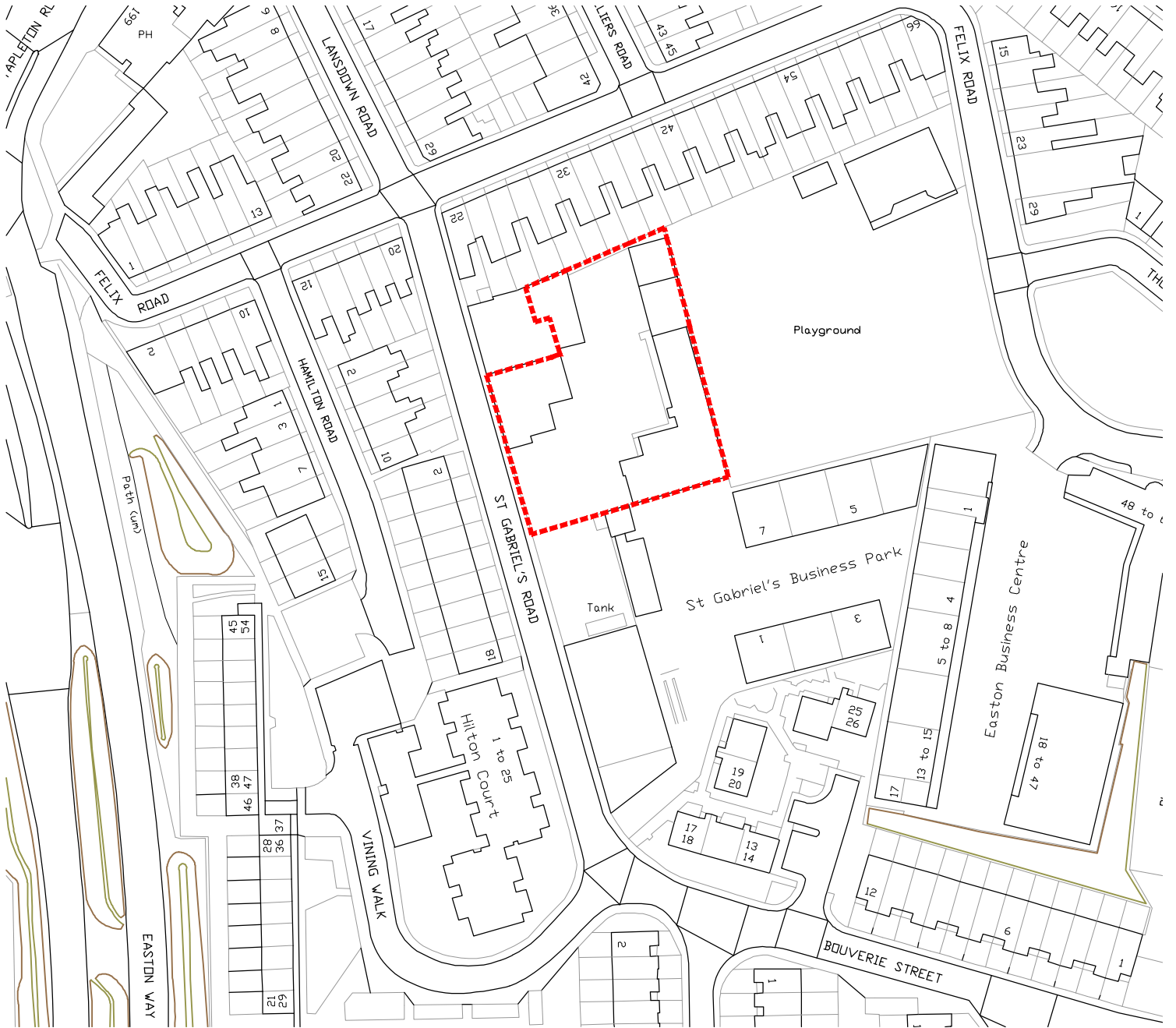
- 3 Advisory Note - PV design guidance:
The applicant is reminded that evidence that the PV design has been approved by an MCS (Microgeneration Certification Scheme) accredited installer to ensure shading is taken into account within the energy generation calculations should be submitted within energy statements and PV details.

commdelgranted
V1.0211

Supporting Documents

5. St Gabriels Court, St Gabriels Road

1. Document issue sheet
2. Site location plan
3. Existing site plan
4. Existing west & north elevations
5. Existing east & south elevations
6. Existing west elevations
7. Existing east elevations
8. Proposed site plan
9. Proposed side & rear elevations commercial building
10. Existing & proposed site section GG
11. Proposed north & west elevations
12. Proposed south & east elevations
13. Comparative shadow study



/	Issued for planning	30/07/2018
A	Text removed, layout amended	30/07/2018
REV	Note	Date



Project **St Gabriel's Court
Bristol**

Drawing Title **Location Plan**

Drawing No. **1647(L)100**

Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.

Scale @A4	drawn by	Date	Rev
1:1250	SO	27/07/18	A



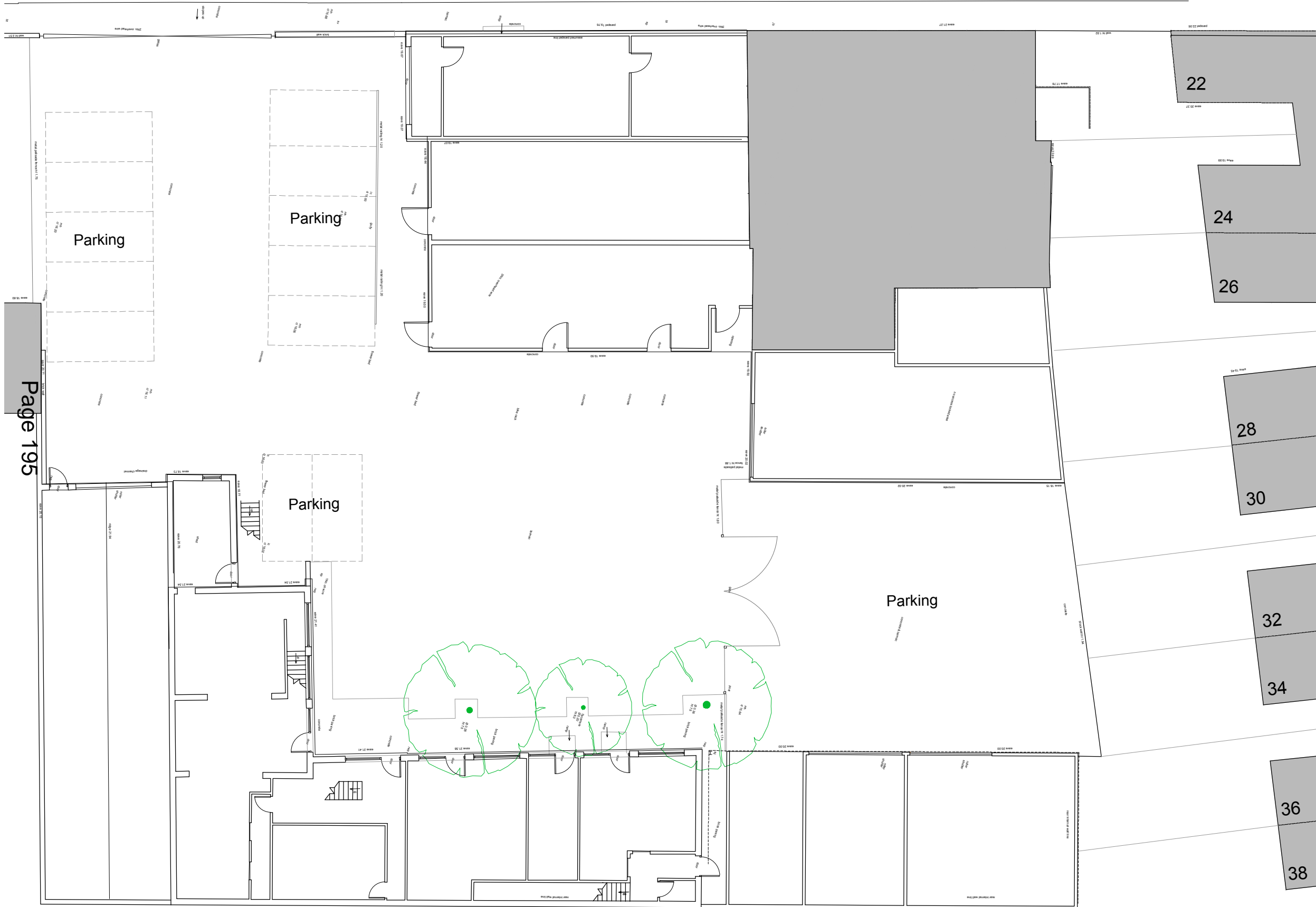


Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.



SITE ENTRANCE

St Gabriels road



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REV	Note	Date
/	Issued for planning	30/07/2018
A	Text added & layout amended	30/07/2018



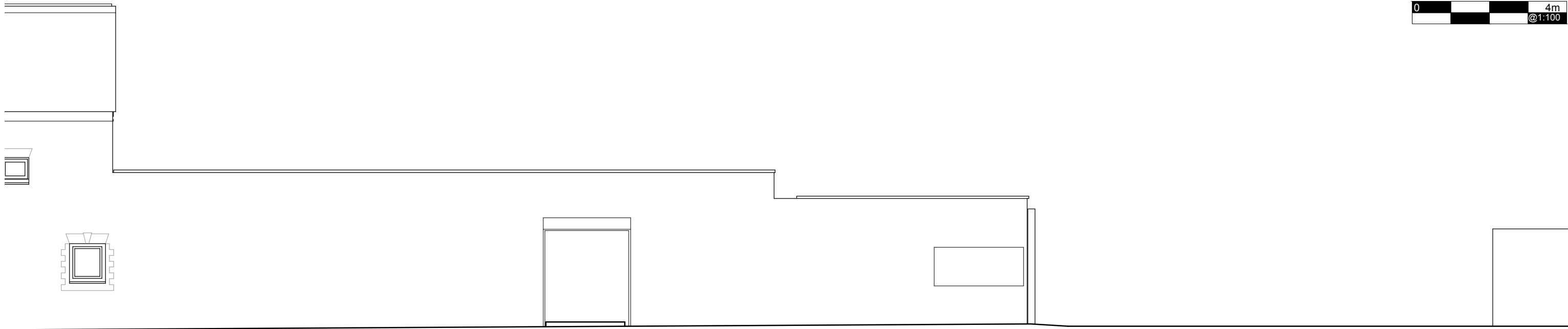
Project St Gabriel's Court, Bristol

Drawing Title Existing Site Plan

Drawing No. 1647(L)104

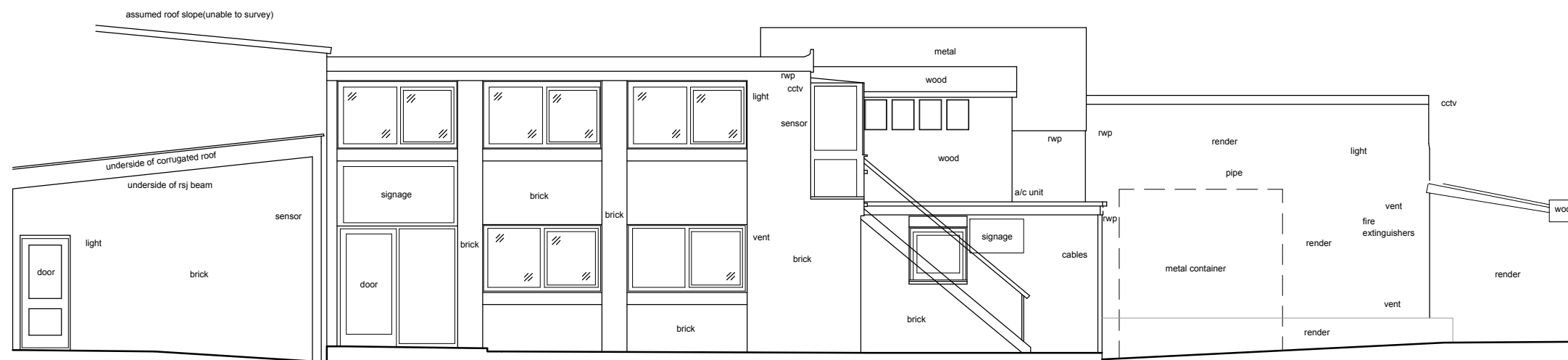
Scale @A3	drawn by	Date	Rev
1:200	SO	27/07/18	A

Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.



Existing West Elevation (St. Gabriel's Road)

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Existing North Elevation

/ Issued for planning 30/07/2018
REV Note Date



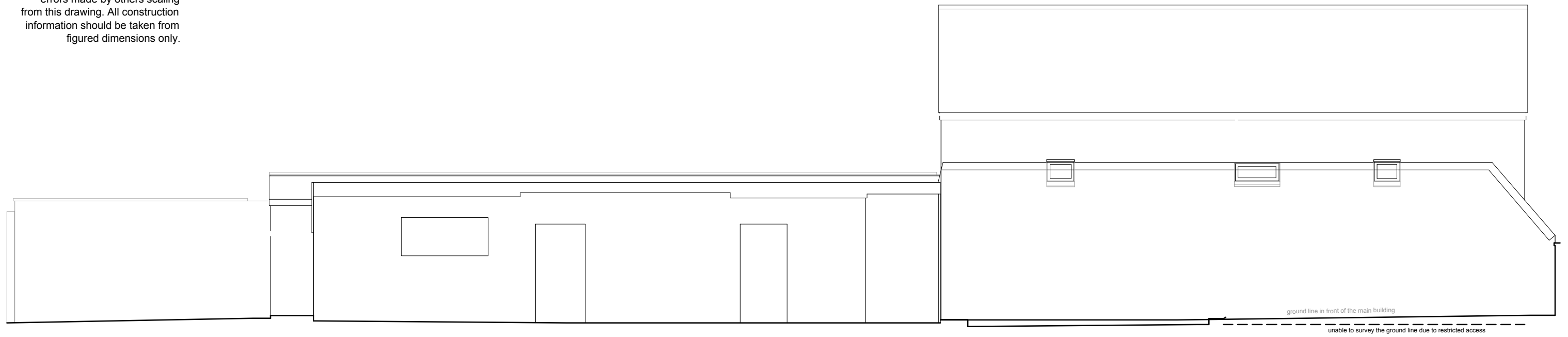
Project St Gabriel's Court, Bristol

Drawing Title Existing West & North Elevations

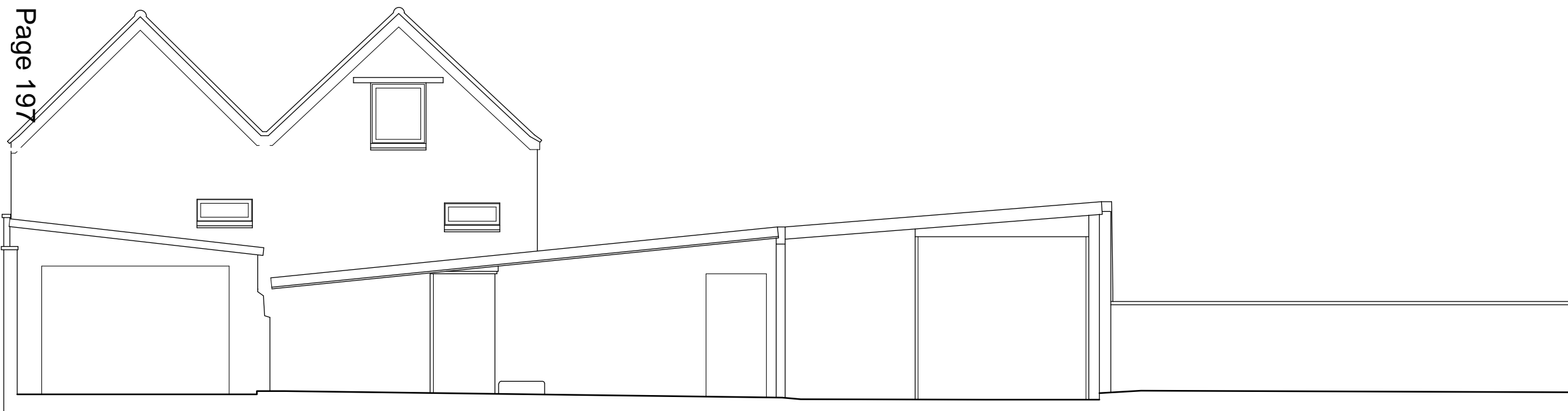
Drawing No. 1647(L)105

Scale @A3 drawn by Date Rev
1:100 SO 27/07/17 /

Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.



Existing East Elevation



Existing South Elevation

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/	Issued for planning	30/07/2018
REV	Note	Date



Project **St Gabriel's Court, Bristol**

Drawing Title **Existing East & South Elevations**

Drawing No. **1647(L)106**

Scale @A3	drawn by	Date	Rev
1:100	SO	27/07/18	-

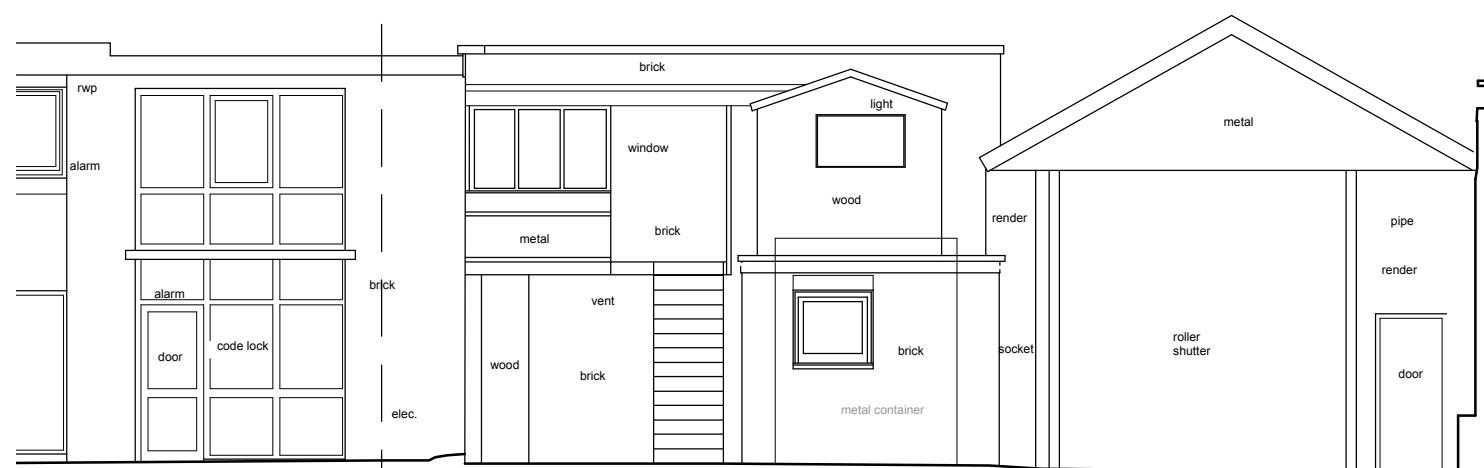


Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.



Existing West Elevation - 1:100

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Existing West Elevation - 1:100

/ Issued for planning 30/07/2018
 REV Note Date



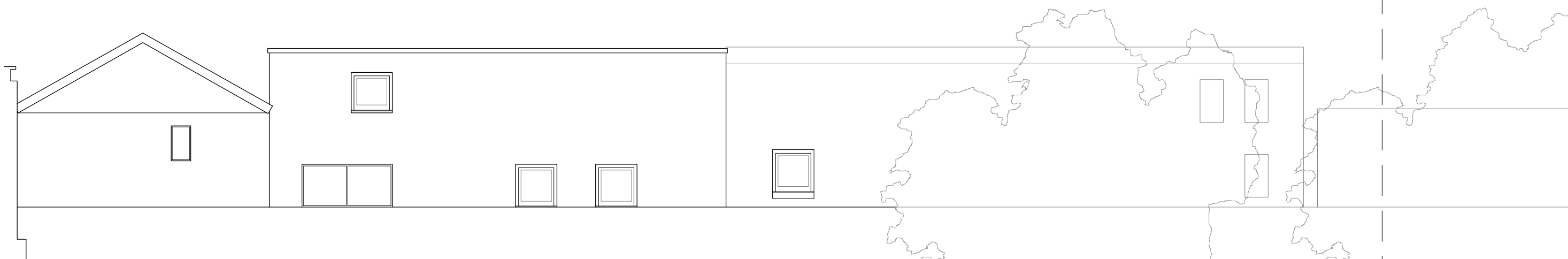
Project St Gabriel's Court, Bristol

Drawing Title Existing West Elevations

Drawing No. 1647(L)107

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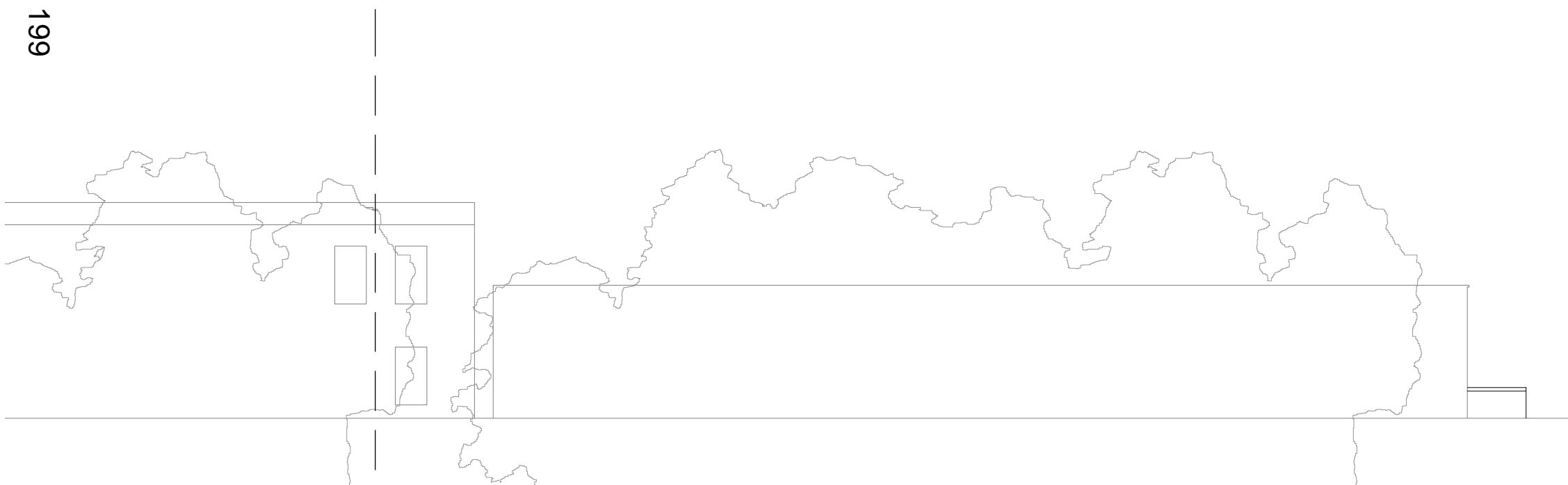
Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.



ELEVATION OBSCURED BY TREES

Existing East Elevation (towards Felix Road Adventure Playground) - 1:100

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ELEVATION OBSCURED BY TREES

Existing East Elevation (towards Felix Road Adventure Playground) - 1:100

REV	Note	Date
/	Issued for planning	30/07/2018



Project **St Gabriel's Court, Bristol**

Drawing Title **Existing East Elevation**

Drawing No. **1647(L)108**

Scale @A3	drawn by	Date	Rev
1:100	SO	30/07/18	-



Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.

FLOOR AREAS	
TYPICAL GROSS HOUSE:	99.3m ²
HOUSE no 10:	93m ²
COMMERCIAL GROUND FLOOR:	197m ²
COMMERCIAL FIRST FLOOR:	188m ²
COMMERCIAL SECOND FLOOR:	154m ²
BIKE STORE:	9.57m ²

- Rain water butt
- Air Source Heat Pump



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REV	Note	Date
/	Issued for planning	30/07/2018
A	External trees removed, layout amended & text added	30/07/2018



Project **St Gabriel's Court, Bristol**

Drawing Title **Proposed Site Plan**

Drawing No. **1647(L)110**

Scale @A3 drawn by SO Date 27/07/18 Rev H

REV	Note	Date
H	Visibility splay revised, bike stands added, notes added	07/02/2019
G	Bike store of commercial element relocated to fit heating system. Heat pumps added to the back garden of units.	08/01/2019

REV	Note	Date
F	Internal layout of commercial element revised	10/12/2018
E	Section titles revised	6/12/2018
D	Site layout revised to updated survey informations	5/12/2018
C	House 10 floor plan revised	23/10/2018
B	House 10 reduced to two stories, parking space removed, landscaping revised, entrance to bike stores secured with gates, paving of house entrance & parking spaces revised	16/10/2018

Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.

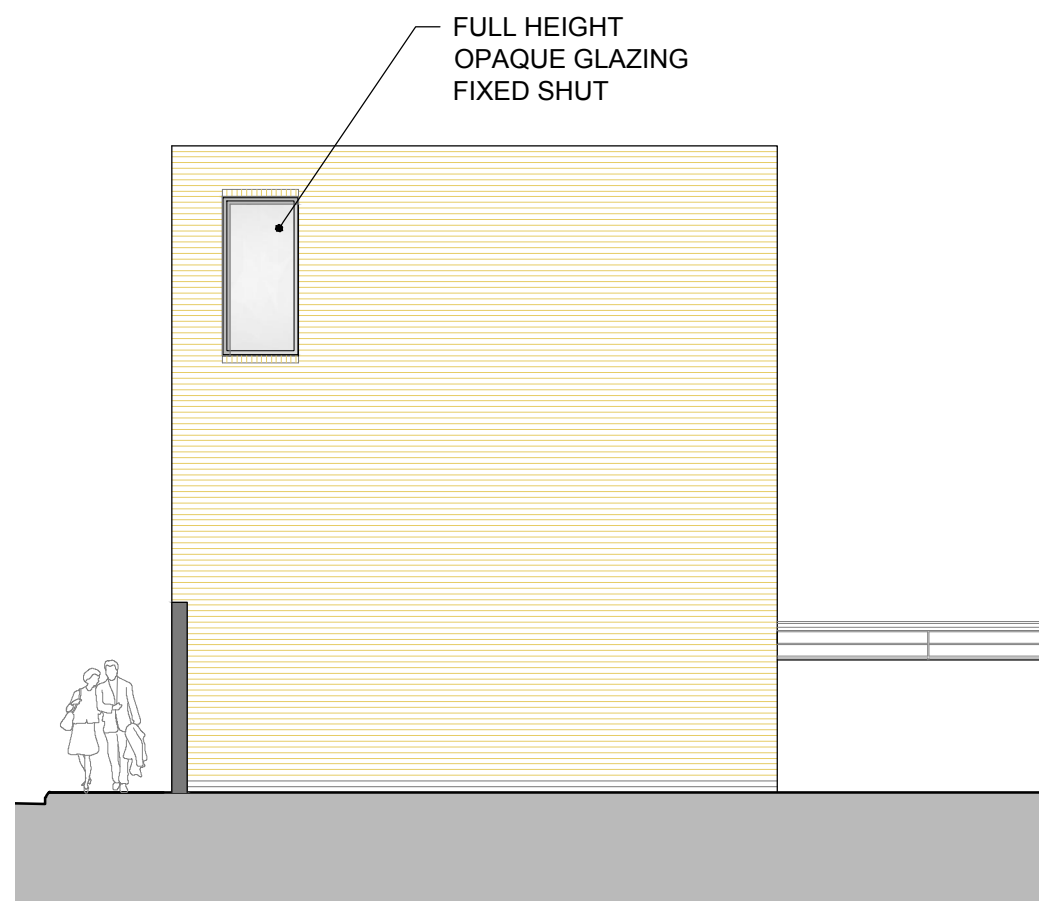


Proposed Rear Elevation (East)

Page 201



Proposed North Elevation



Proposed South Elevation

MATERIALS	
ROOF:	- Trocal single ply membrane with standing seam
WINDOW:	- PCC Aluminium RAL 7016
WALL:	- Clay Facing brick, buff - Through colour render, white

REV	Note	Date
B	Note added on Proposed South Elevation	5/12/2018
A	North Elevation revised, landscaping reviewed, entrance to bike store secured with gate	17/10/2018
/	Issued for planning	30/07/2018



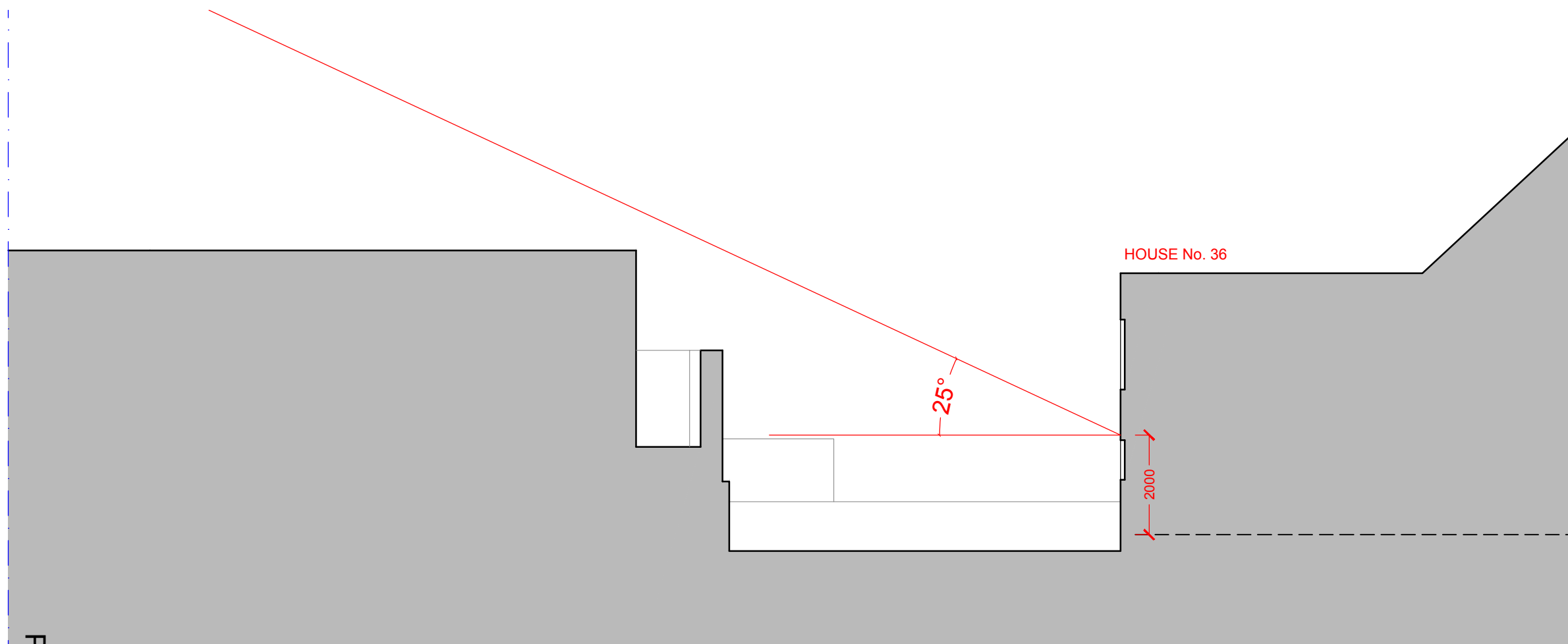
Project St Gabriel's Court, Bristol

Drawing Title Proposed Rear & Side Elevations of Commercial

Drawing No. 1647(L)122

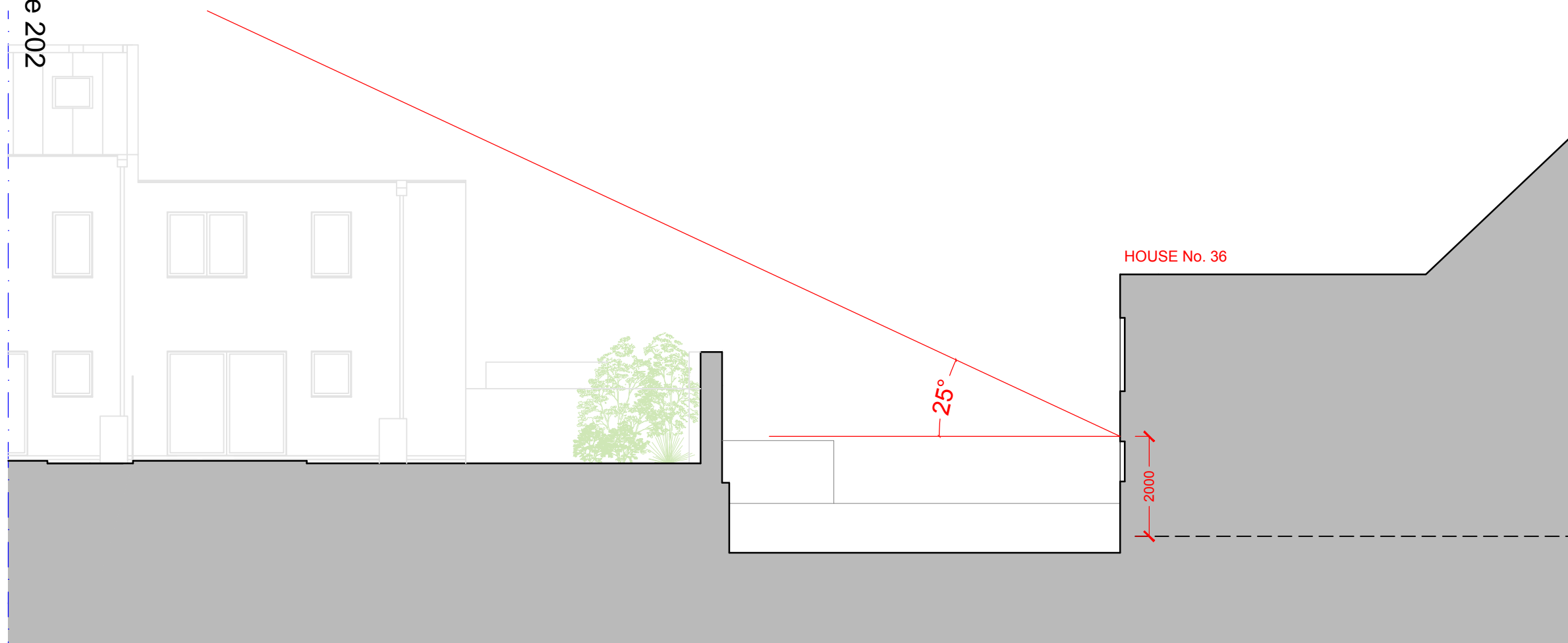
Scale @A3 drawn by SO Date 27/07/18 Rev B

Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.



Existing Site Section GG 1:100

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Proposed Site Section GG 1:100

REV	Note	Date
B	Proposed section GG revised	10/12/208
A	Section title revised	06/12/208



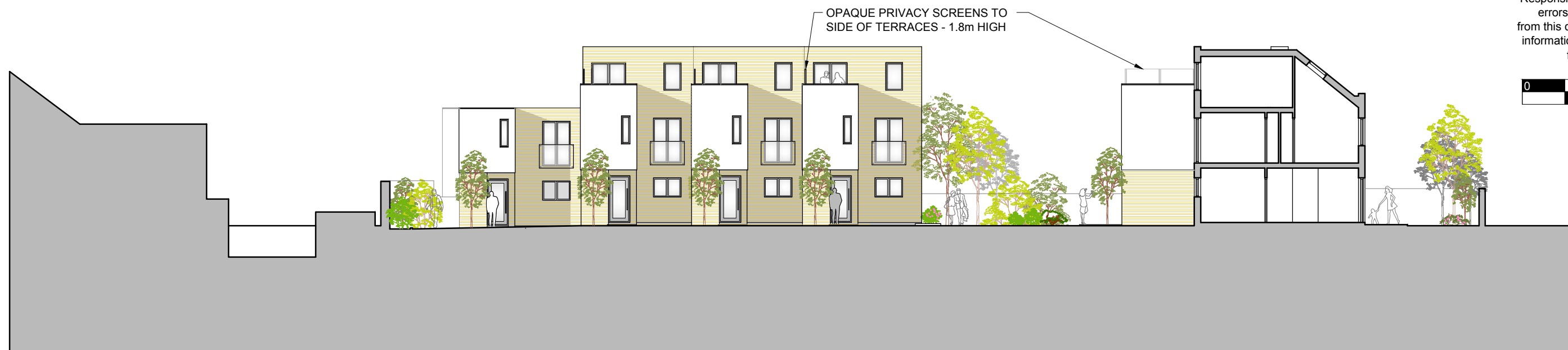
Project St Gabriel's Court, Bristol

Drawing Title Existing and Proposed Site Section GG

Drawing No. 1647(L)132

Scale @A3 drawn by SO Date 05/12/18 Rev B

Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.



Proposed Front West Elevation - AA



Proposed Front North Elevation - BB

MATERIALS
ROOF: - Trocal single ply membrane with standing seam
WINDOW: - PCC Aluminium RAL 7016
WALL: - Clay Facing brick, buff - Through colour render, white



Project **St Gabriel's Court, Bristol**

Drawing Title **Proposed Front North & West Elevations**

Drawing No. **1647(L)128**

REV	Note	Date
E	Privacy screens added to side of terraces	08/01/2019
D	Amendments to Front West Elevation	10/12/2018
C	Front West Elevation updated to new survey information	5/12/2018
B	Front West Elevation revised	19/10/2018
A	Landscaping revised, entrance to bike store secured with gate, Front West Elevation amended	18/10/2018
/	Issued for planning	30/07/2018

Scale @A3 drawn by Date Rev
1:200 SO 27/07/18 E

Responsibility is not accepted for errors made by others scaling from this drawing. All construction information should be taken from figured dimensions only.



Proposed East Elevation - CC

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Proposed South Elevation - DD

REV	Note	Date
E	Air source heat pump added to the back elevation of all units.	08/01/2019
D	South Elevation - DD & East Elevation - CC updated	10/12/2018
C	East Elevation - CC updated to new survey information	5/12/2018
B	Two windows at the end of back elevation removed	23/10/2018
A	House 10 reduced to two stories	17/10/2018
/	Issued for planning	30/07/2018



Project **St Gabriel's Court, Bristol**

Drawing Title **Proposed East & South Elevations**

Drawing No. **1647(L)129**

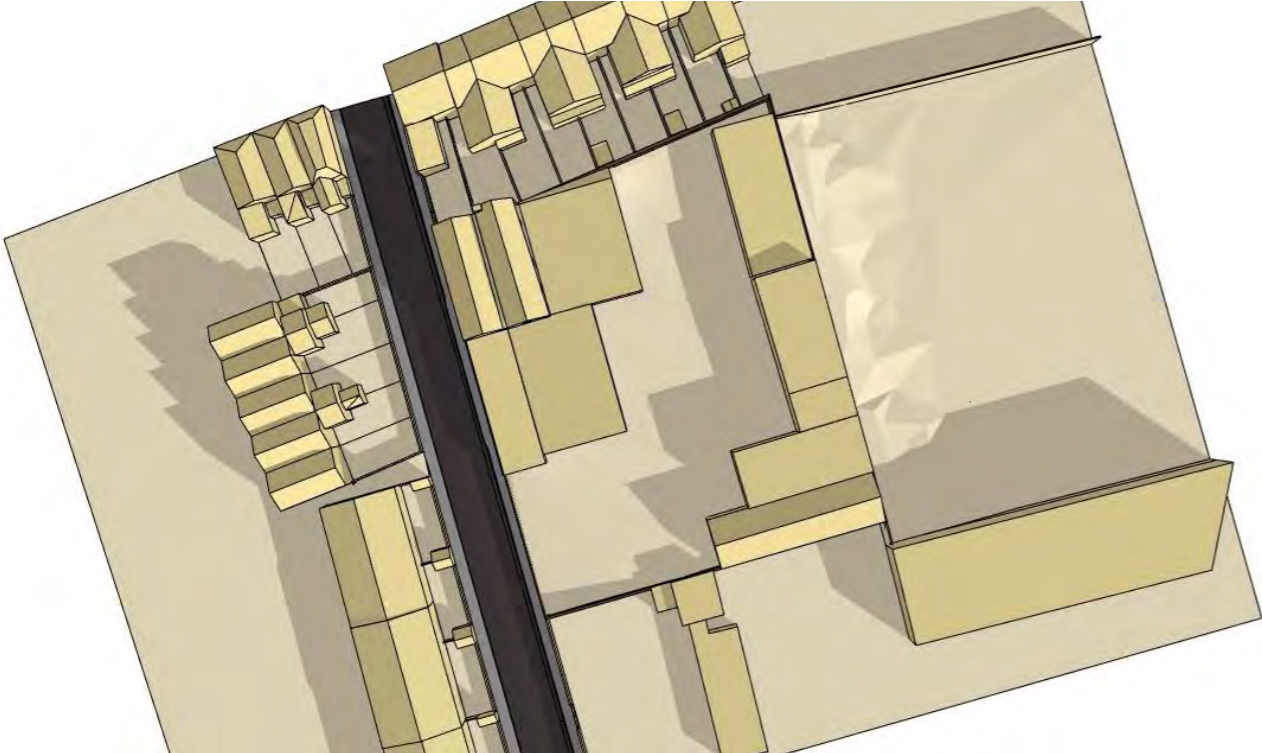
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MATERIALS
ROOF: - Trocal single ply membrane with standing seam
WINDOW: - PCC Aluminium RAL 7016
WALL: - Clay Facing brick, buff - Through colour render, white

1647 - ST. GABRIEL'S COURT
COMPARATIVE SHADOW STUDY
12/12/2018
Rev A - 6/02/1019

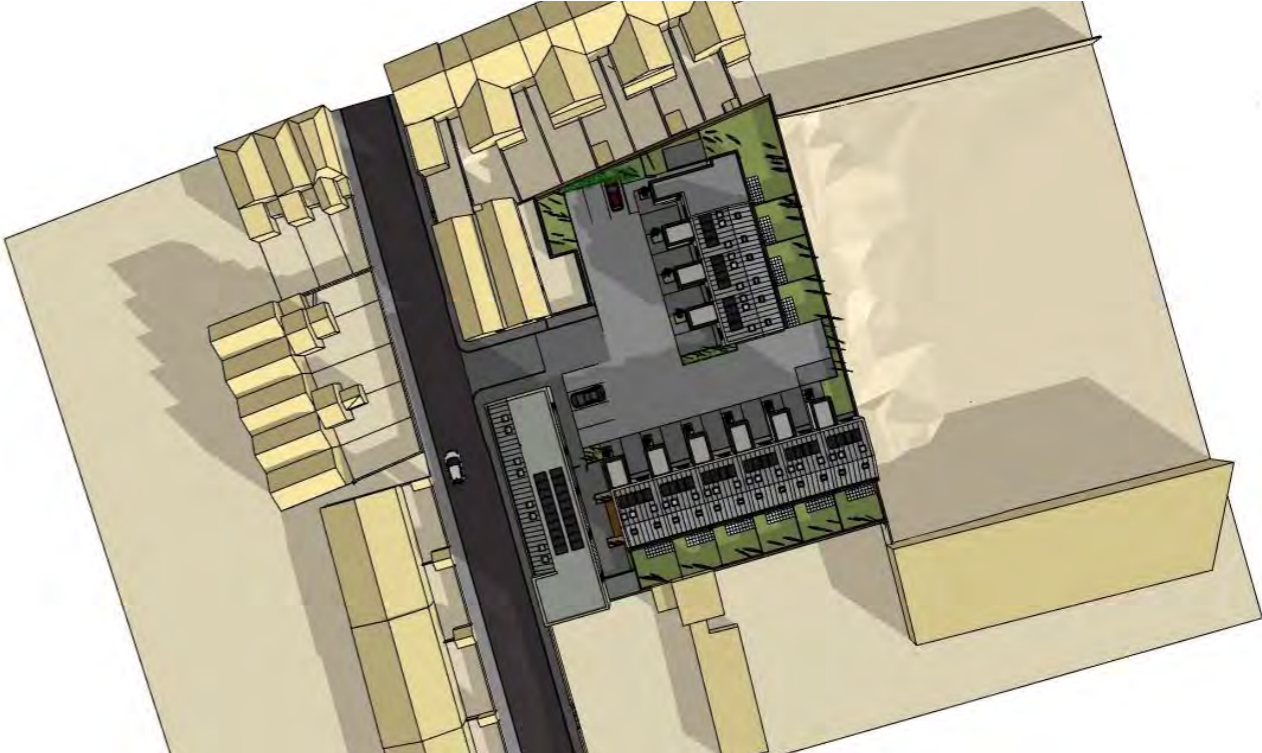
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MARCH - 9am



PROPOSED

21st MARCH - 9am



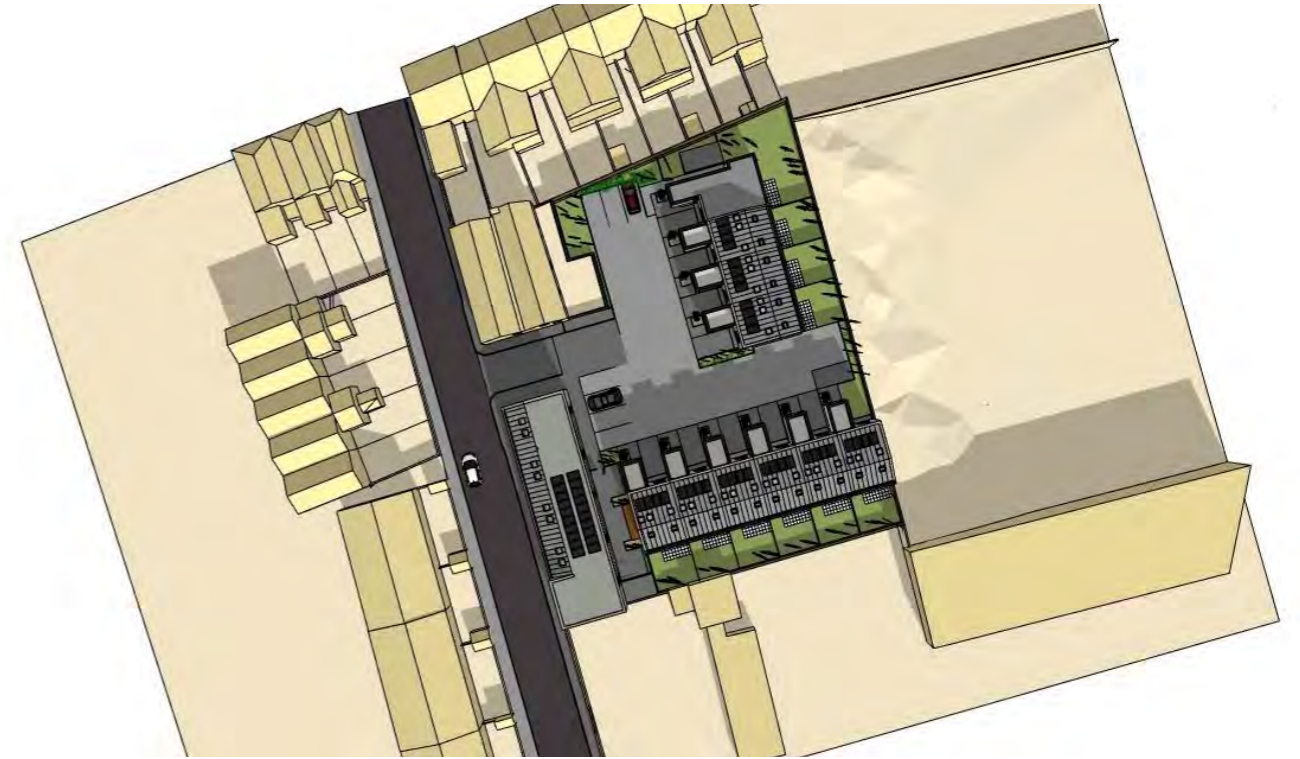
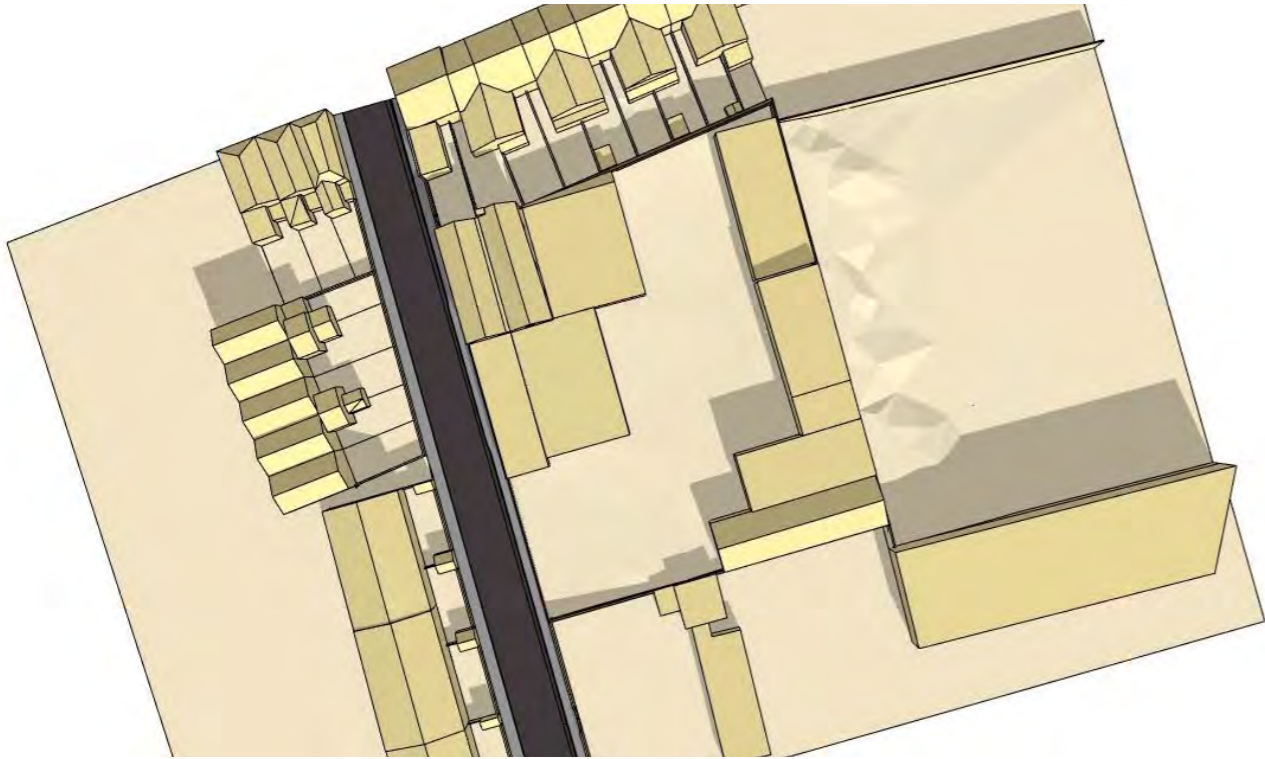
Page 205

EXISTING

PROPOSED

21st MARCH - 11am

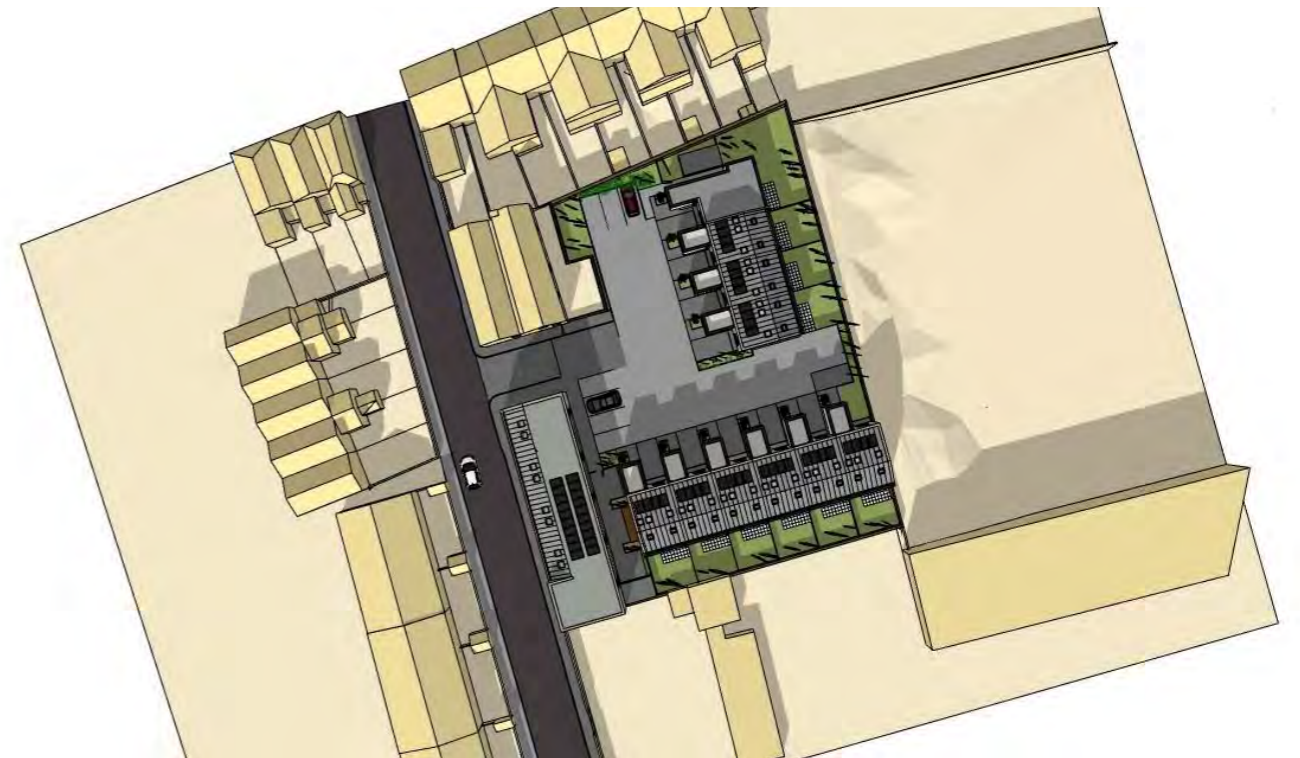
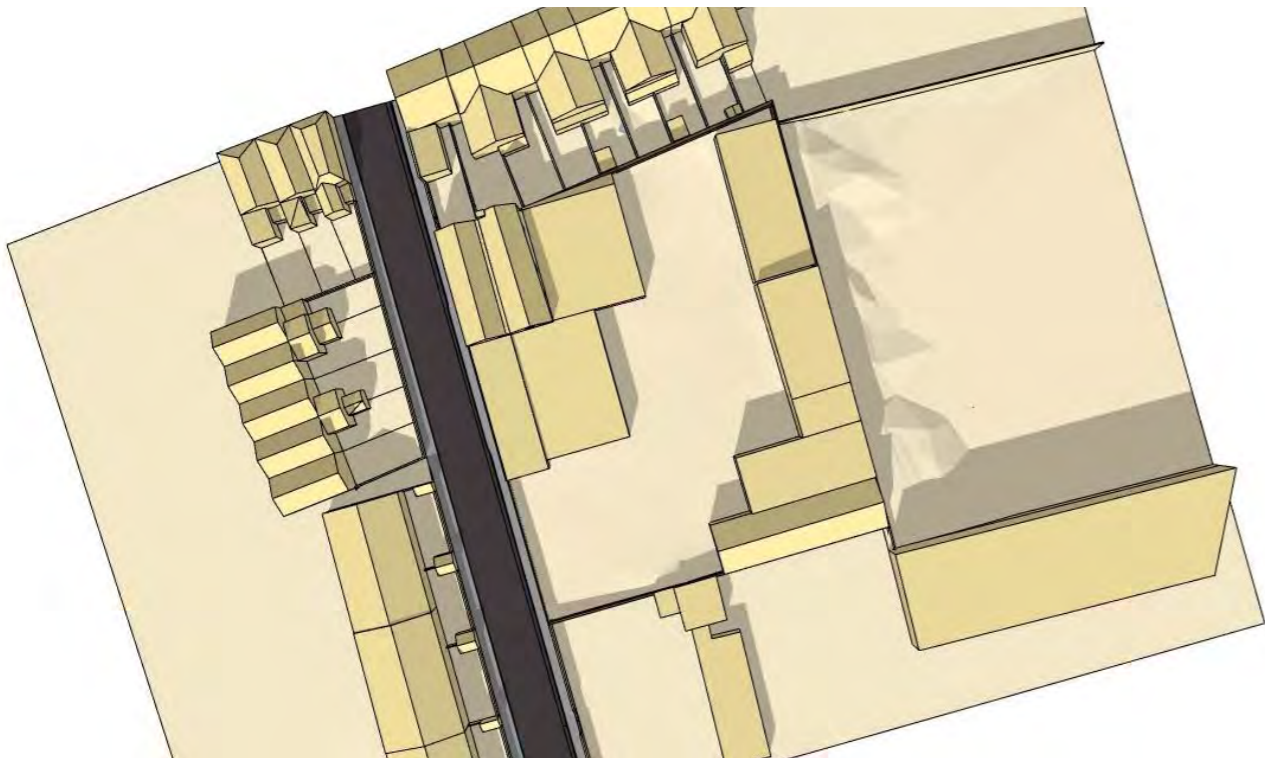
21st MARCH - 11am



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21st MARCH - 1pm

21st MARCH - 1pm

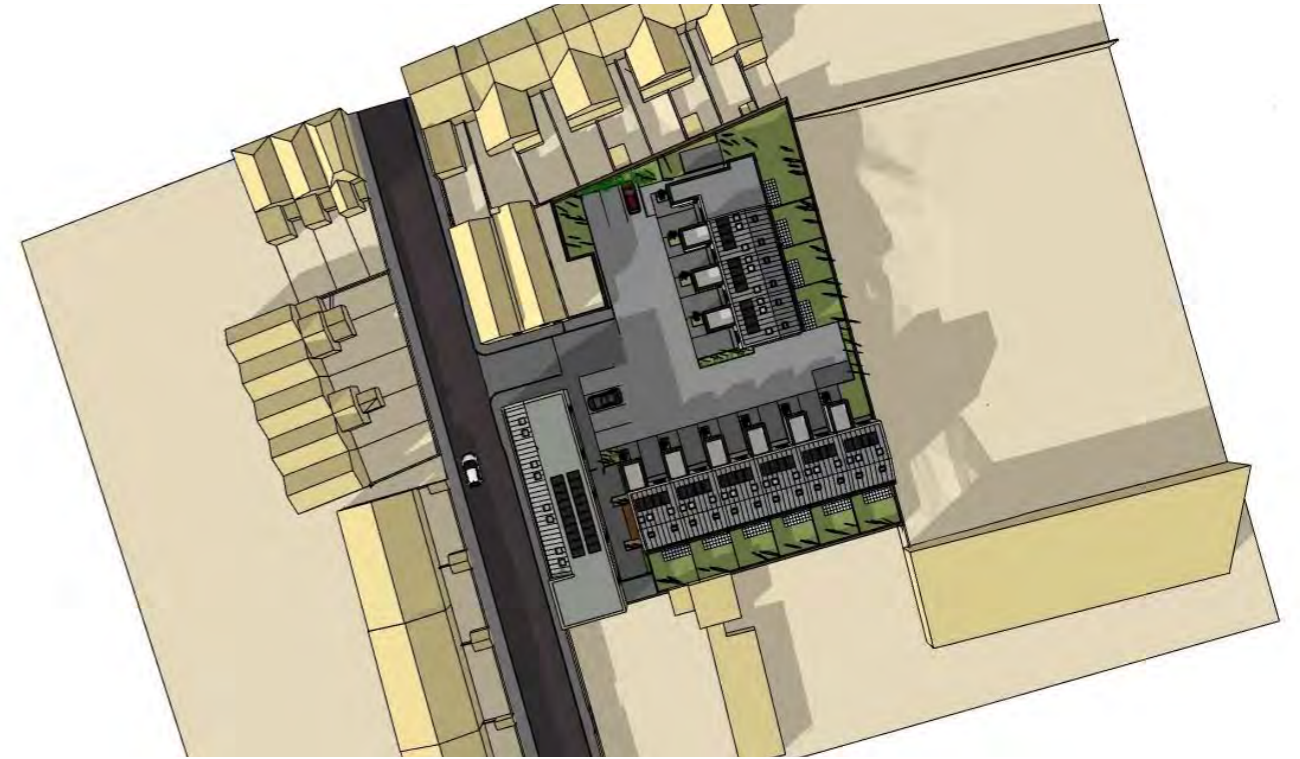
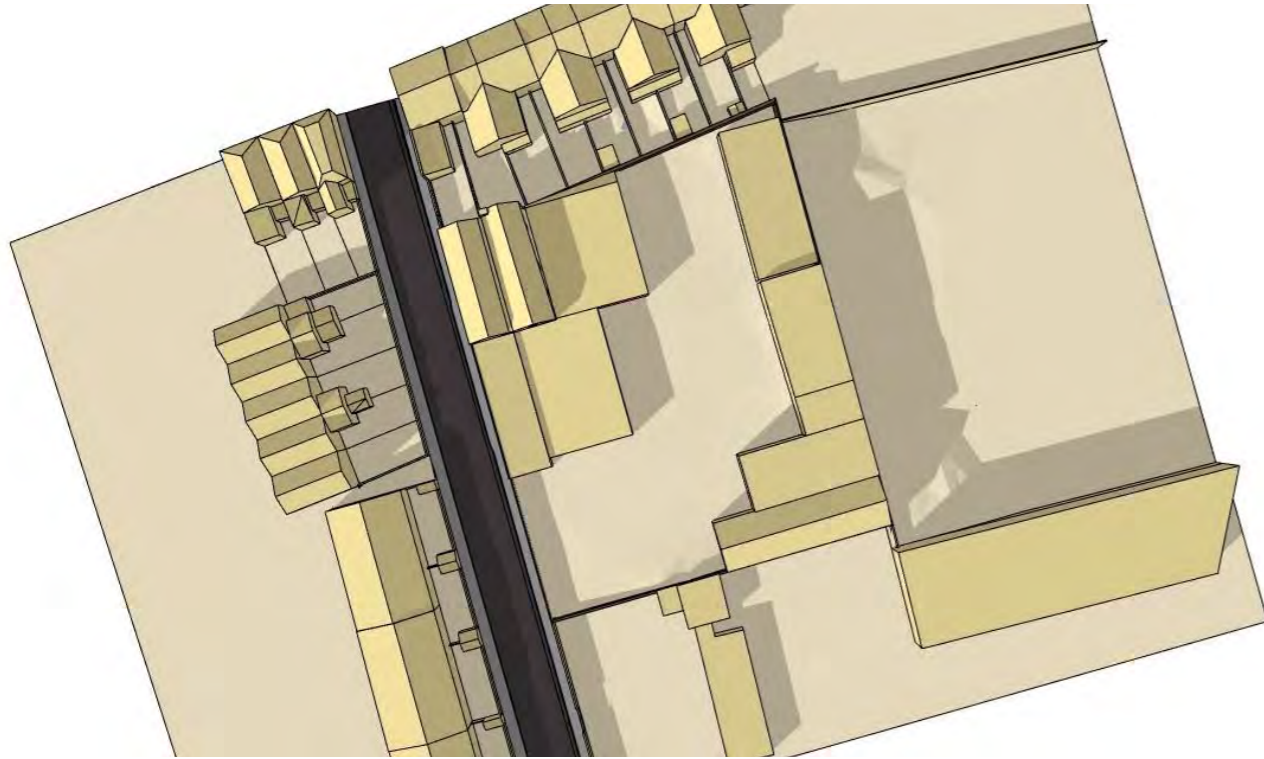


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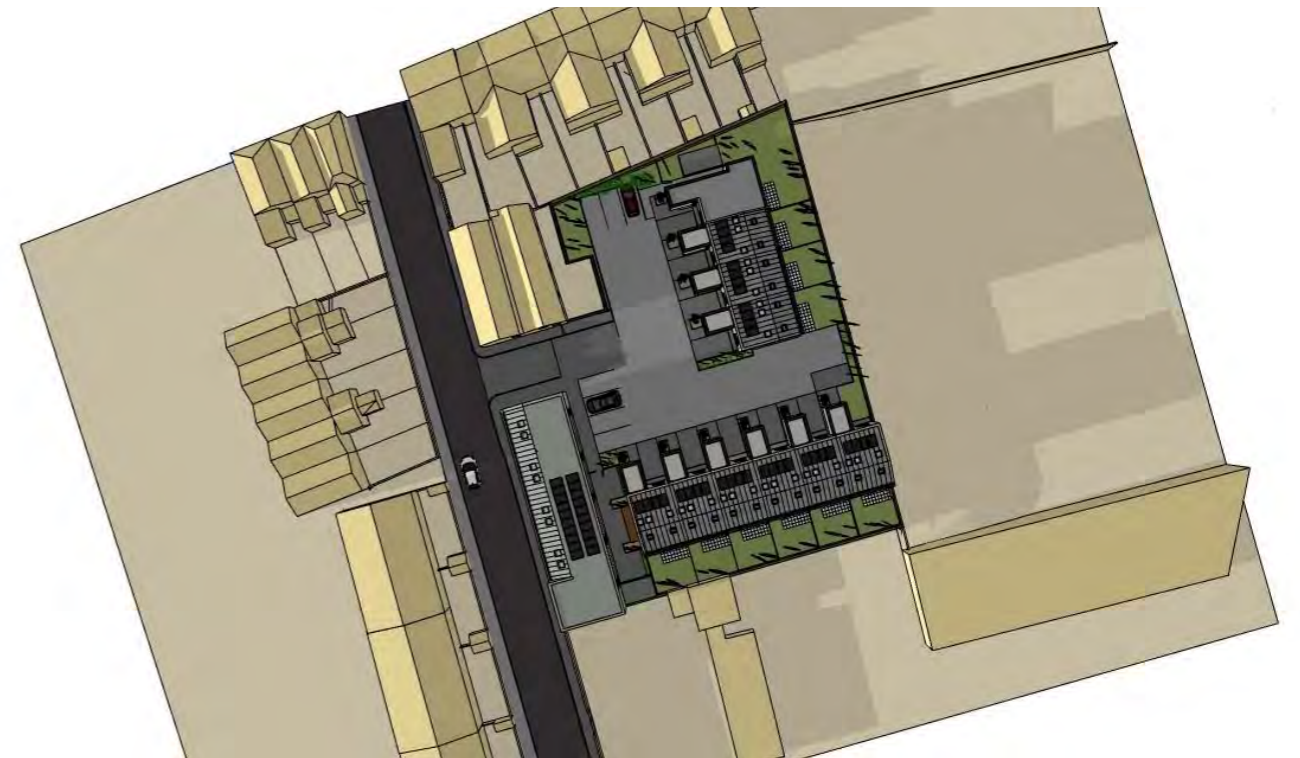
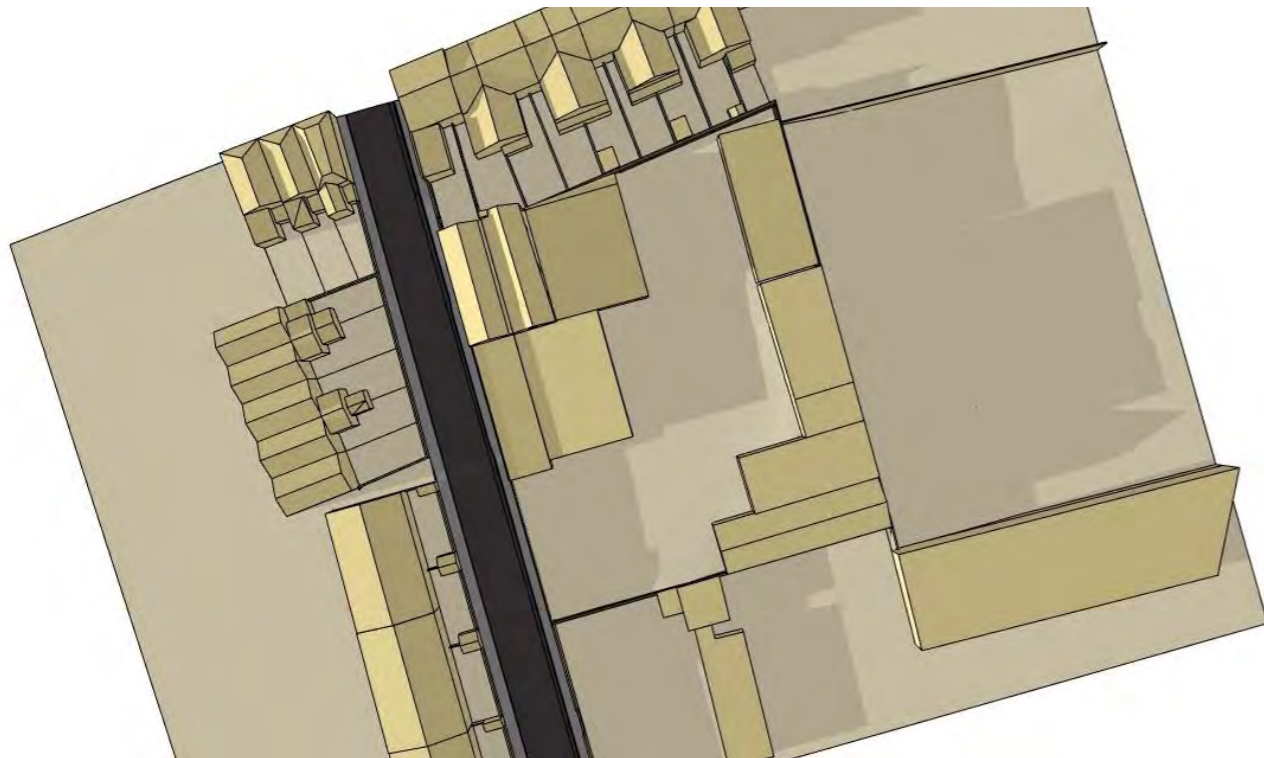
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21st MARCH - 5pm

21st MARCH - 5pm

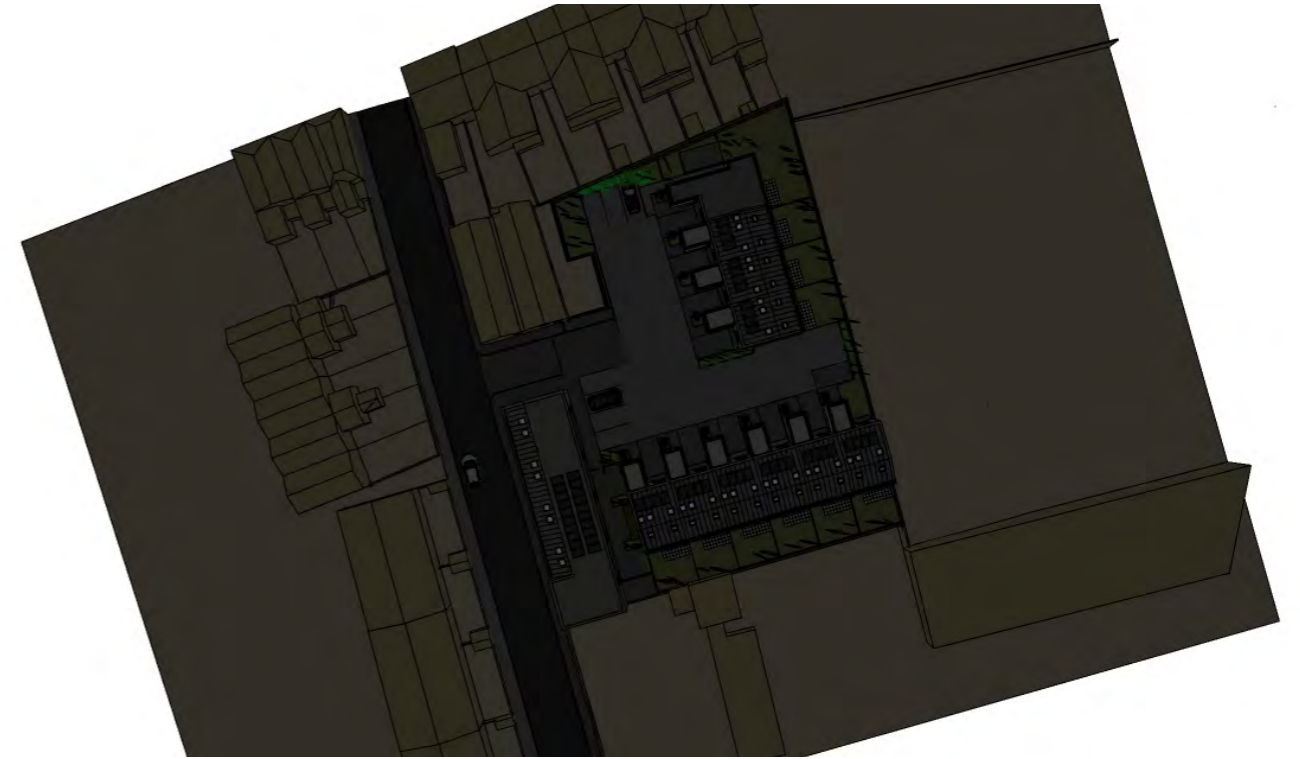
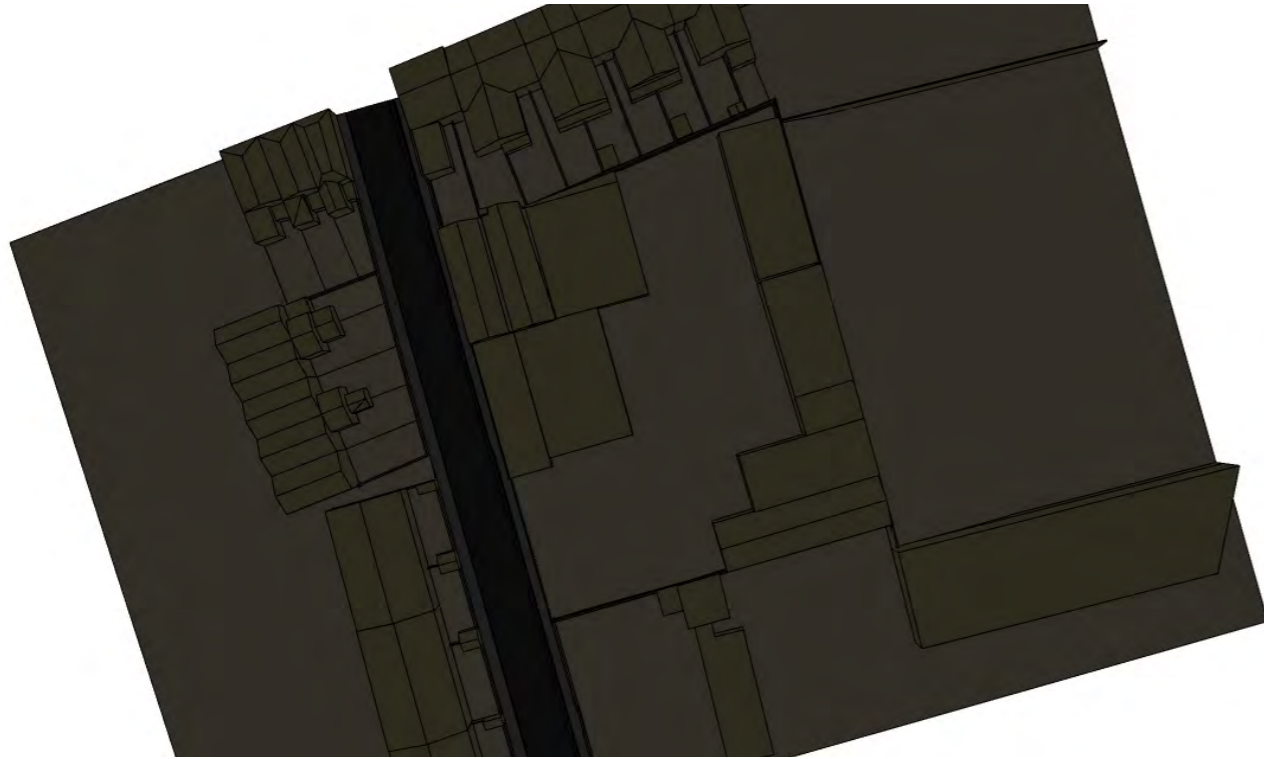


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PROPOSED

21st MARCH - 7pm

21st MARCH - 7pm

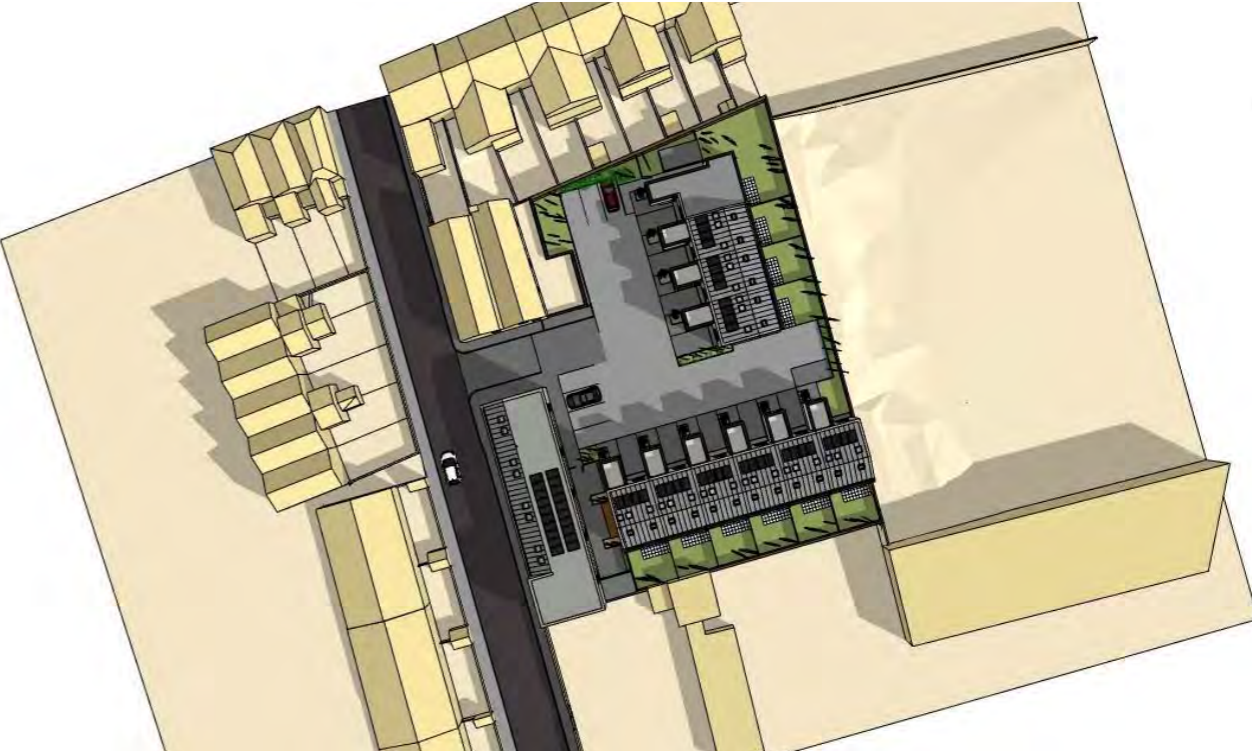
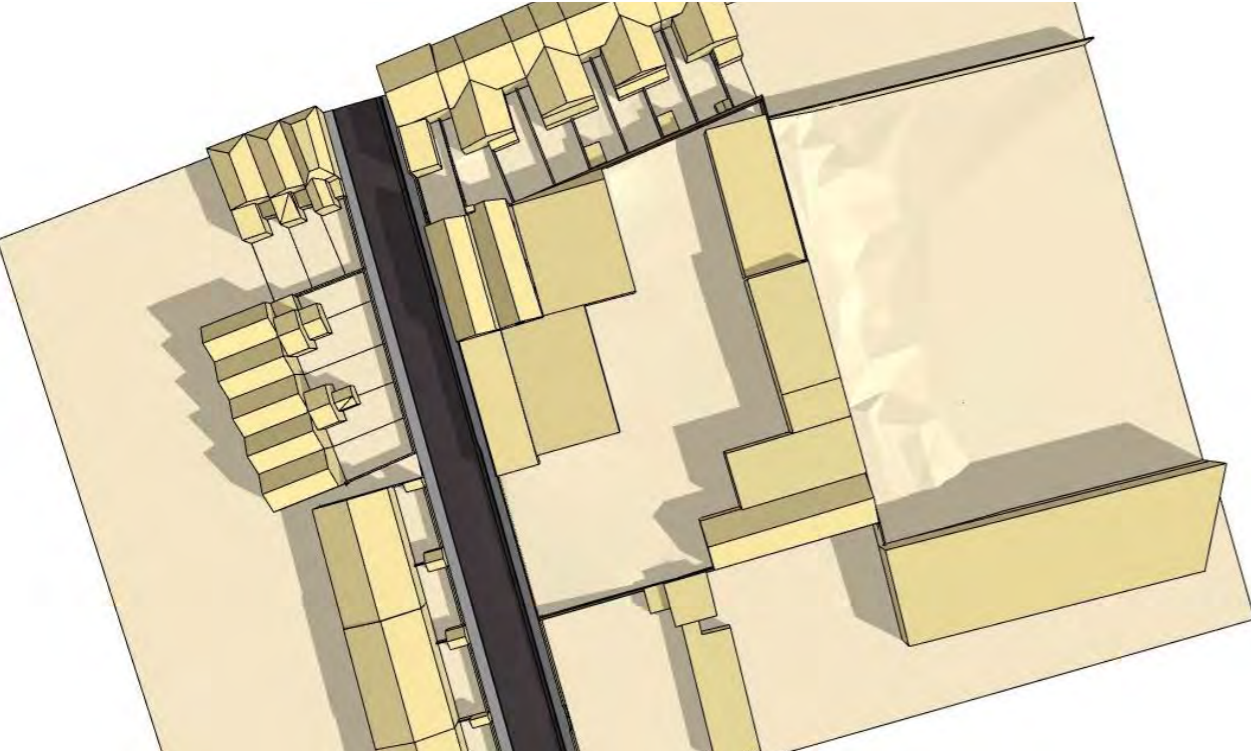


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PROPOSED

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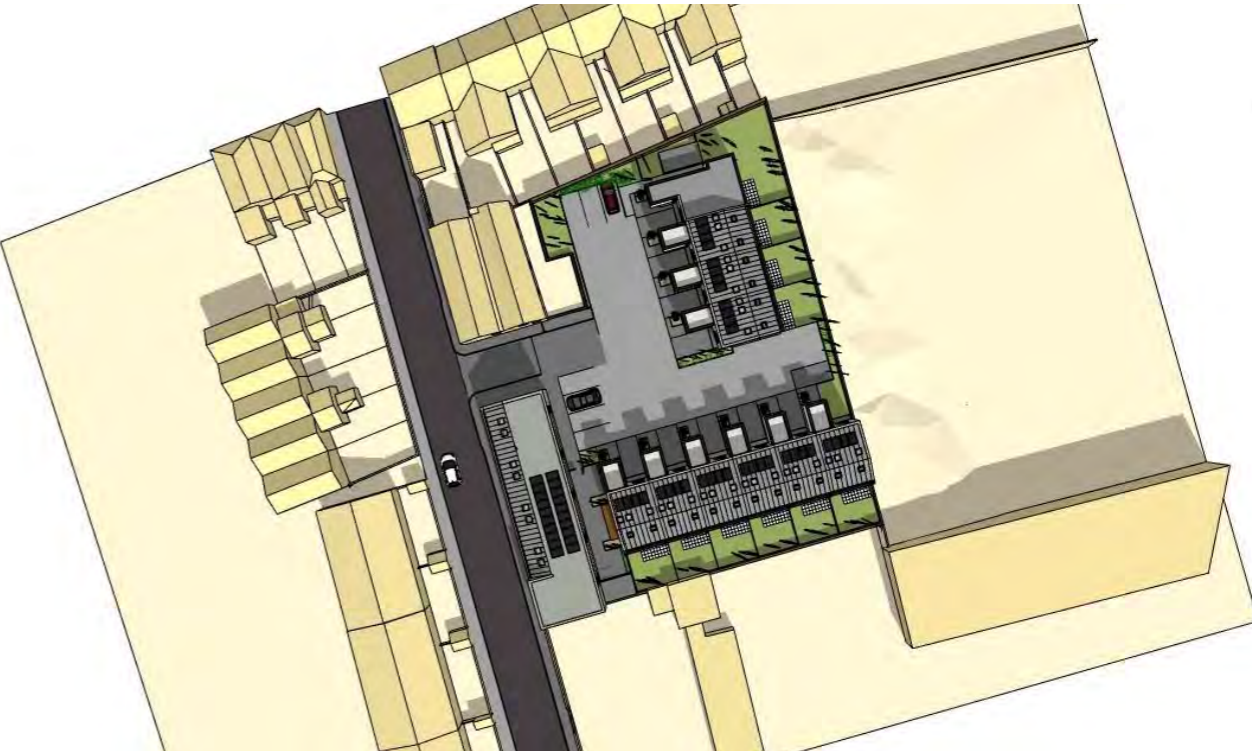
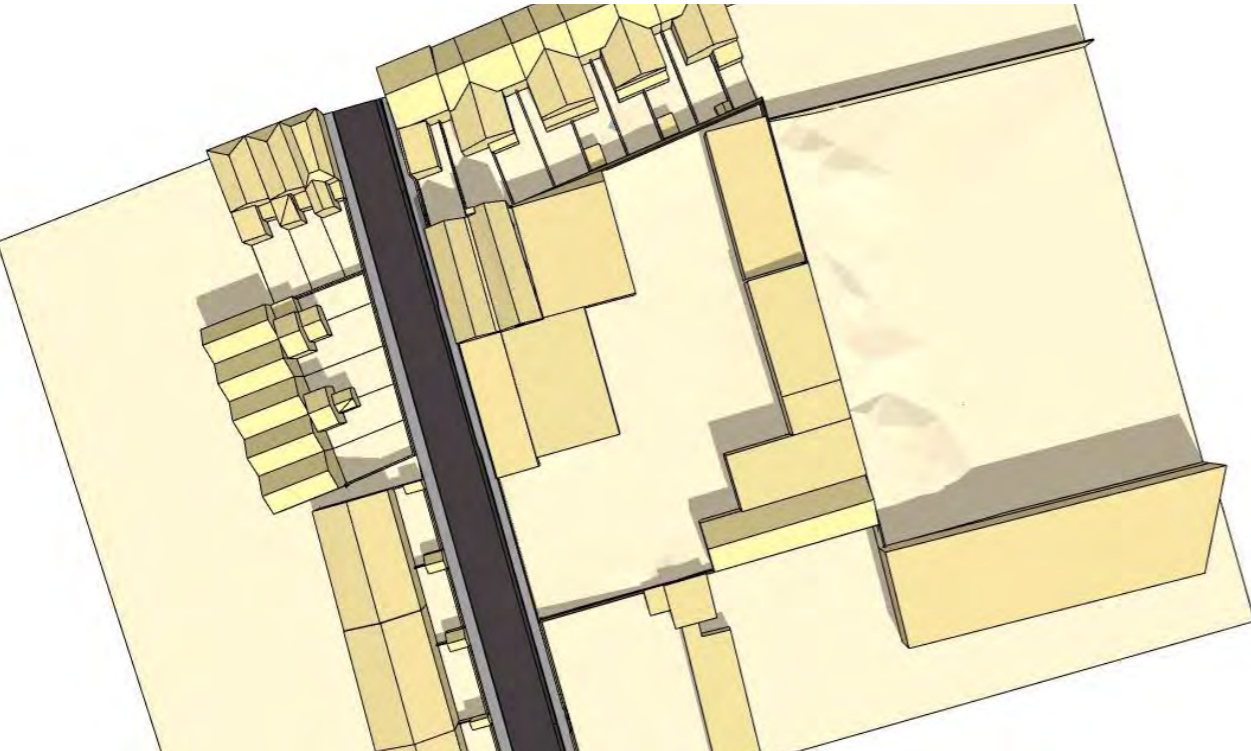
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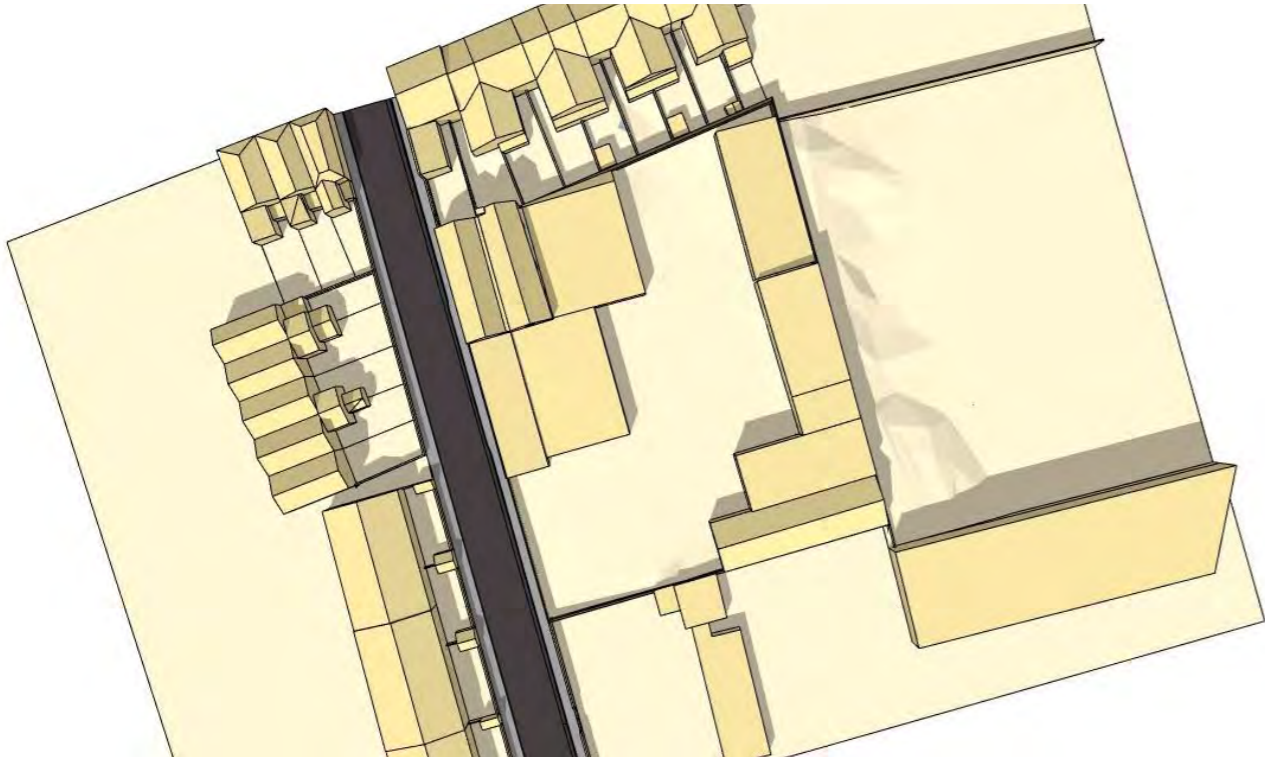
MAY - 11am

1st MAY - 11am



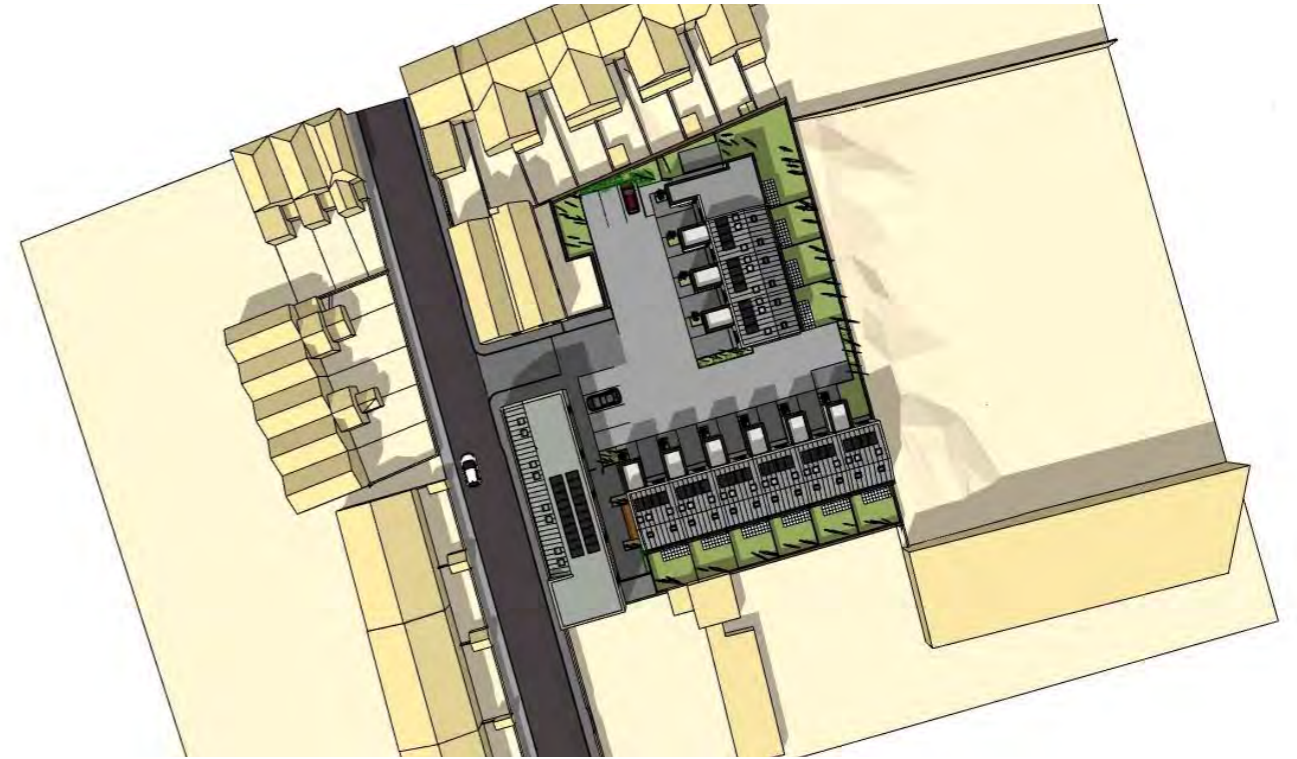
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1st MAY - 1pm



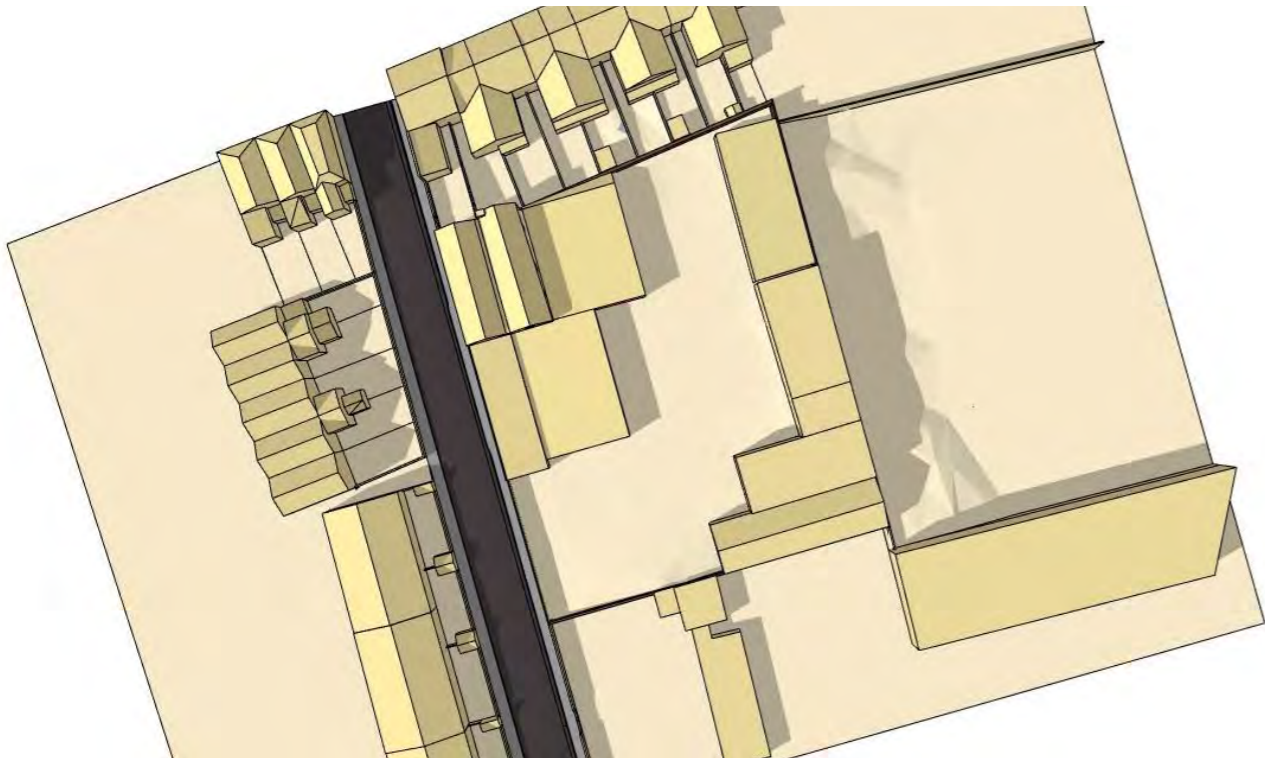
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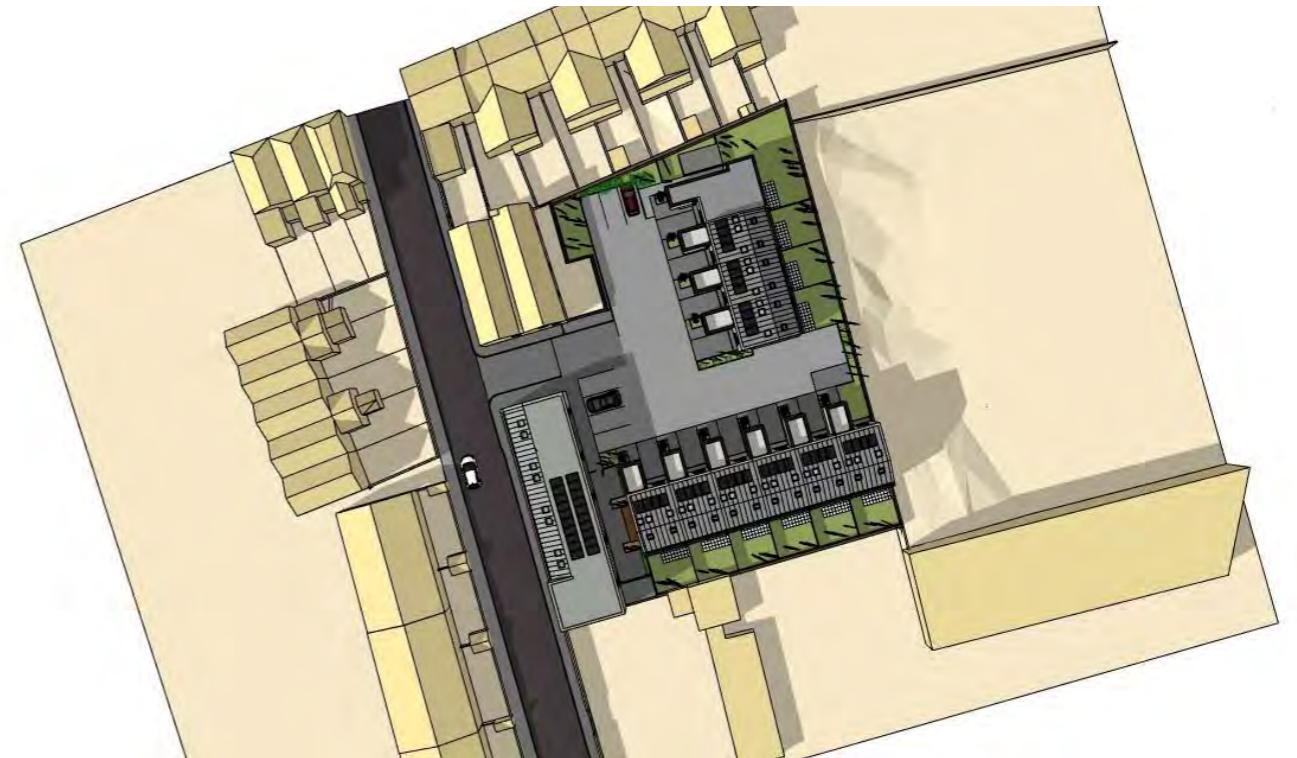


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MAY - 3pm

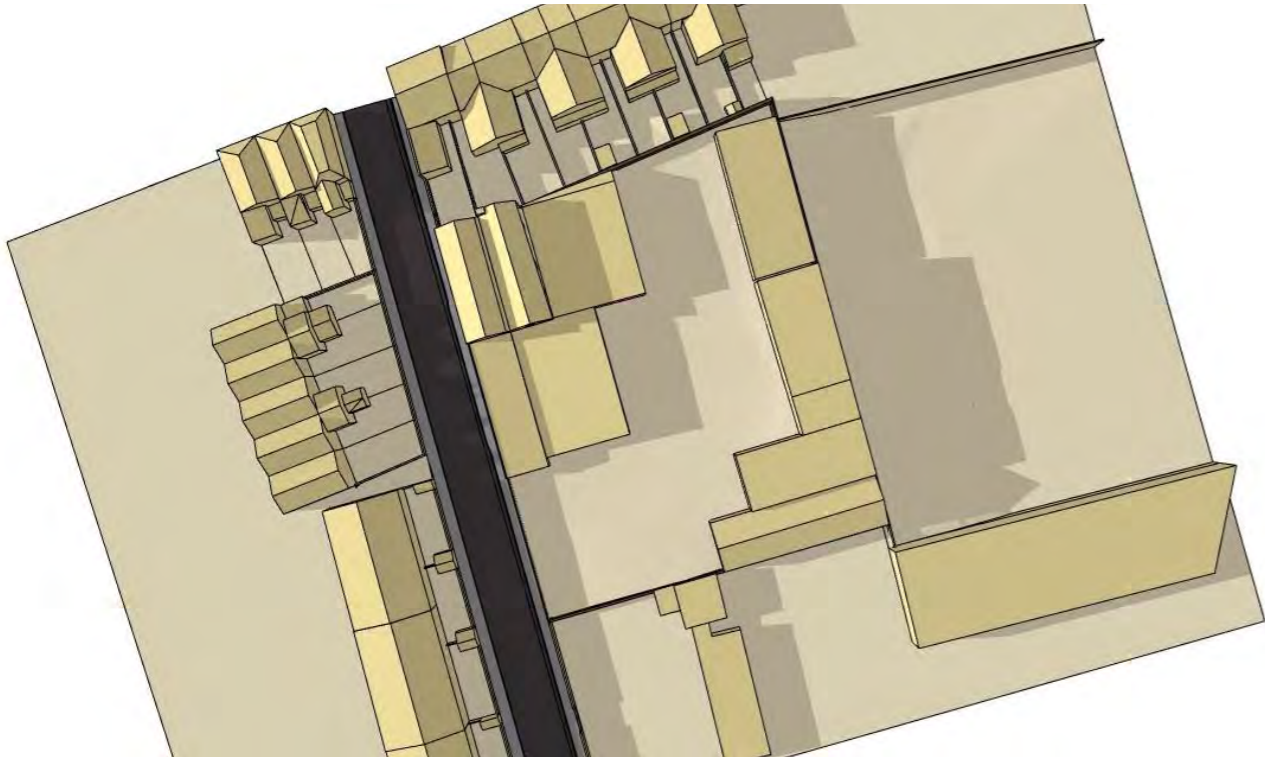


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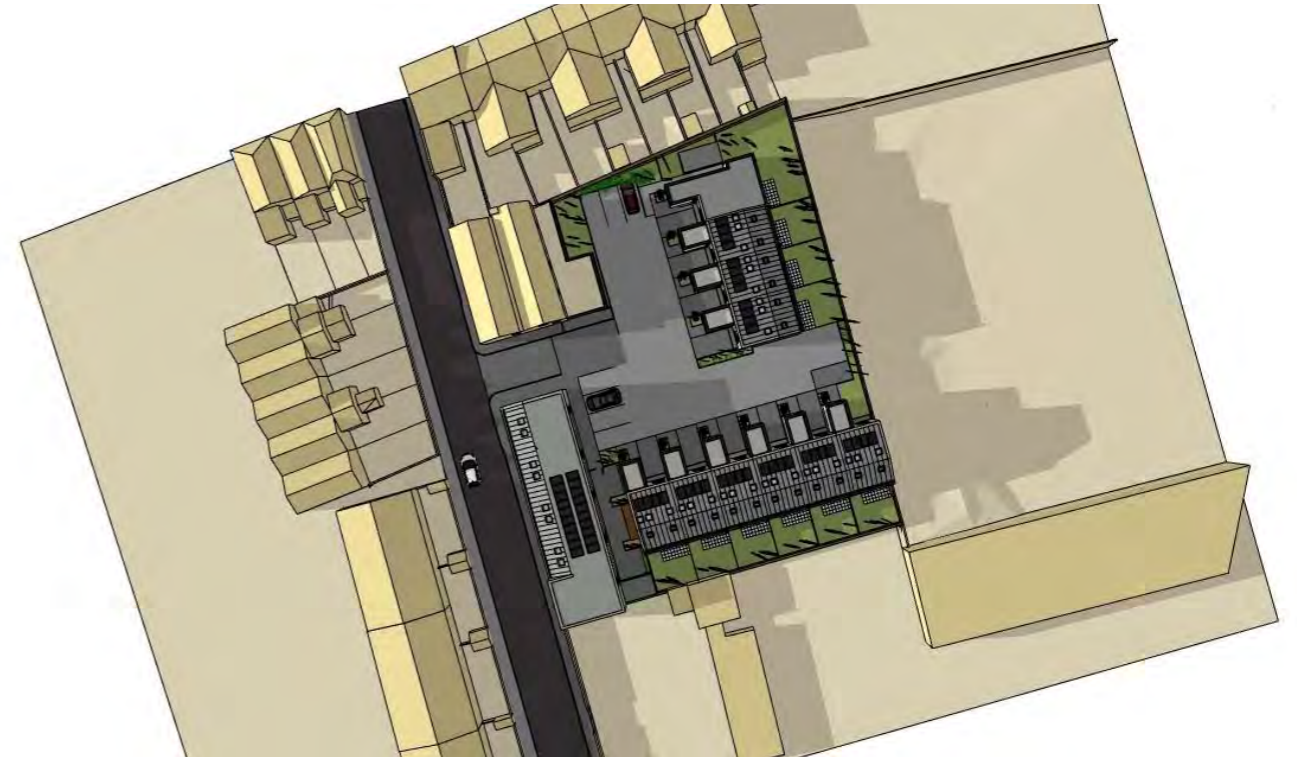
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1st MAY - 5pm



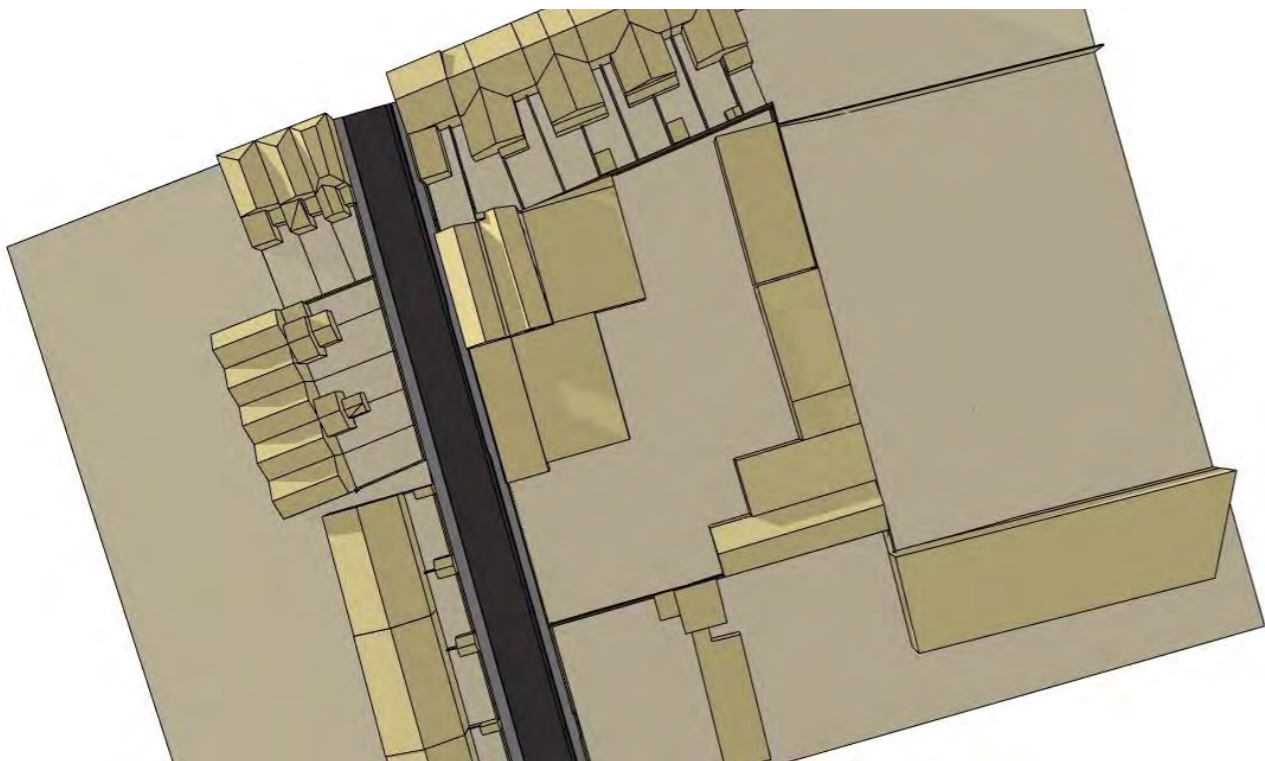
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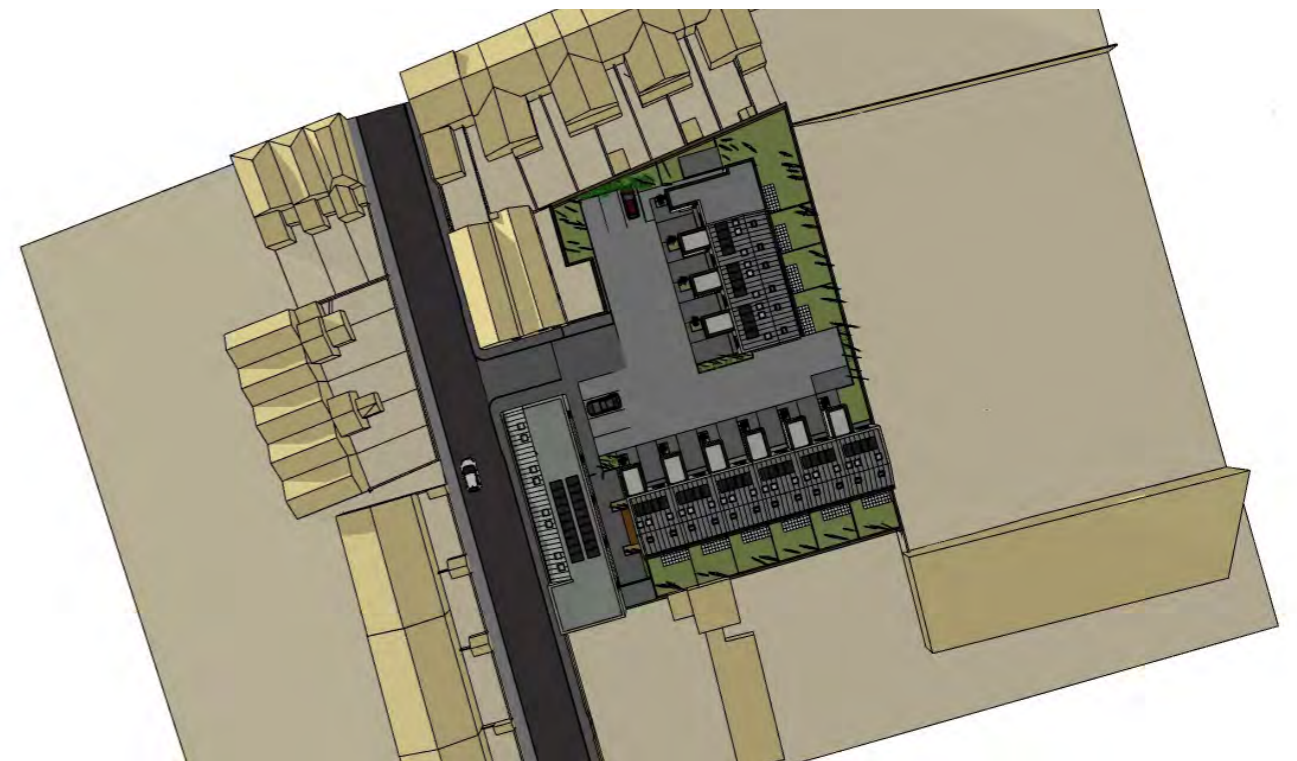


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MAY - 7pm



1st MAY - 7pm

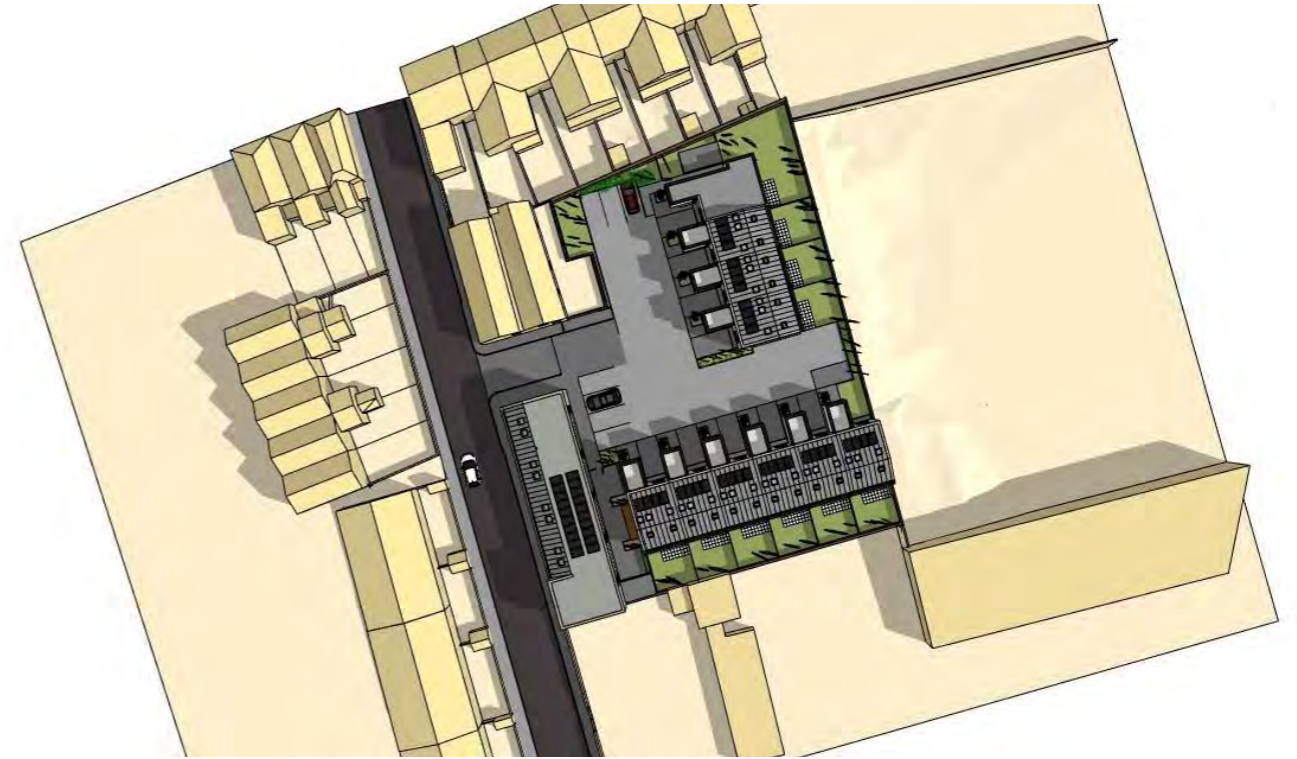
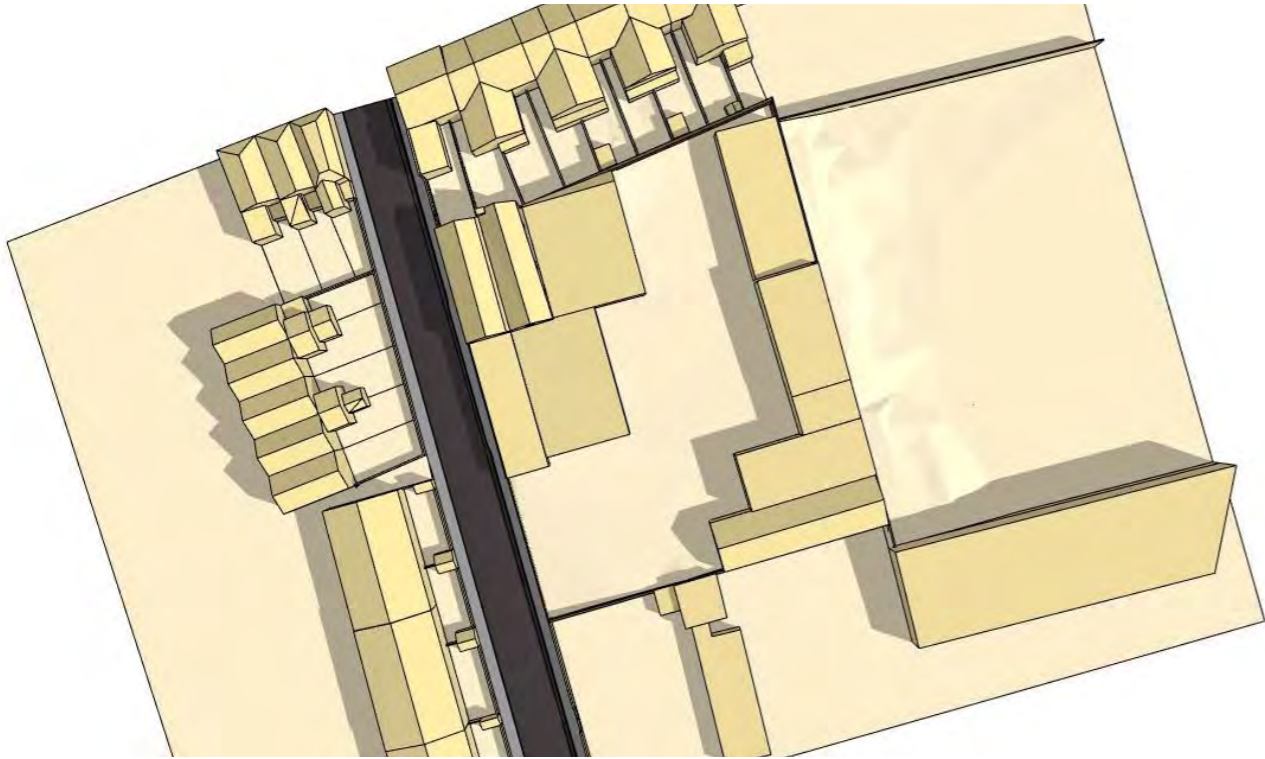


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PROPOSED

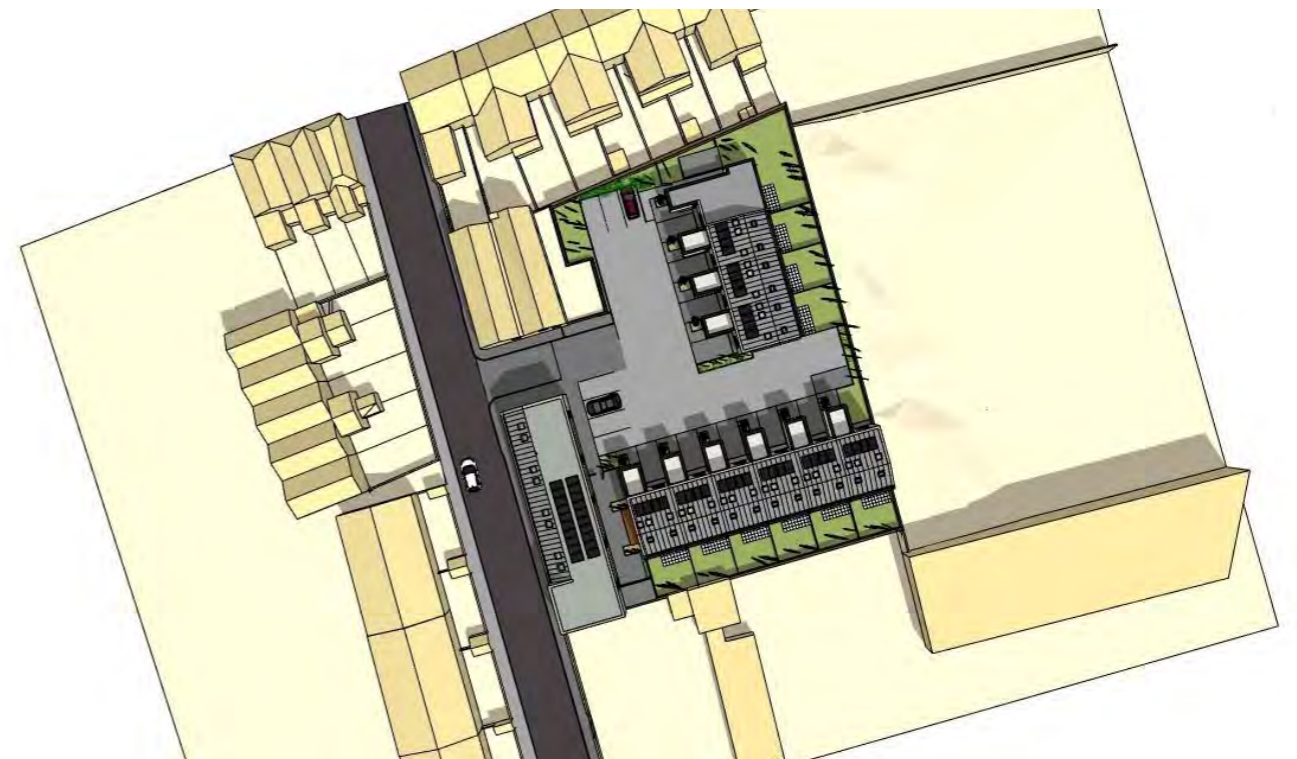
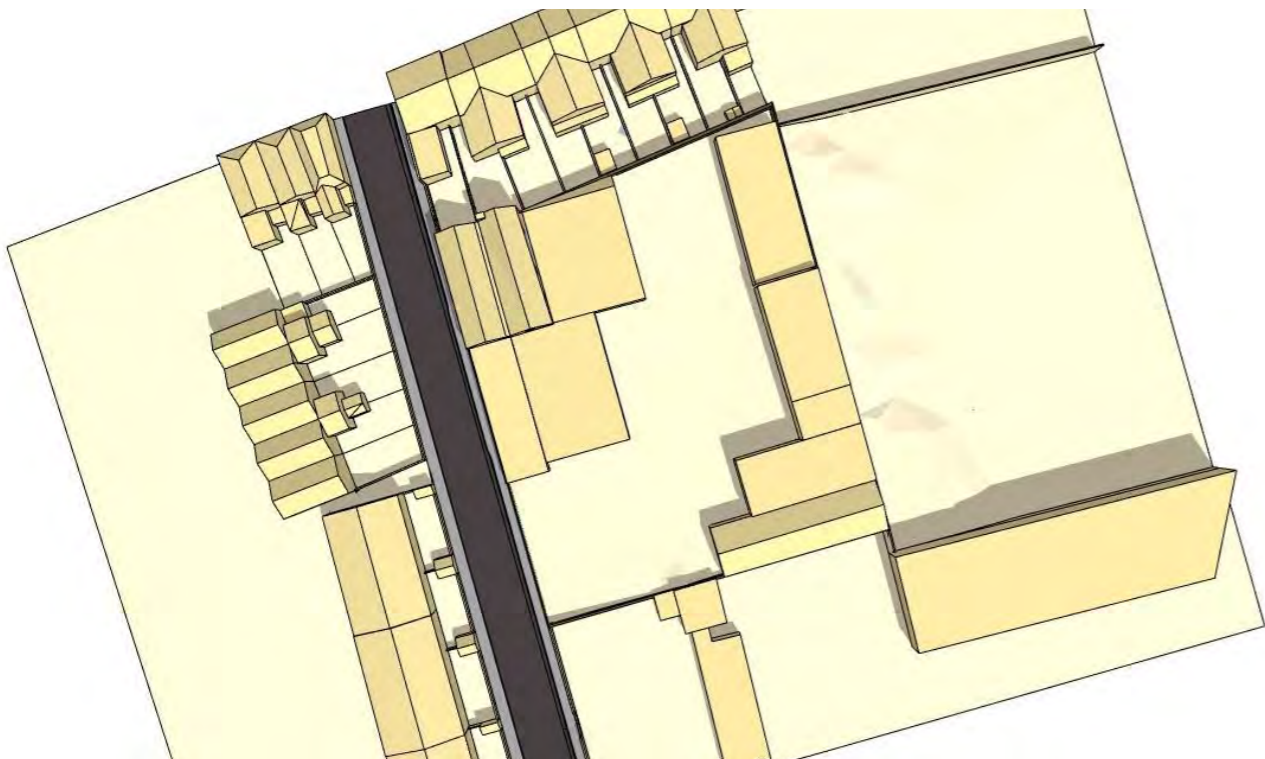
21st JUNE - 9am

21st JUNE - 9am



Page 212
21st JUNE - 11am

21st JUNE - 11am

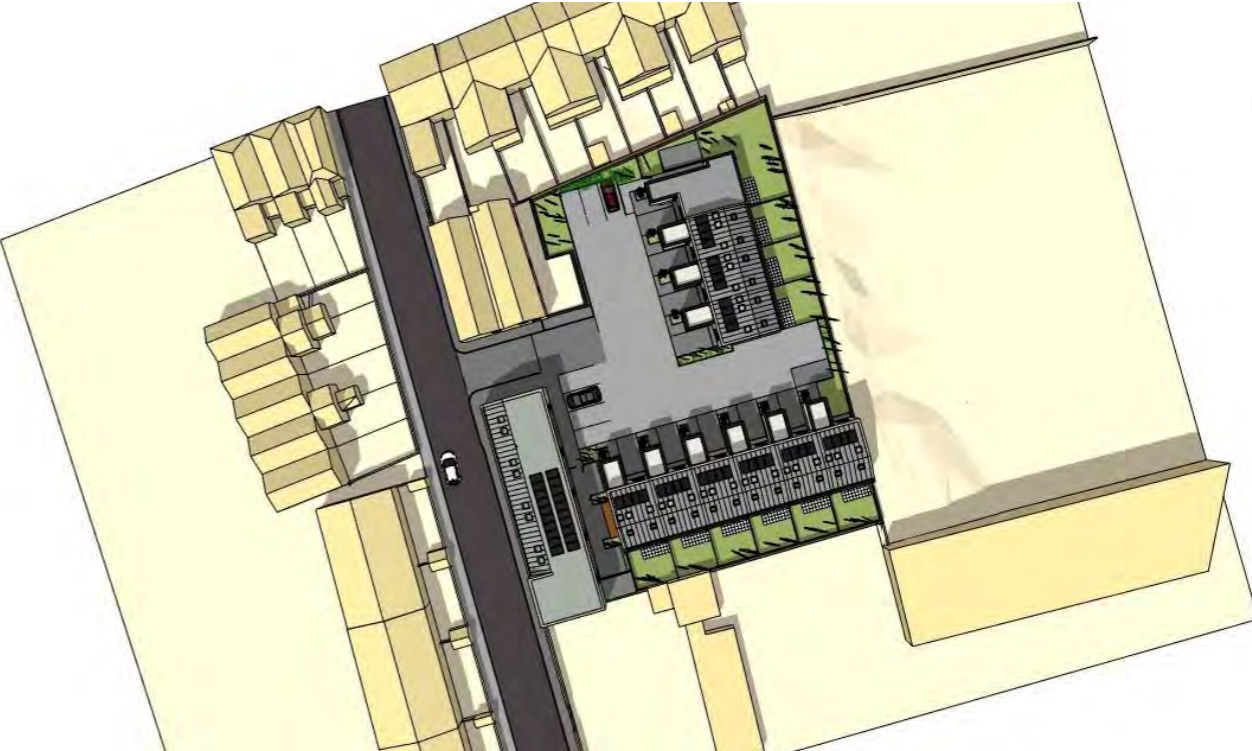
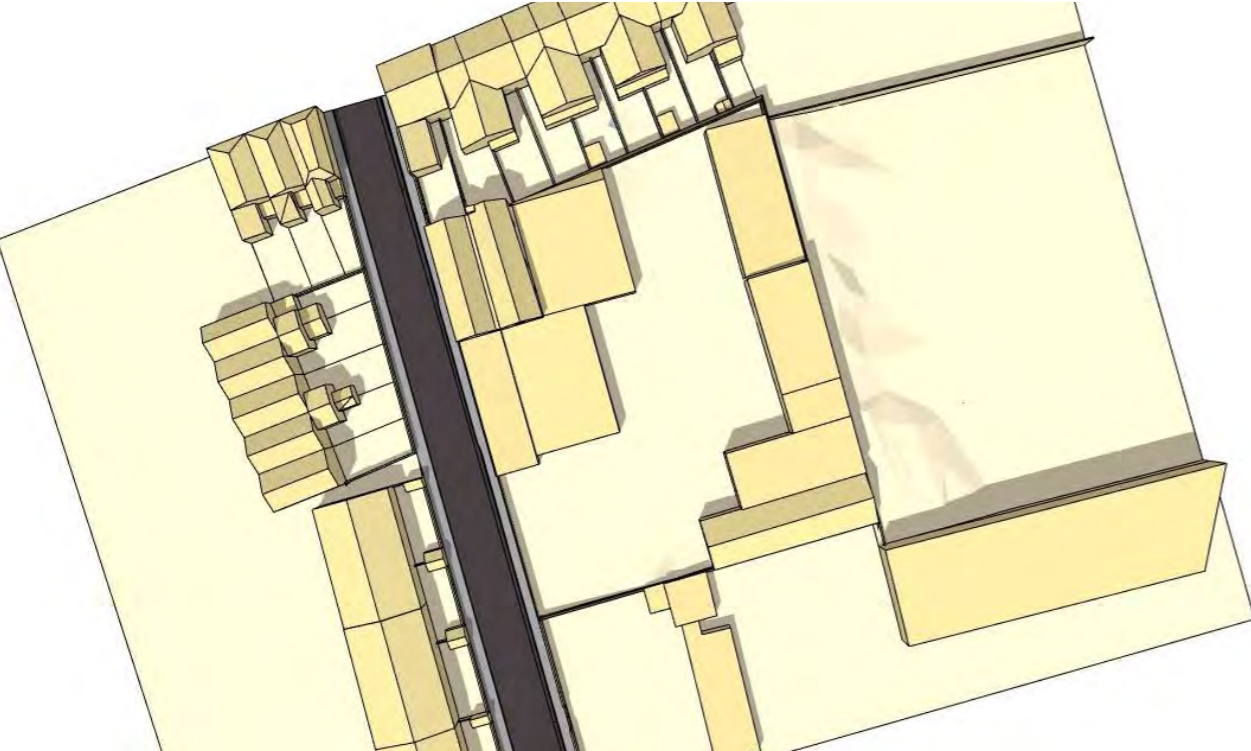


EXISTING

PROPOSED

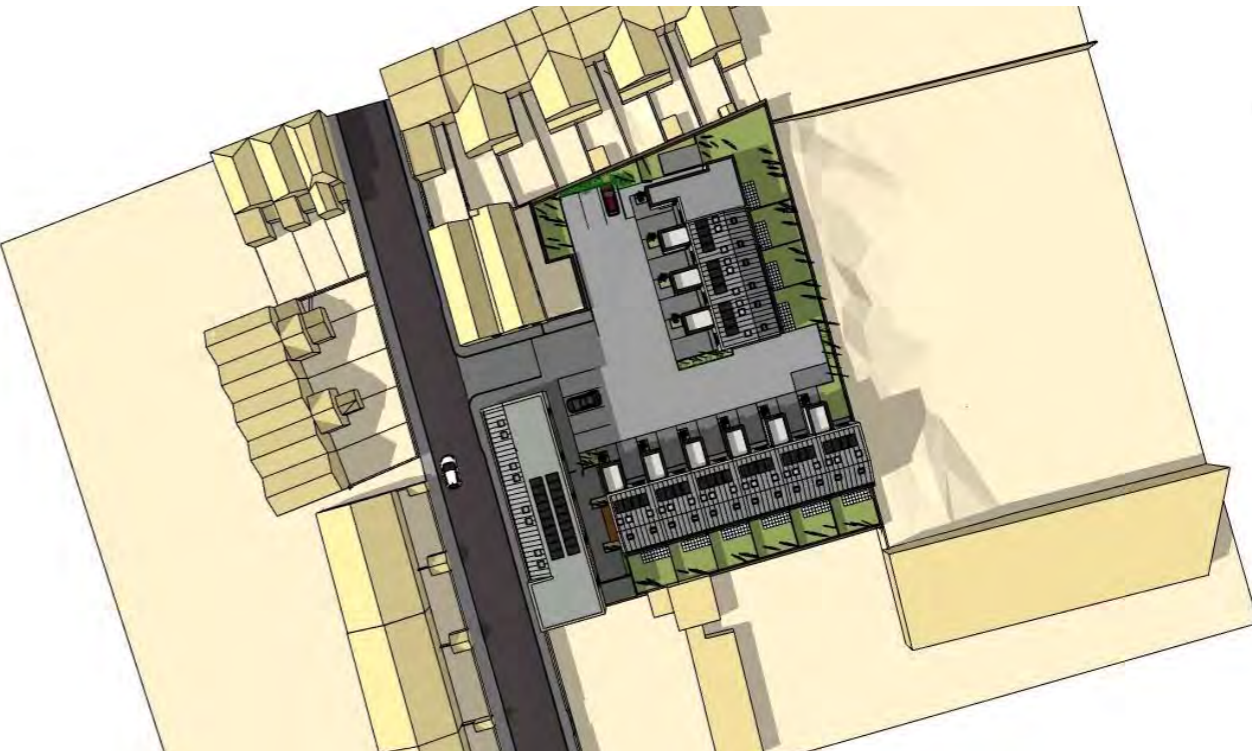
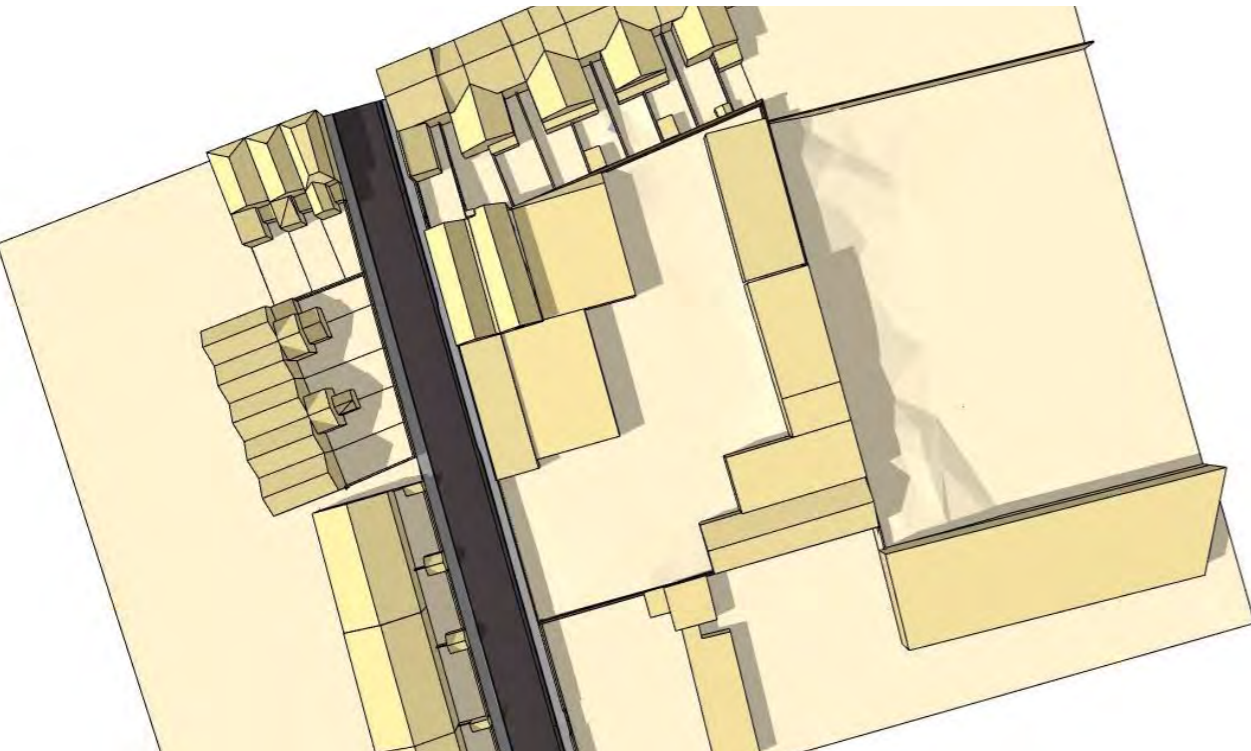
21st JUNE - 1pm

21st JUNE - 1pm



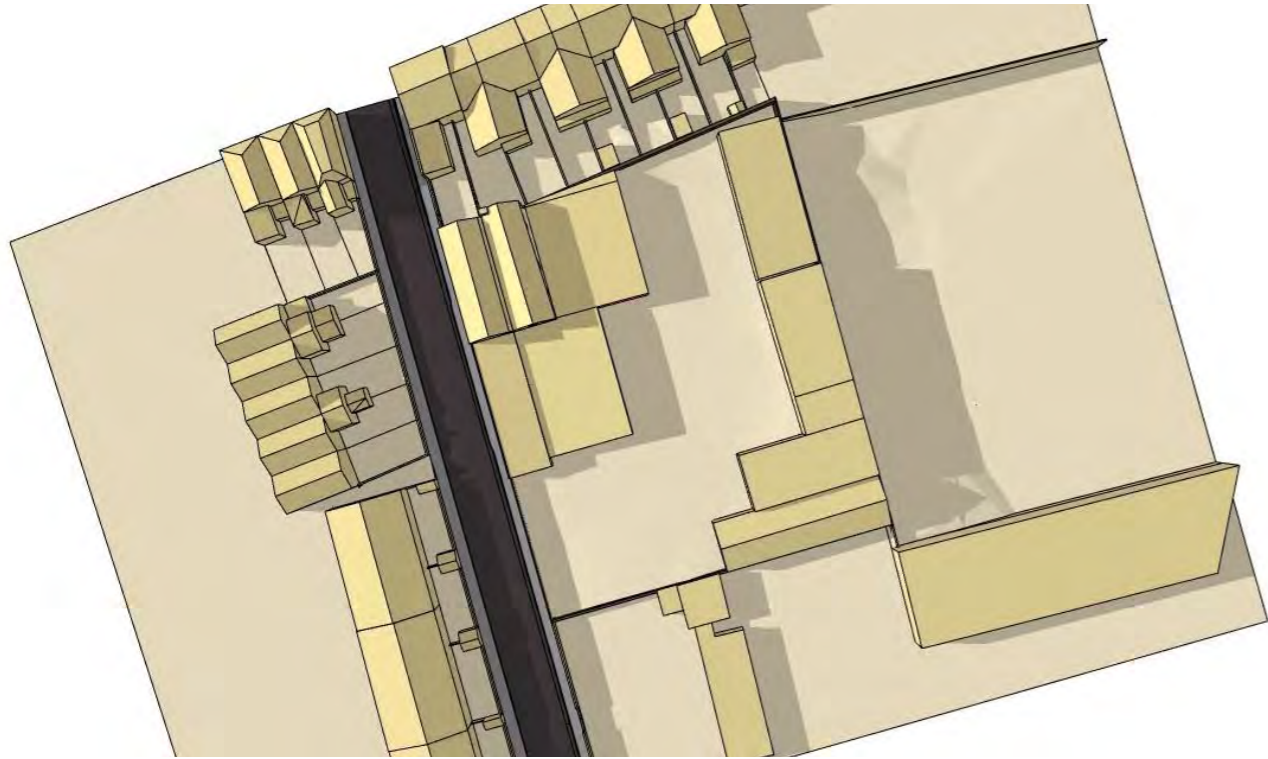
Page 213
21st JUNE - 3pm

21st JUNE - 3pm



EXISTING

21st JUNE - 5pm

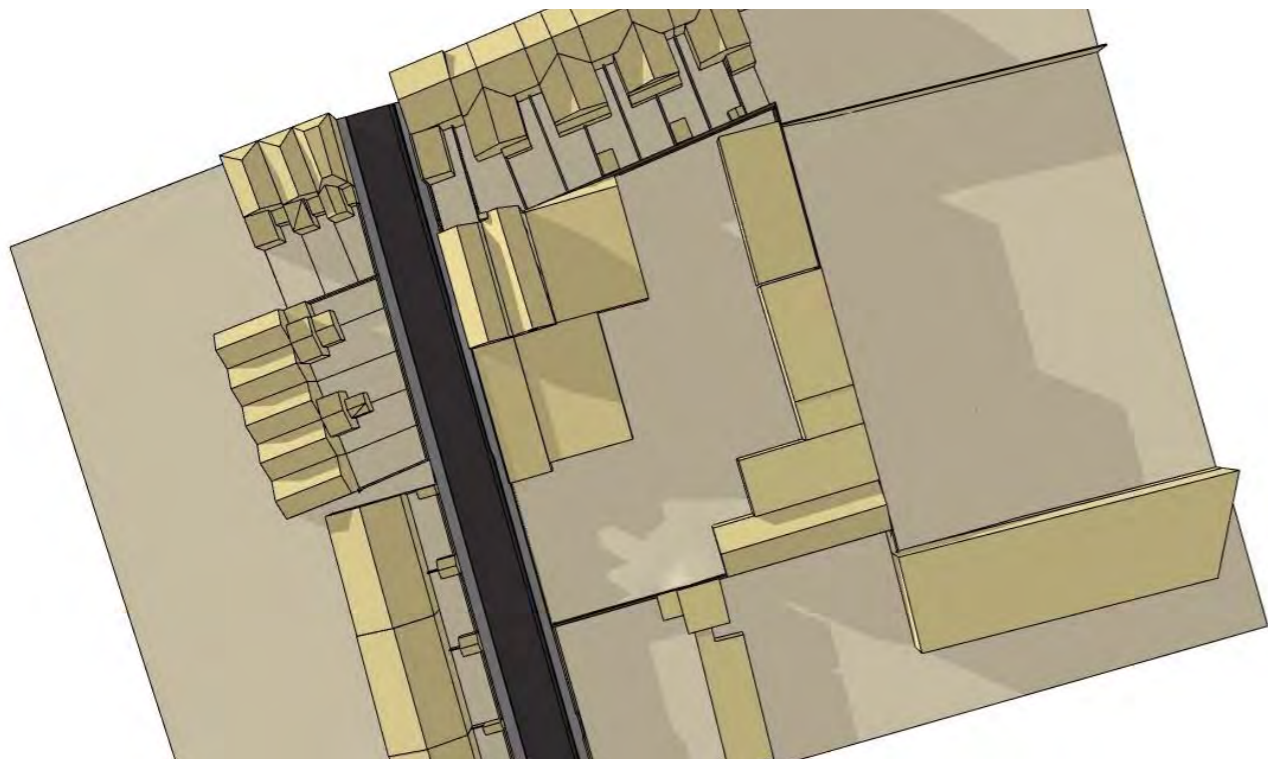


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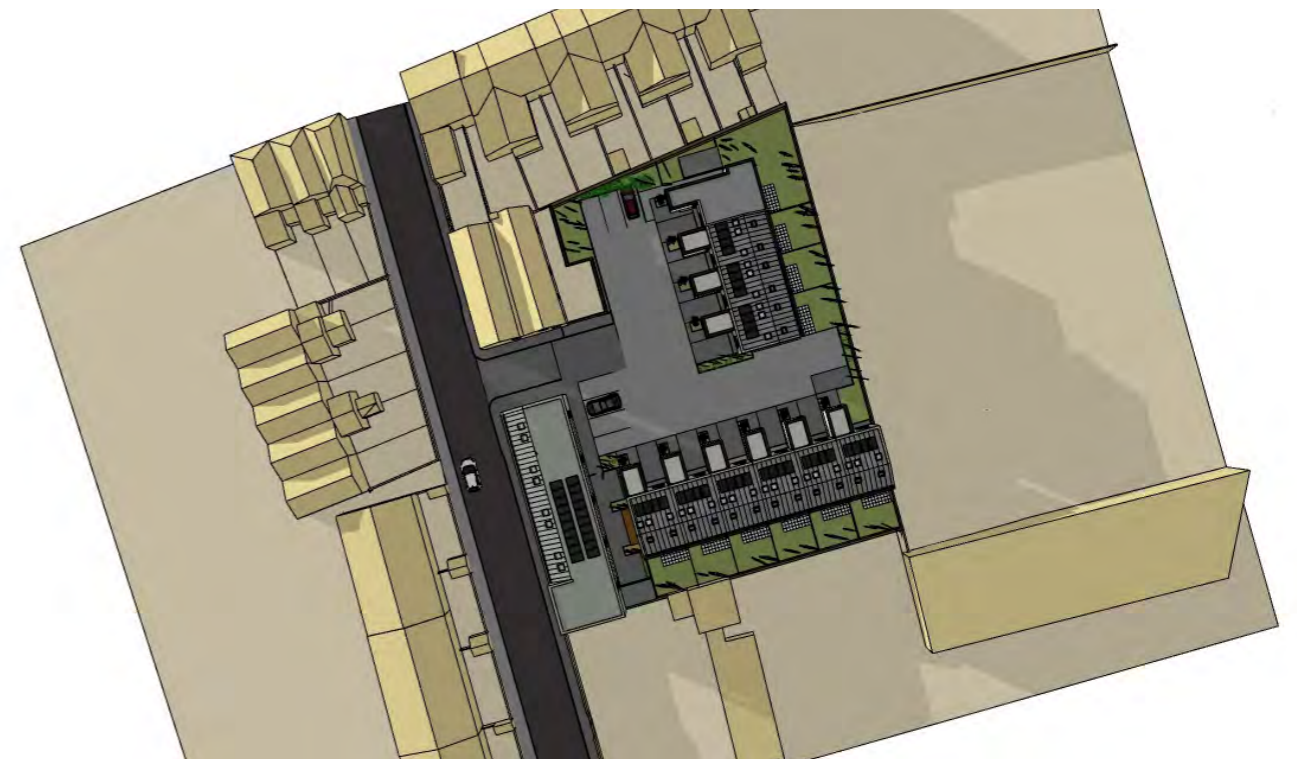
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Page 214
21st JUNE - 7pm



21st JUNE - 7pm

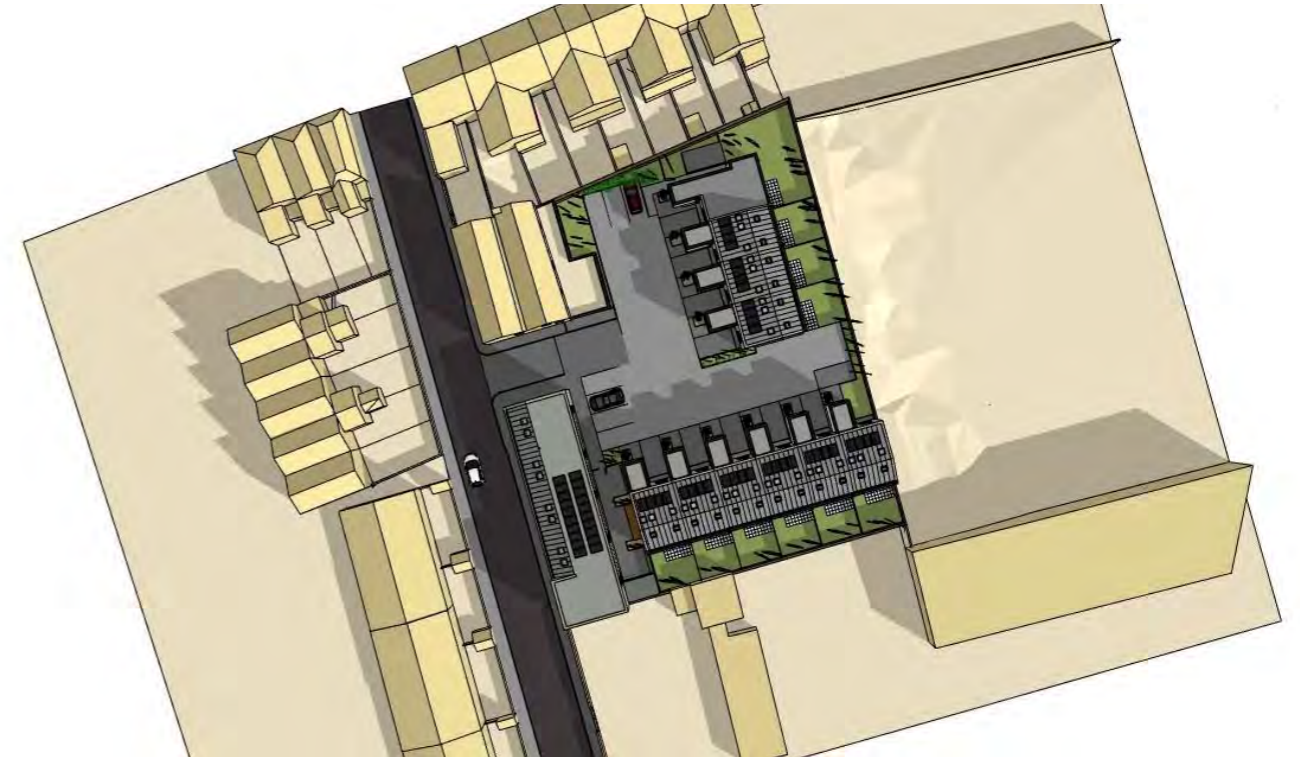
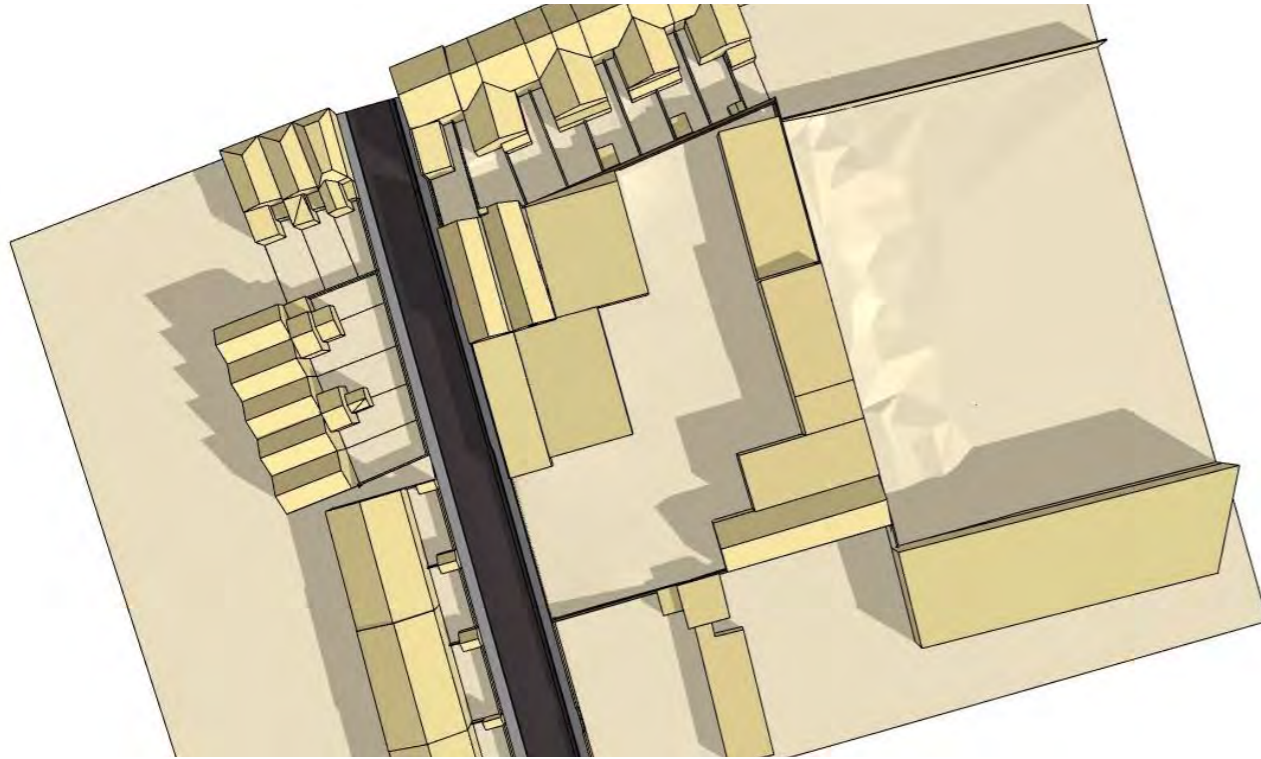


EXISTING

PROPOSED

1st SEPTEMBER - 9am

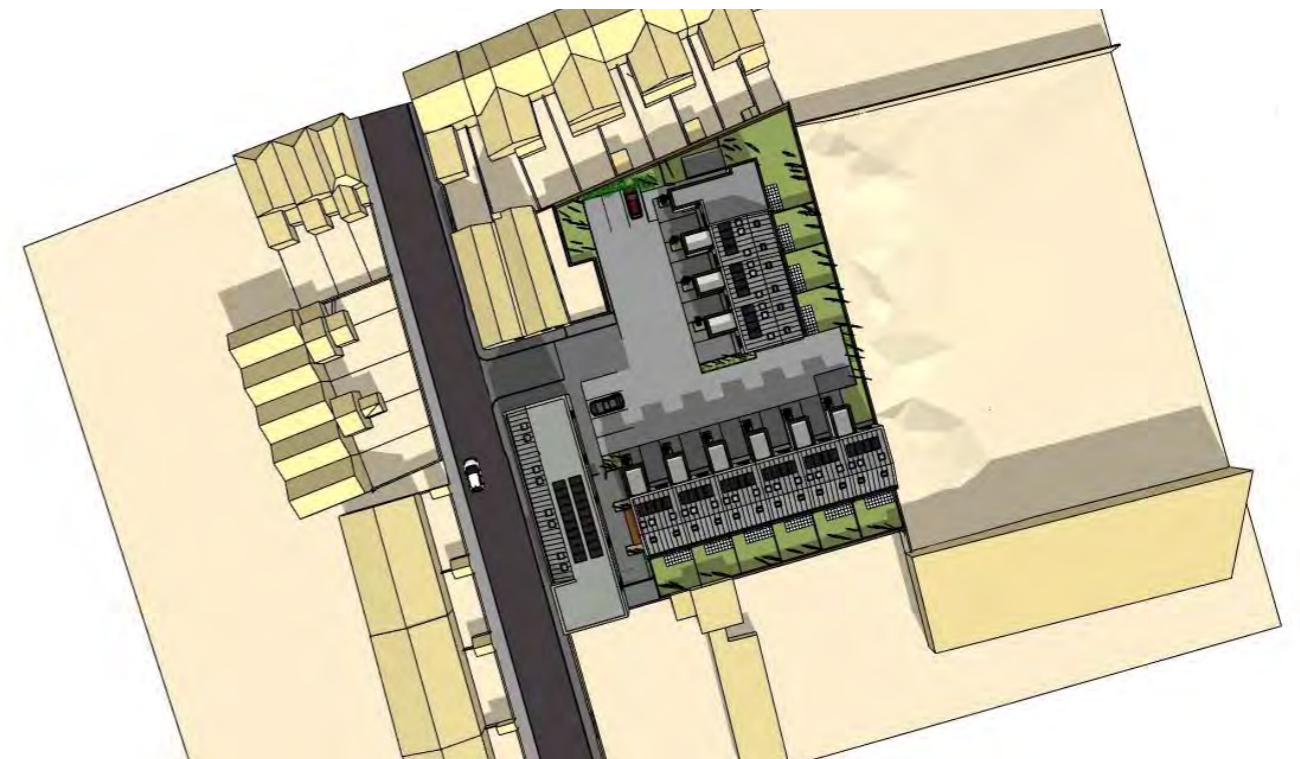
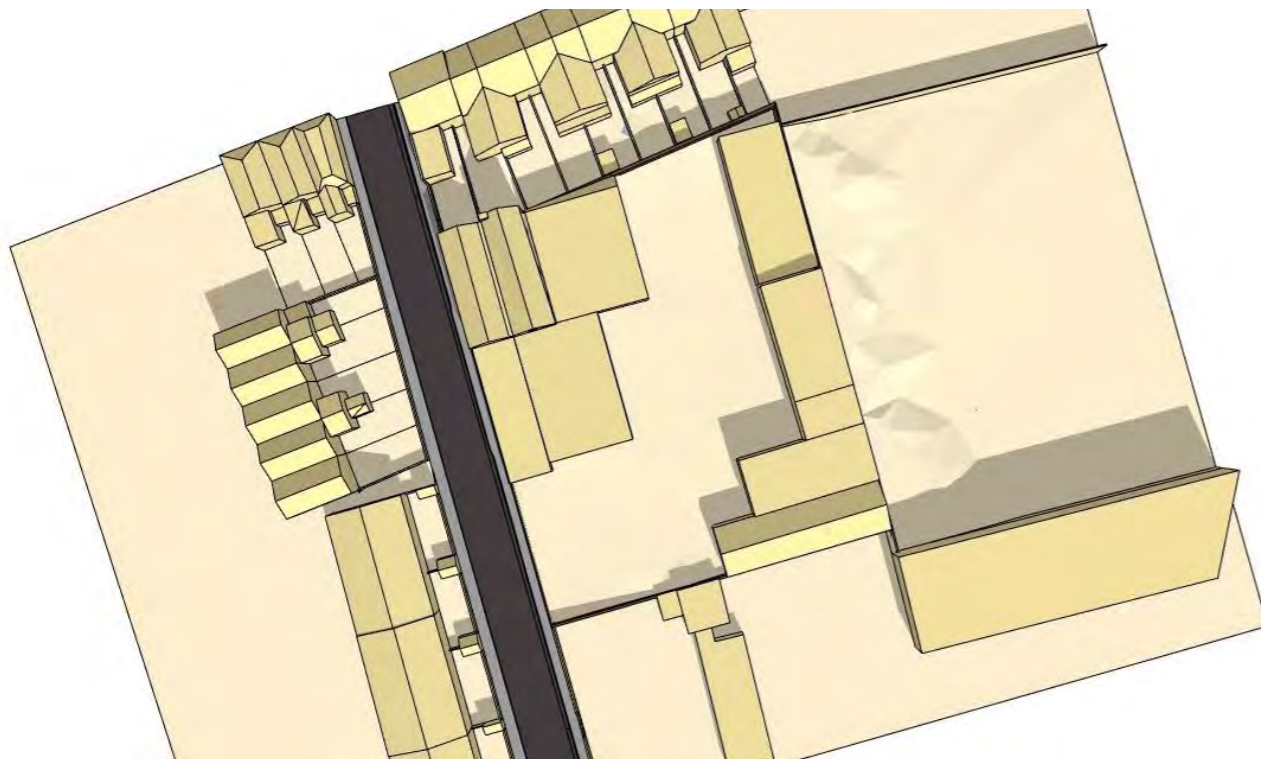
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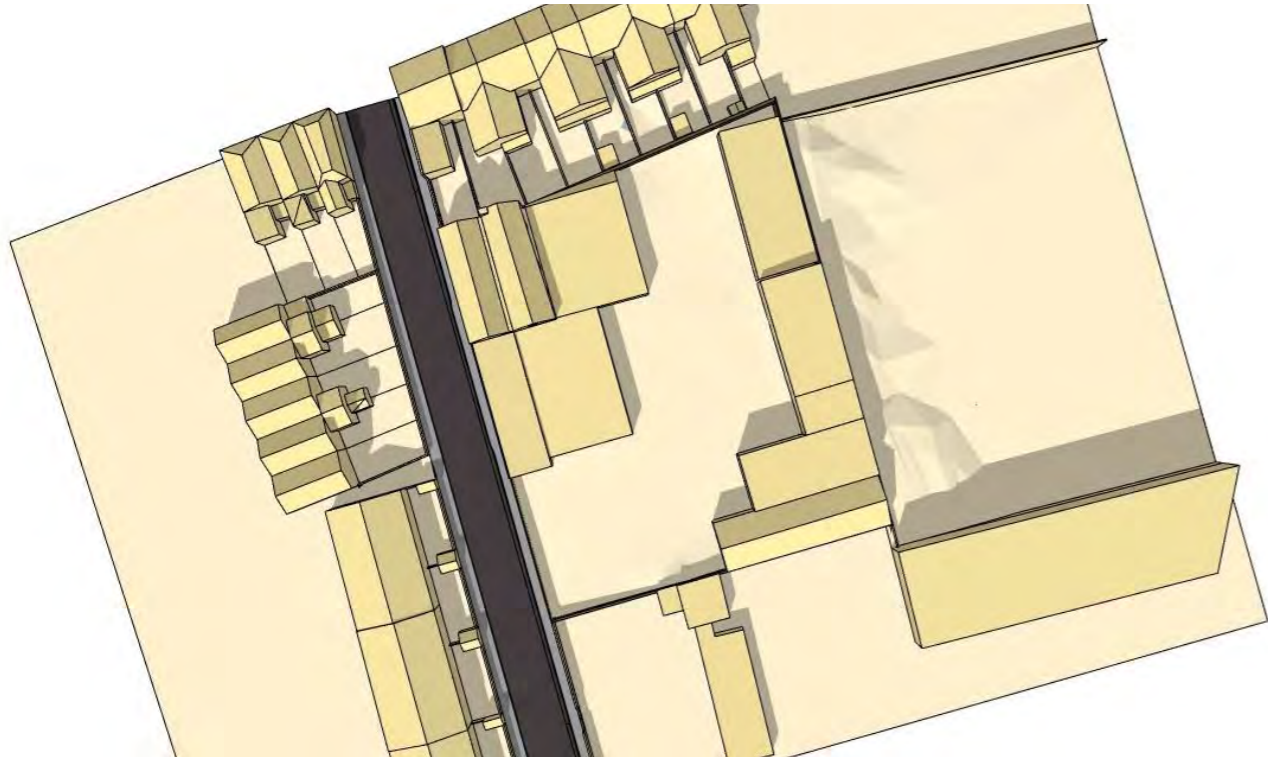
SEPTEMBER - 11am

1st SEPTEMBER - 11am



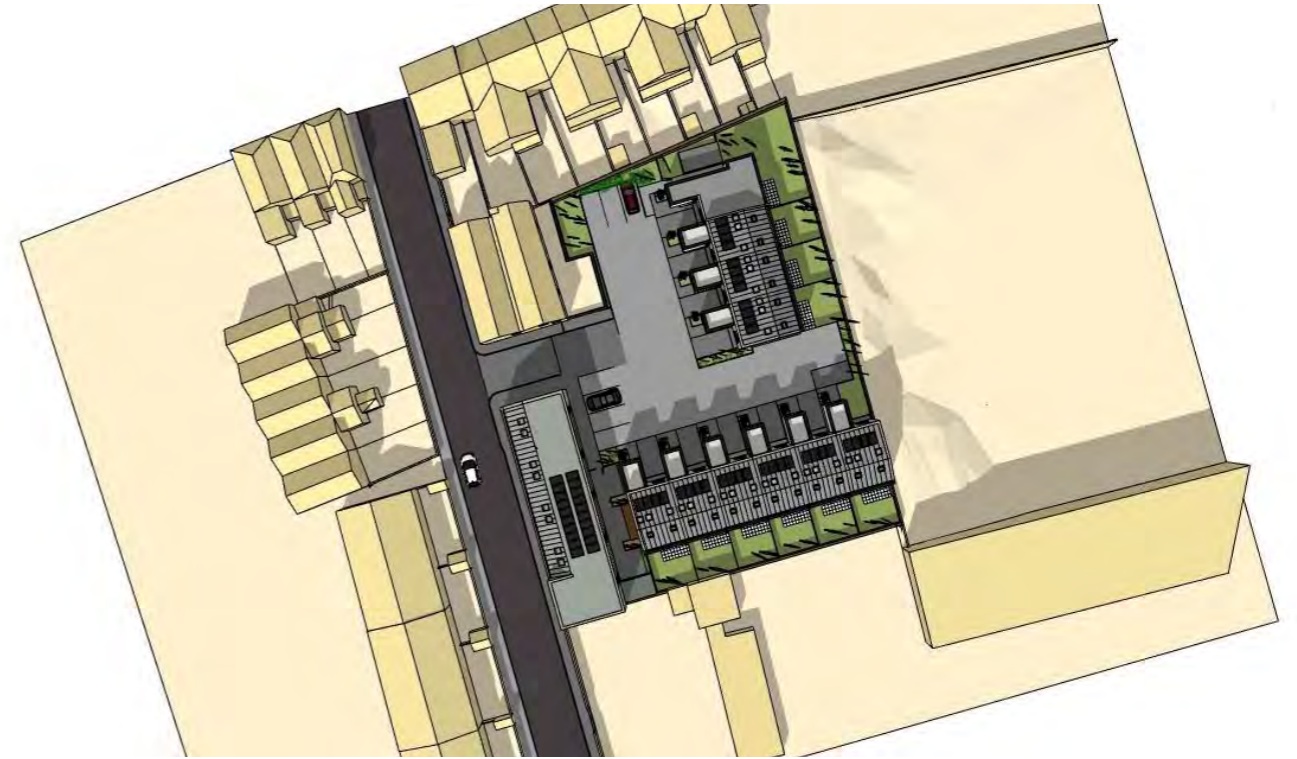
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1st SEPTEMBER - 1pm



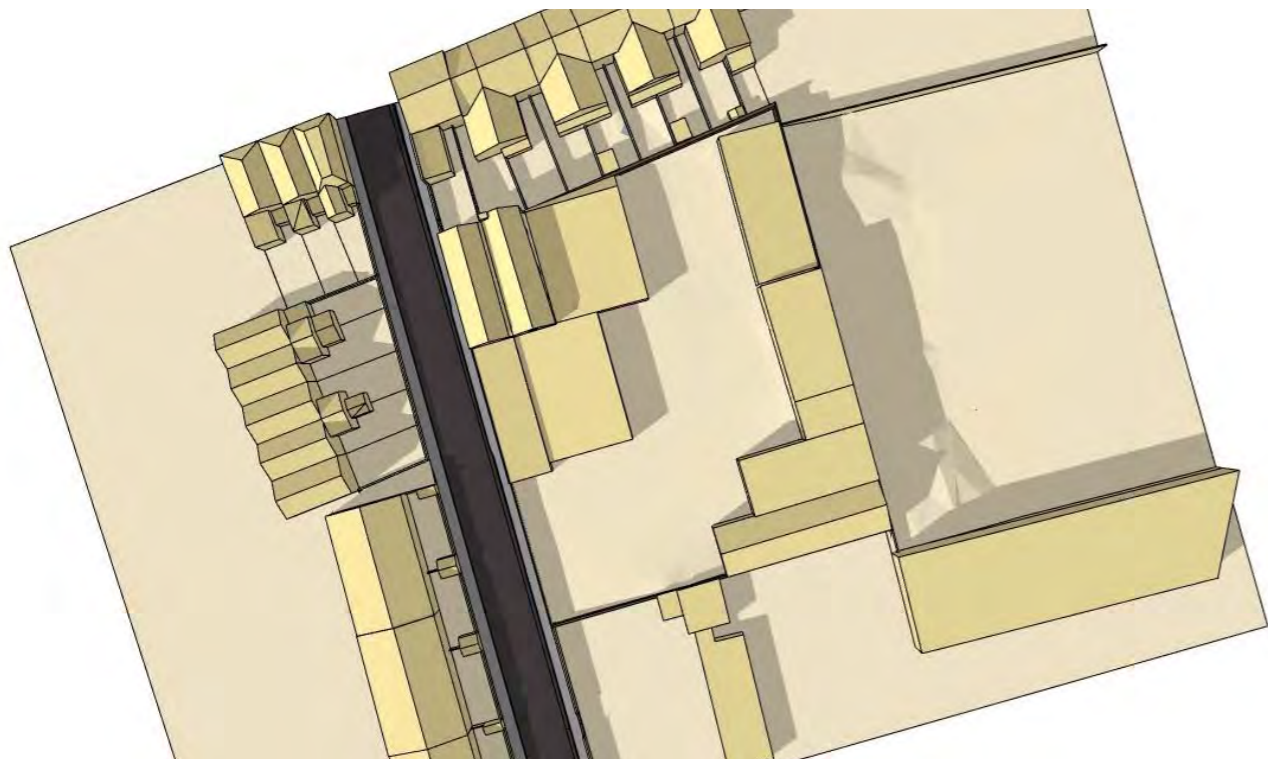
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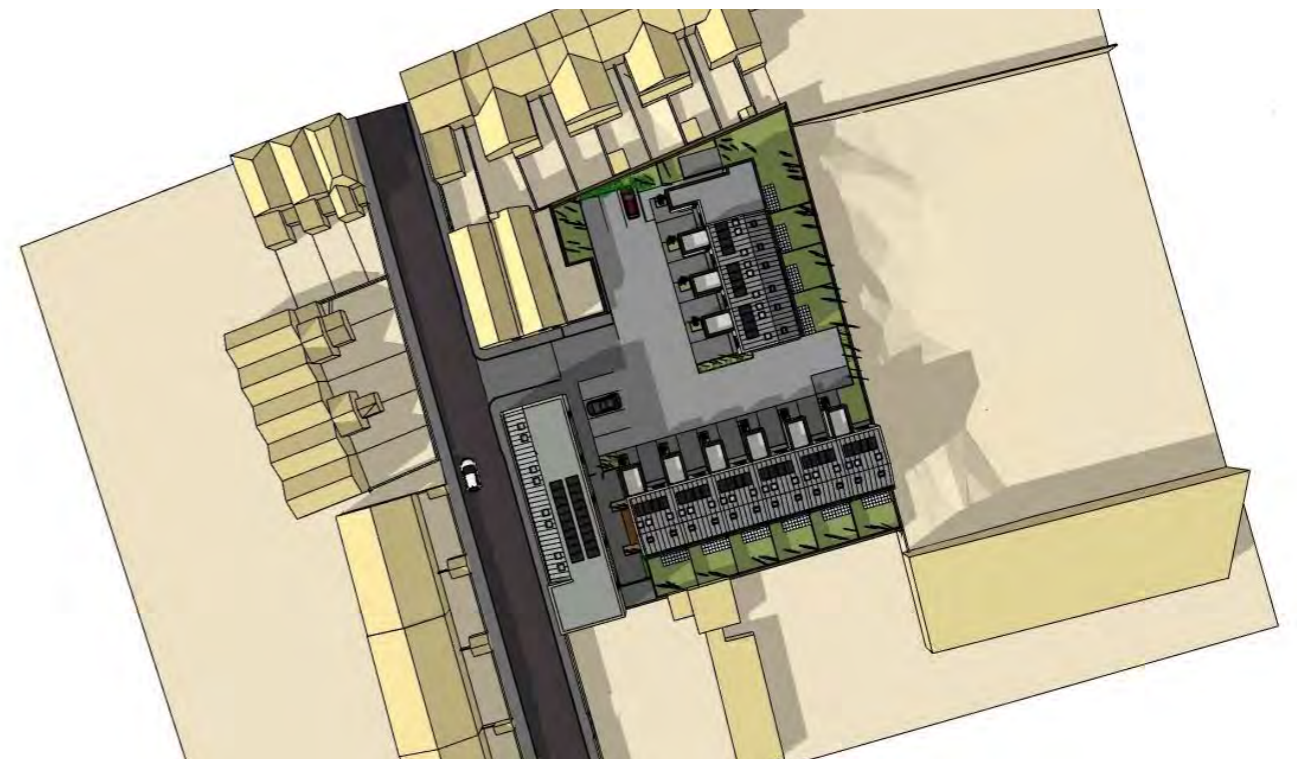


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SEPTEMBER - 3pm



1st SEPTEMBER - 3pm

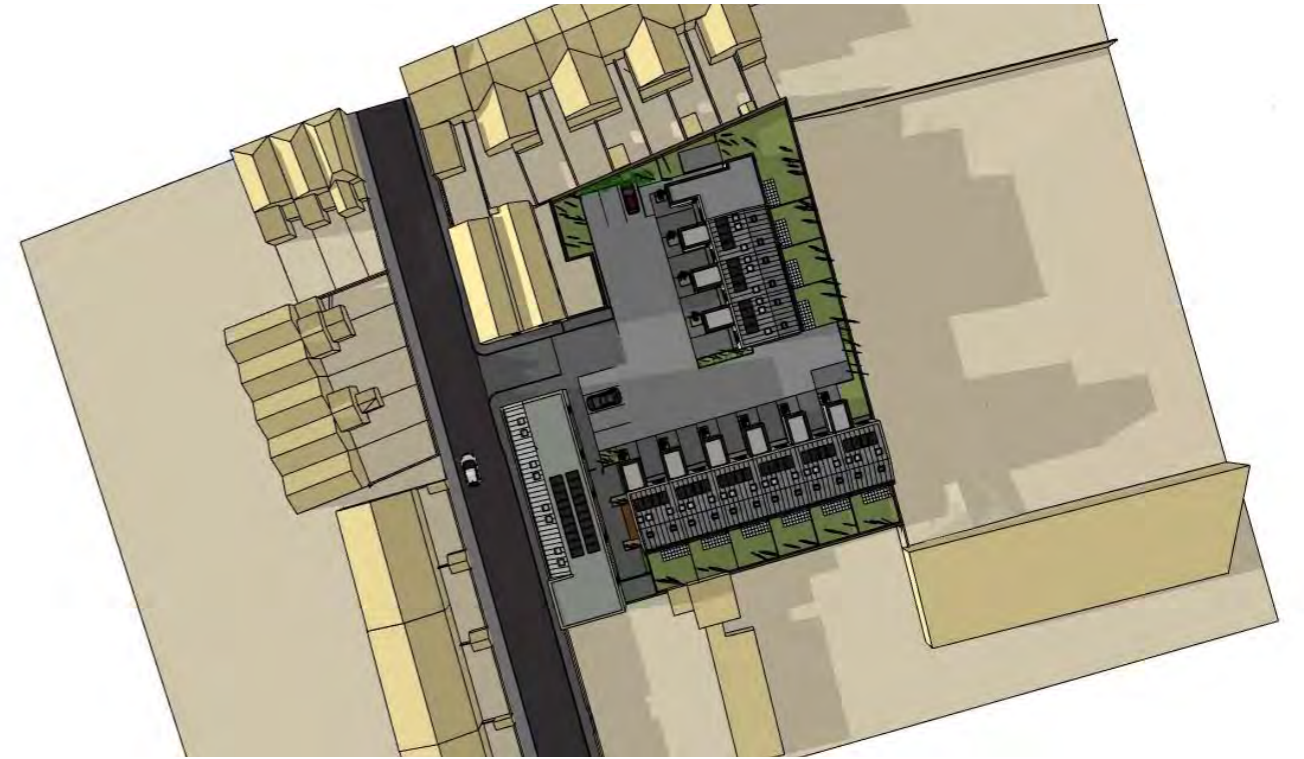
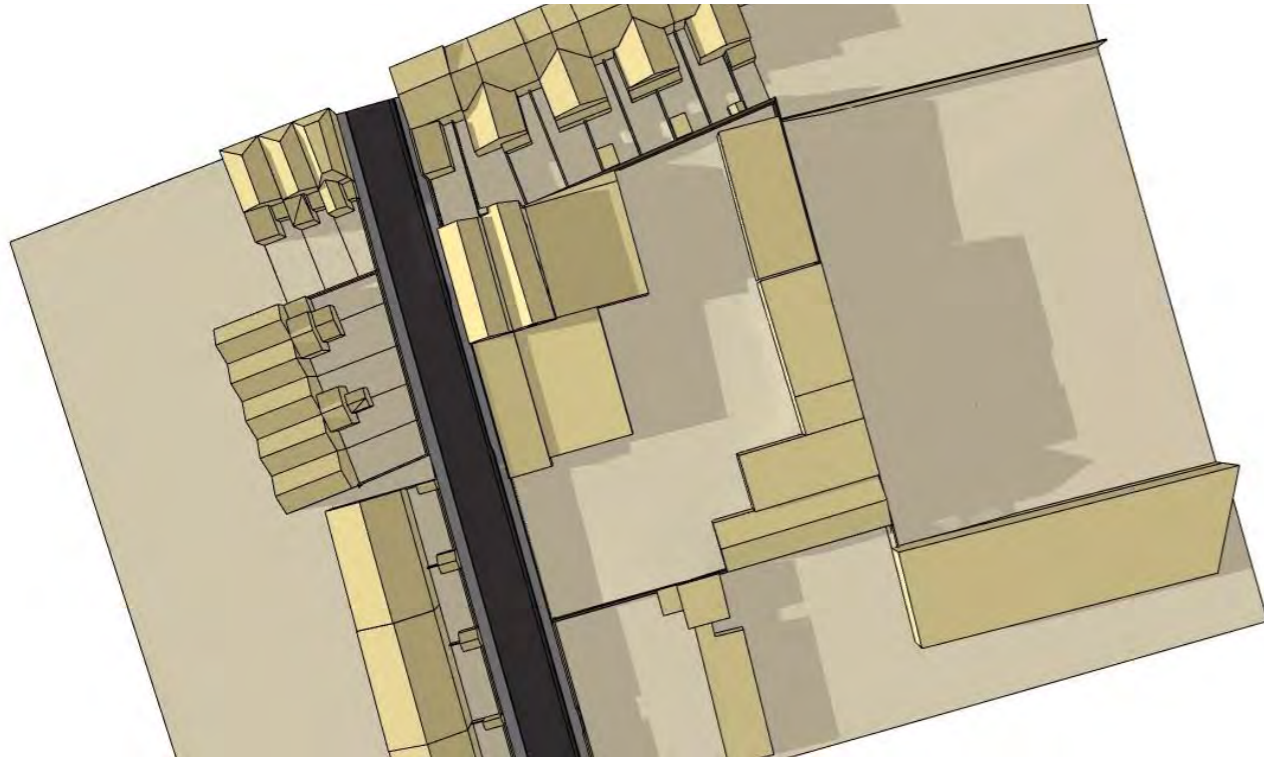


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PROPOSED

1st SEPTEMBER - 5pm

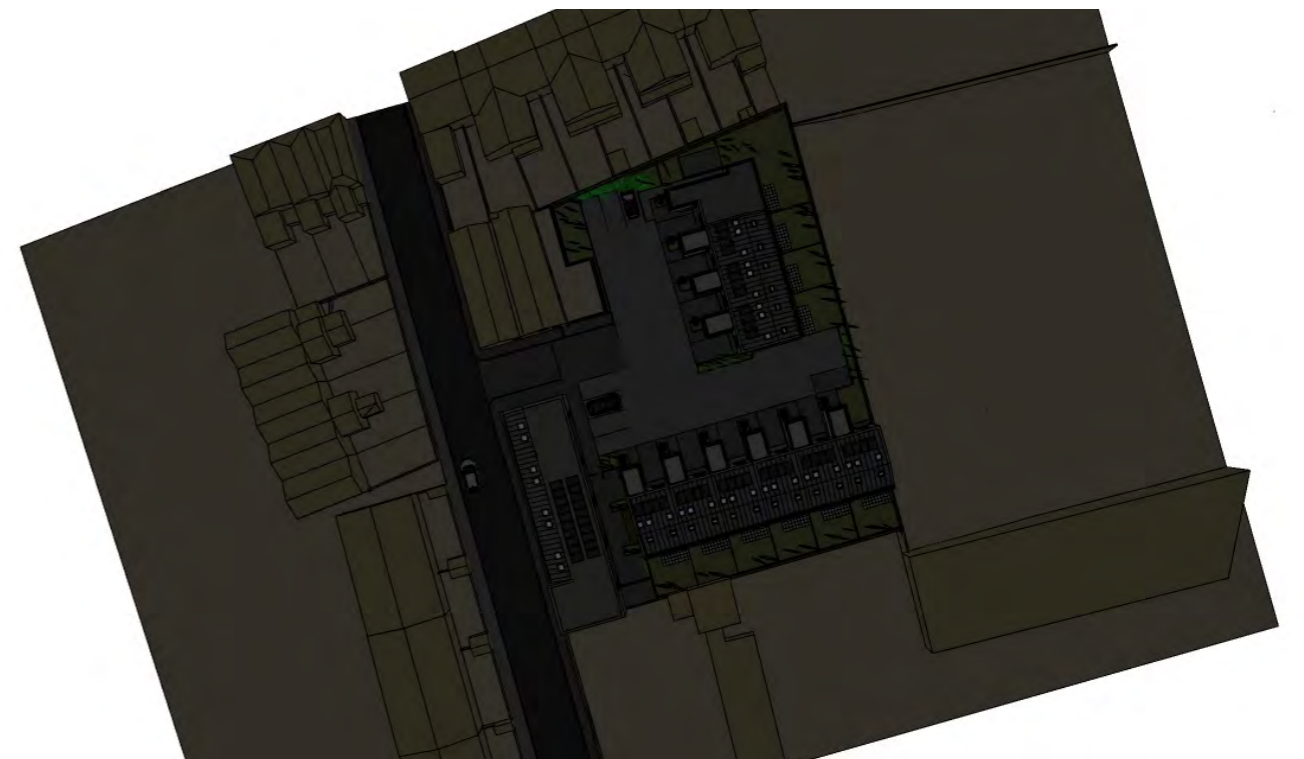
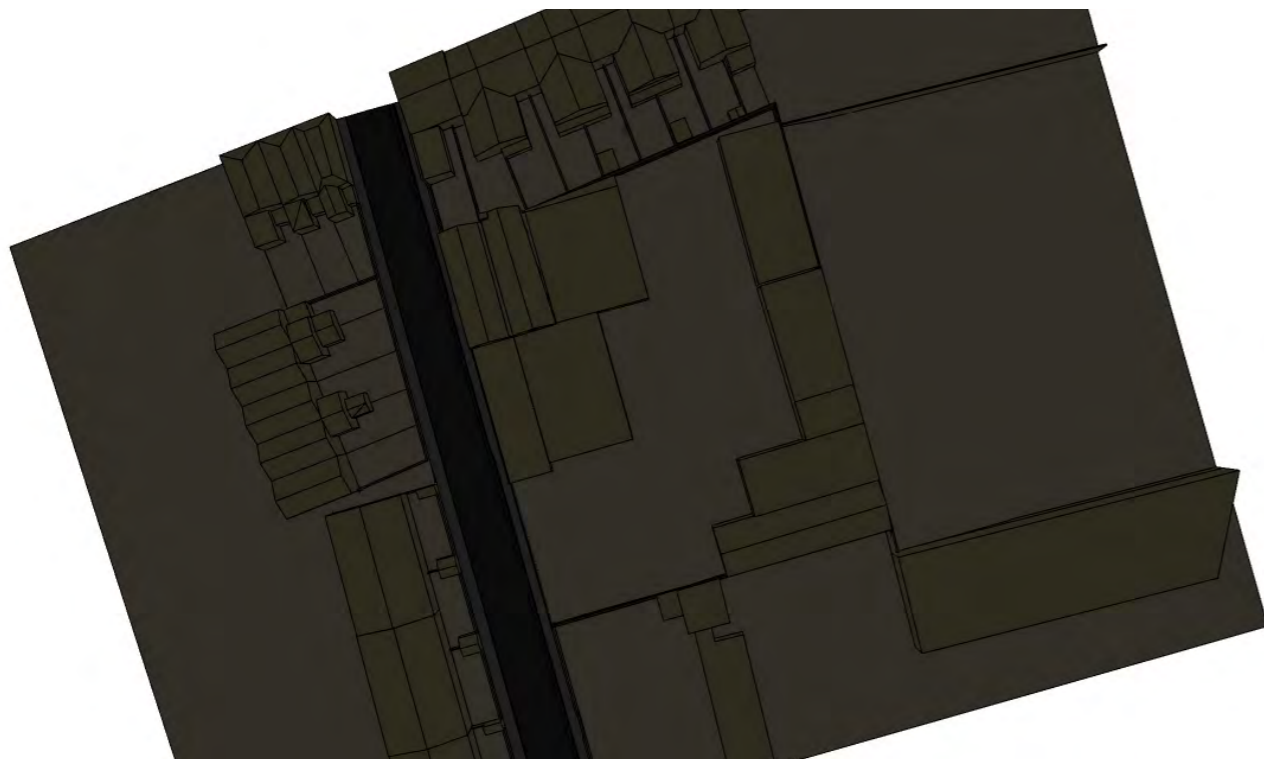
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1st SEPTEMBER - 7pm

1st SEPTEMBER - 7pm

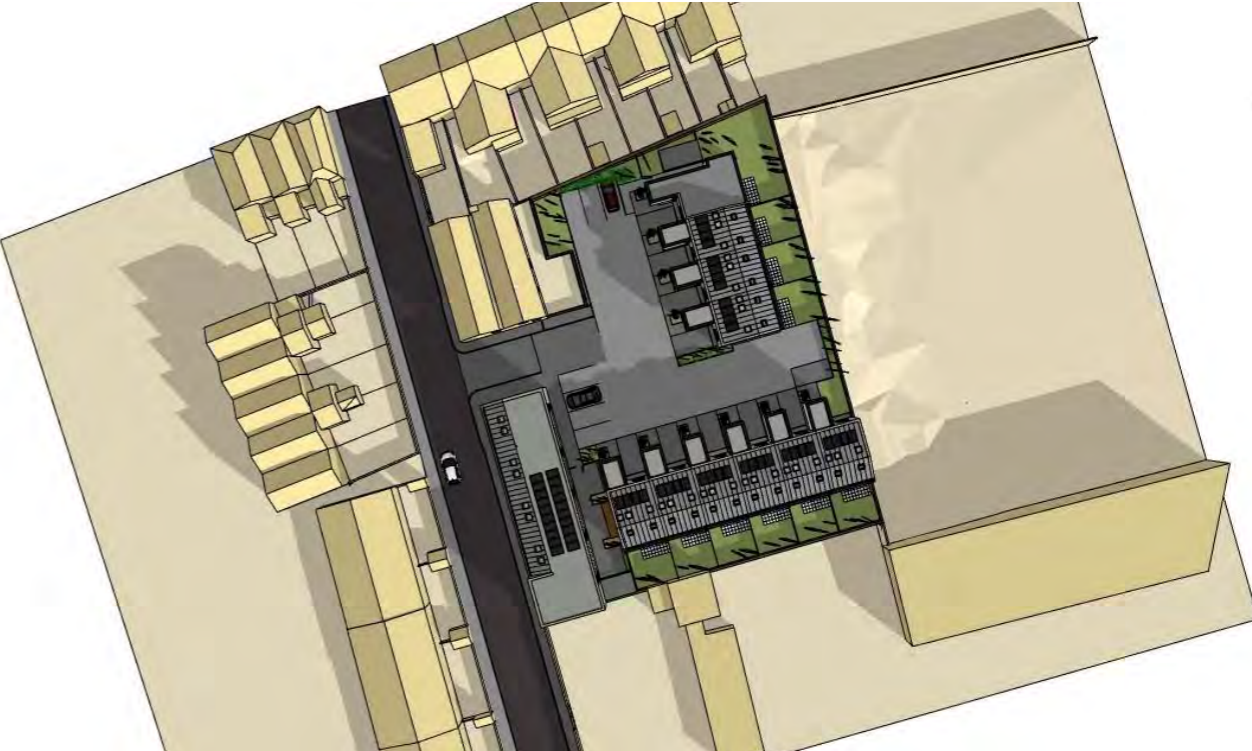
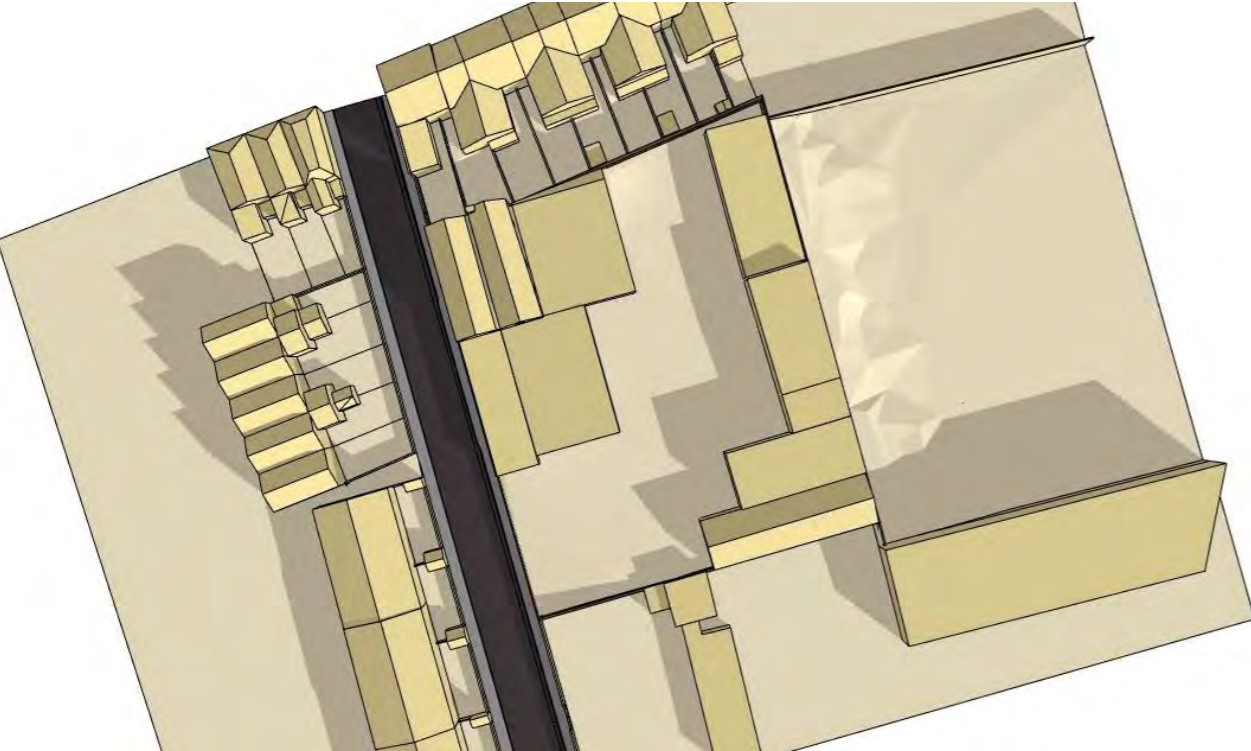


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21st SEPTEMBER - 9am

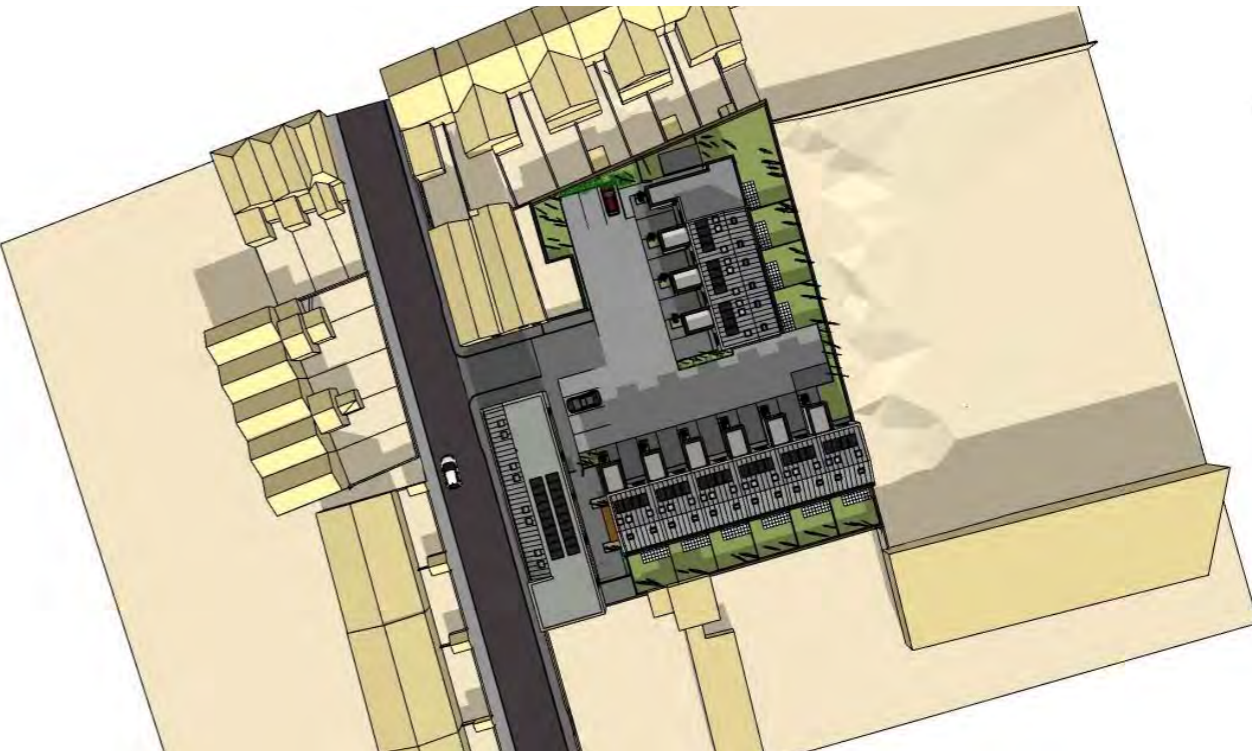
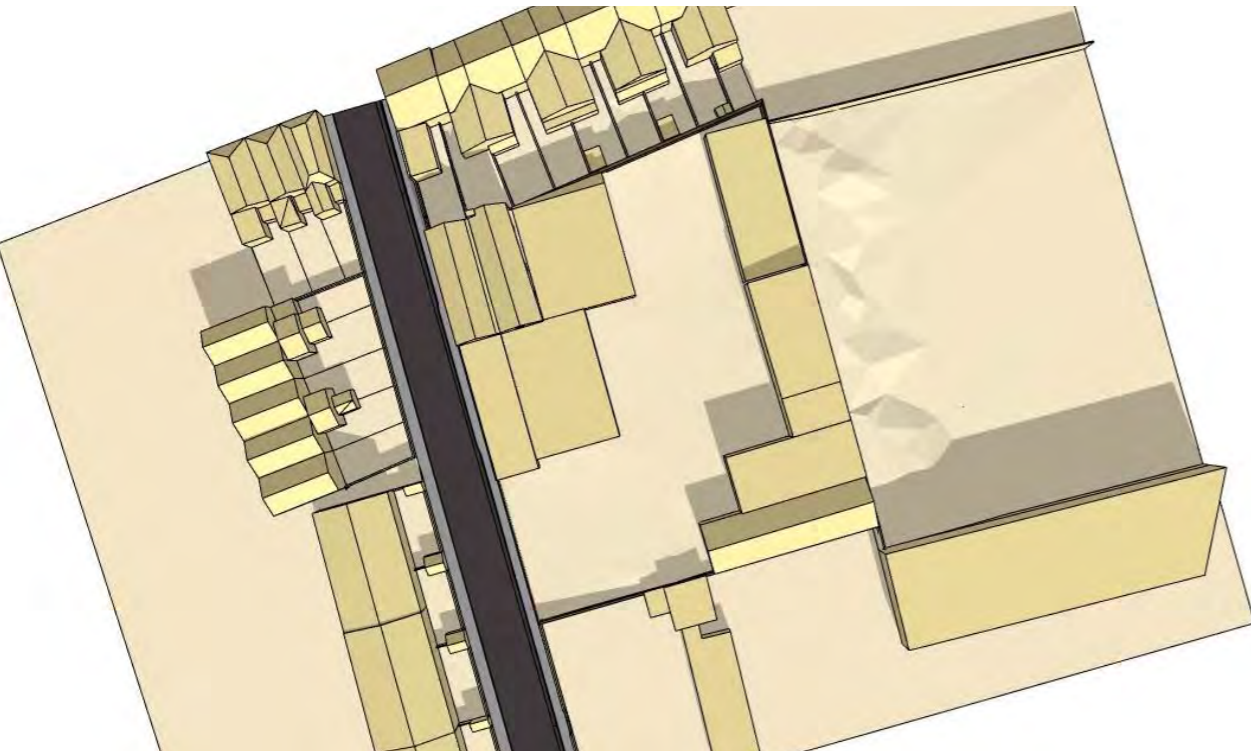
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21st SEPTEMBER - 11am

21st SEPTEMBER - 11am

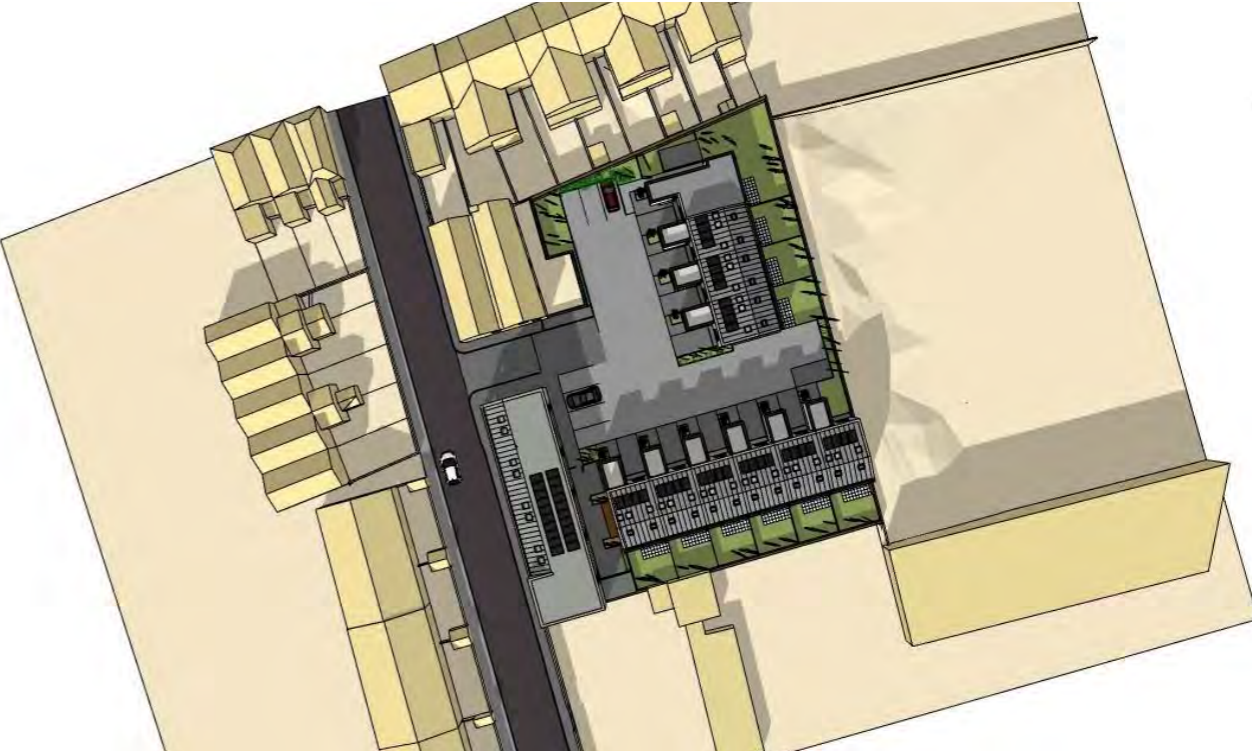
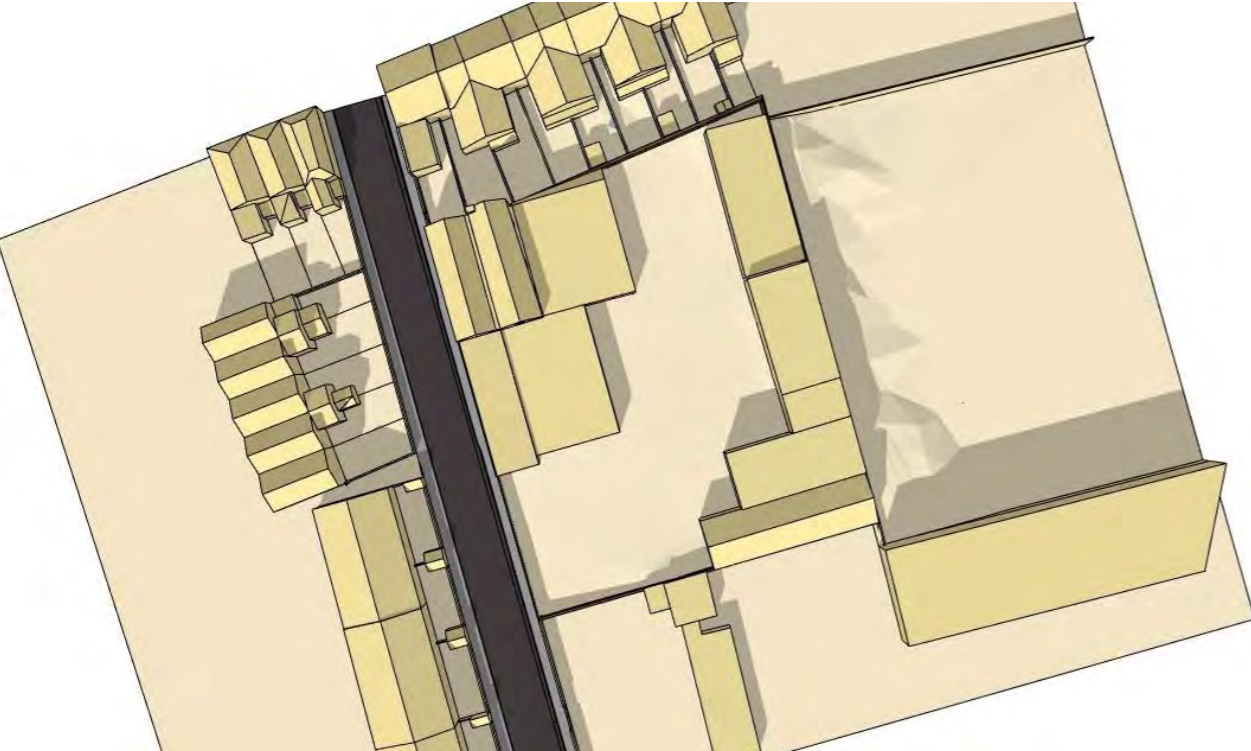


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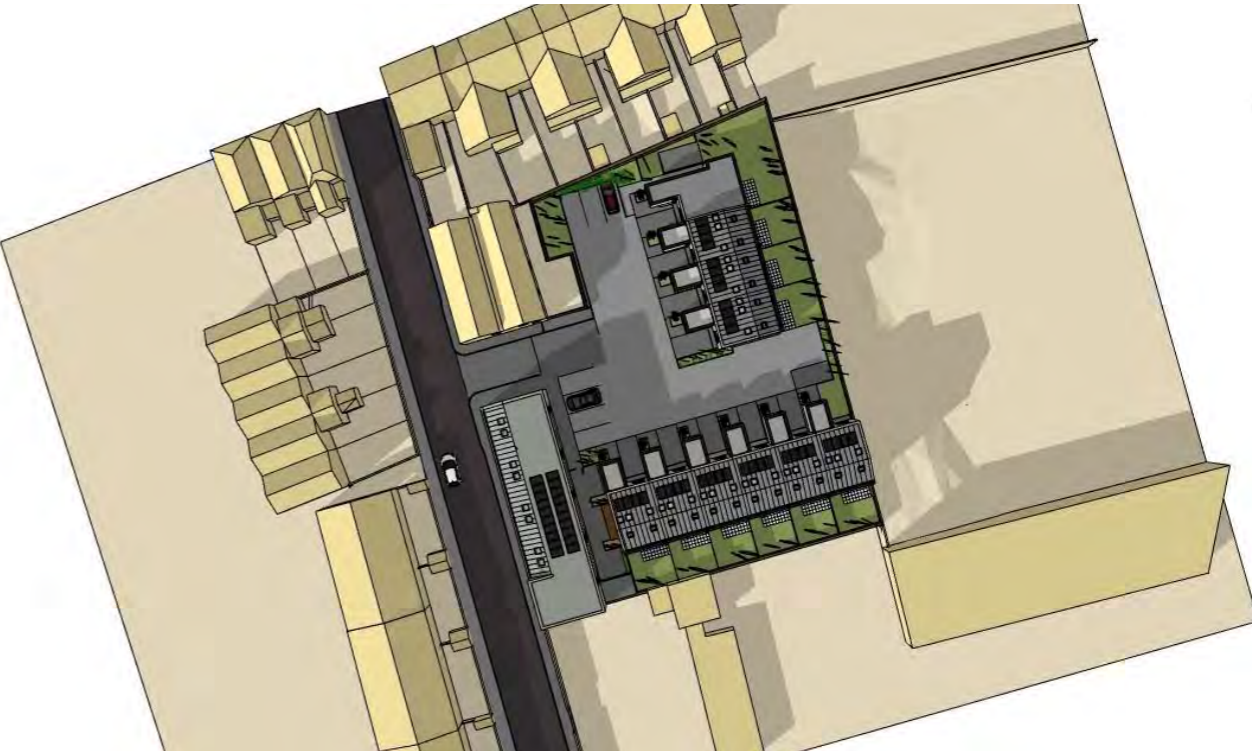
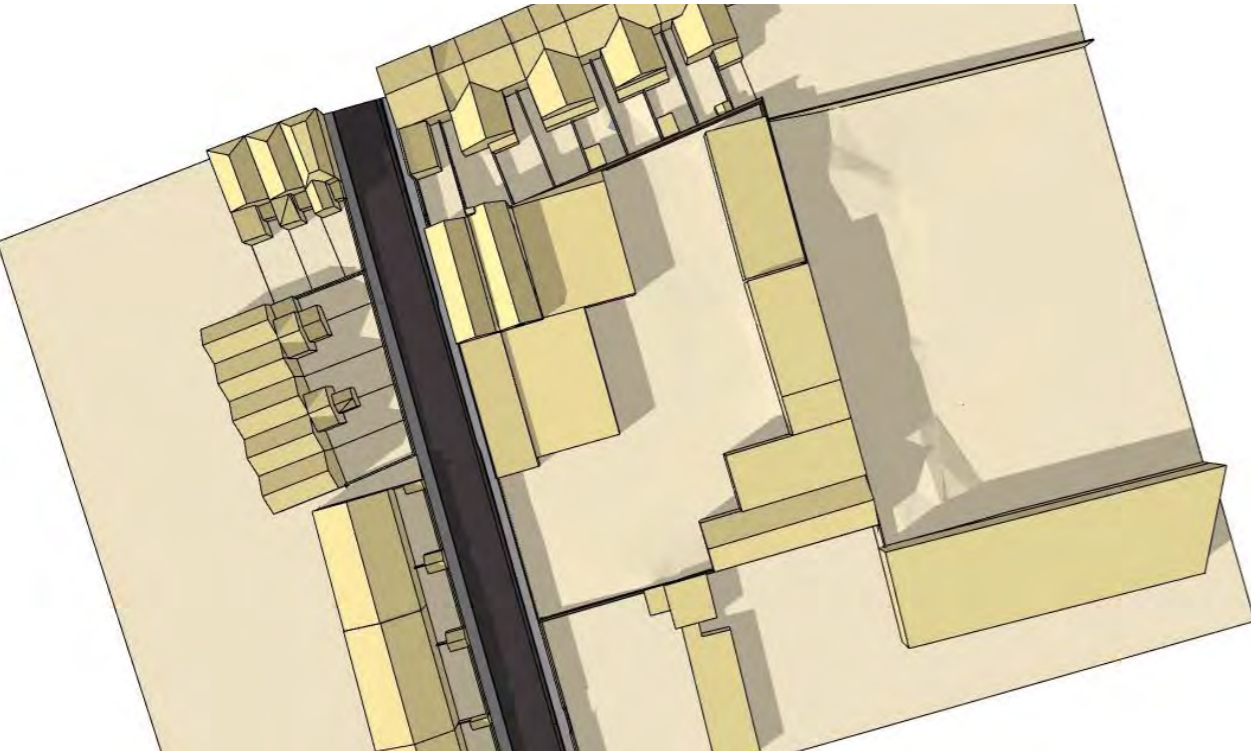
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21st SEPTEMBER - 3pm

21st SEPTEMBER - 3pm

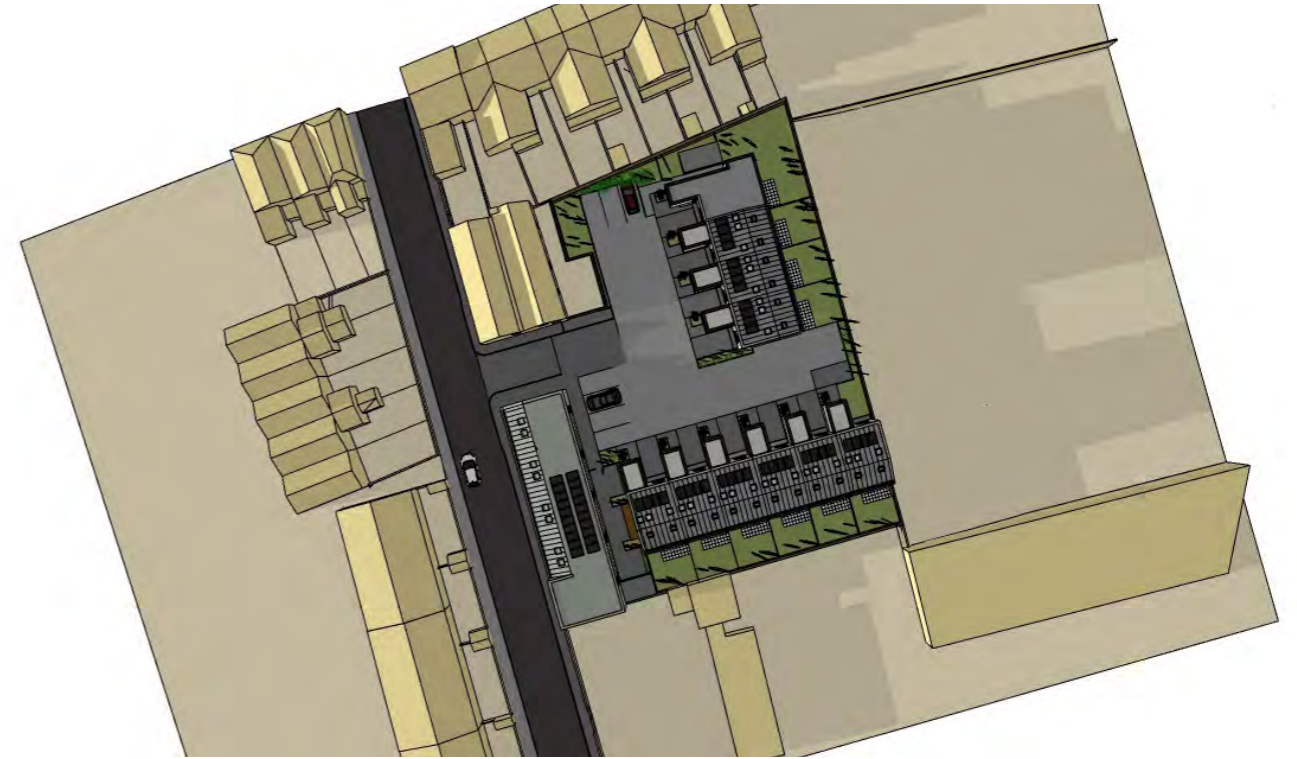
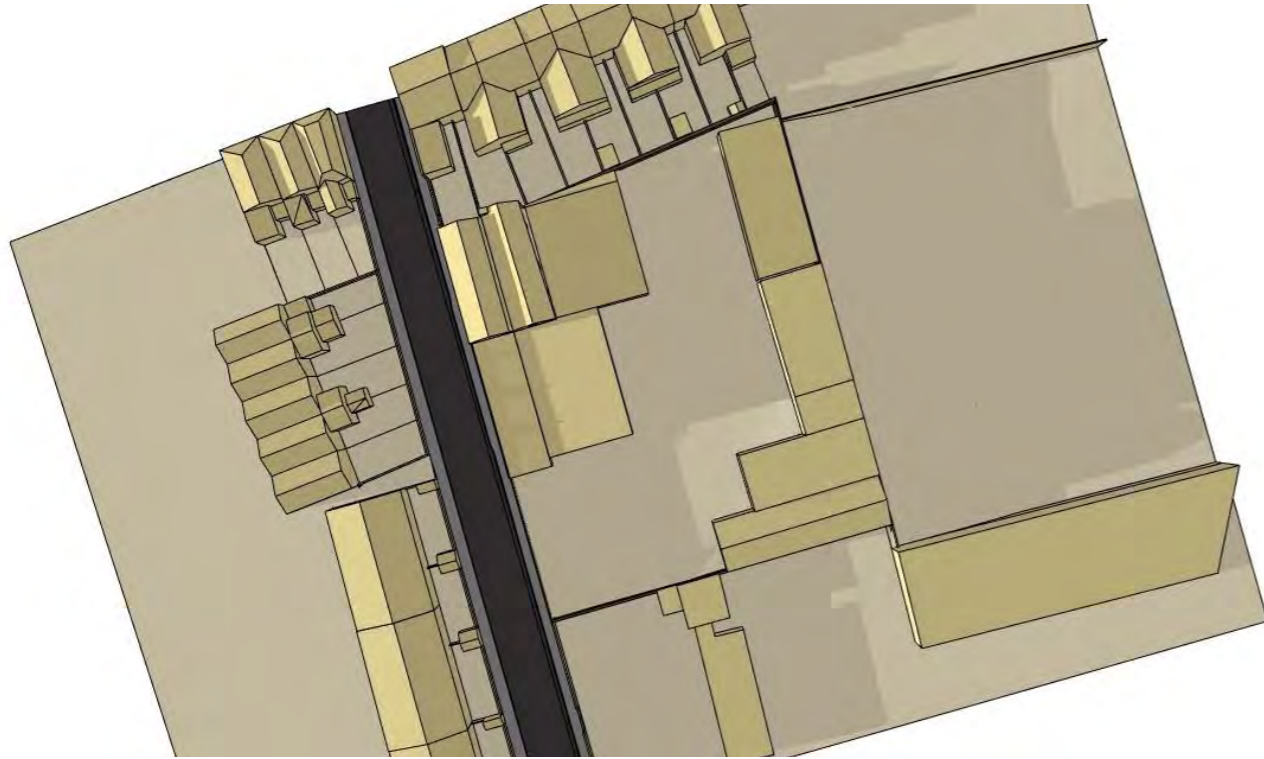


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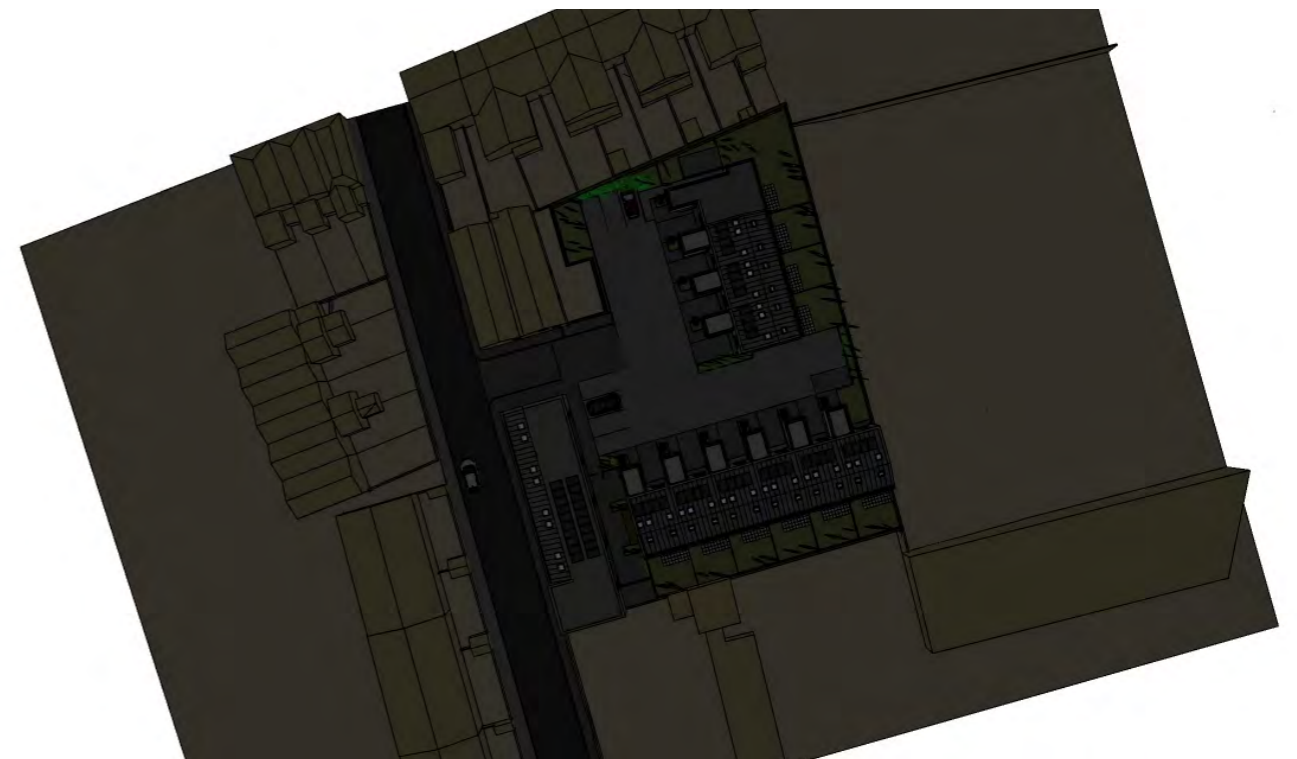
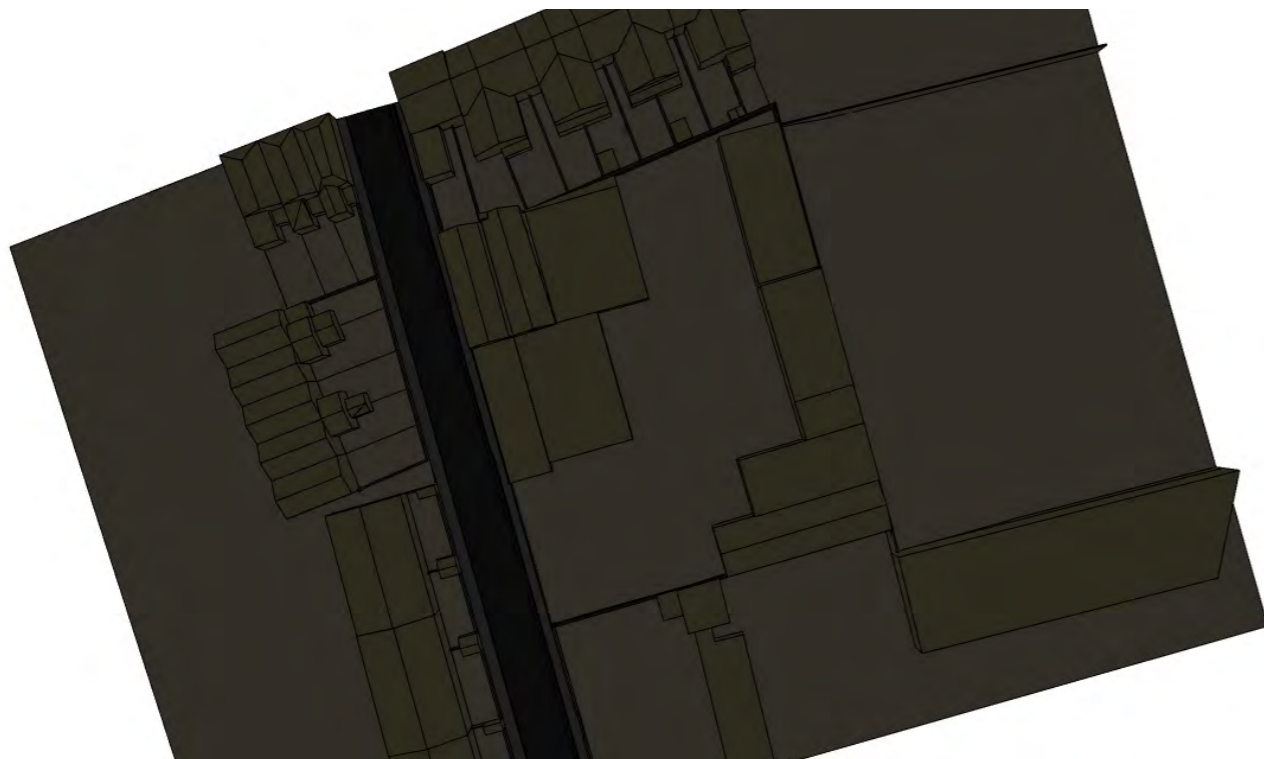
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21st SEPTEMBER - 7pm

21st SEPTEMBER - 7pm

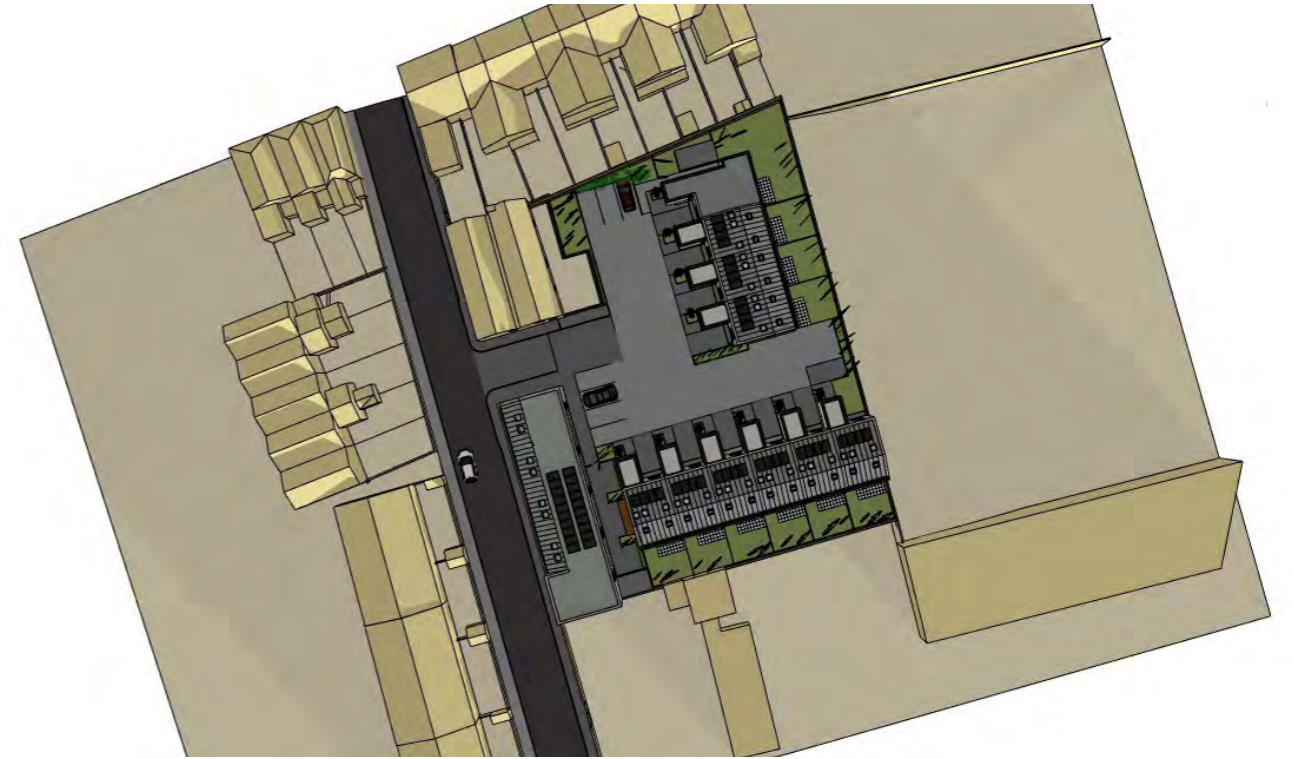
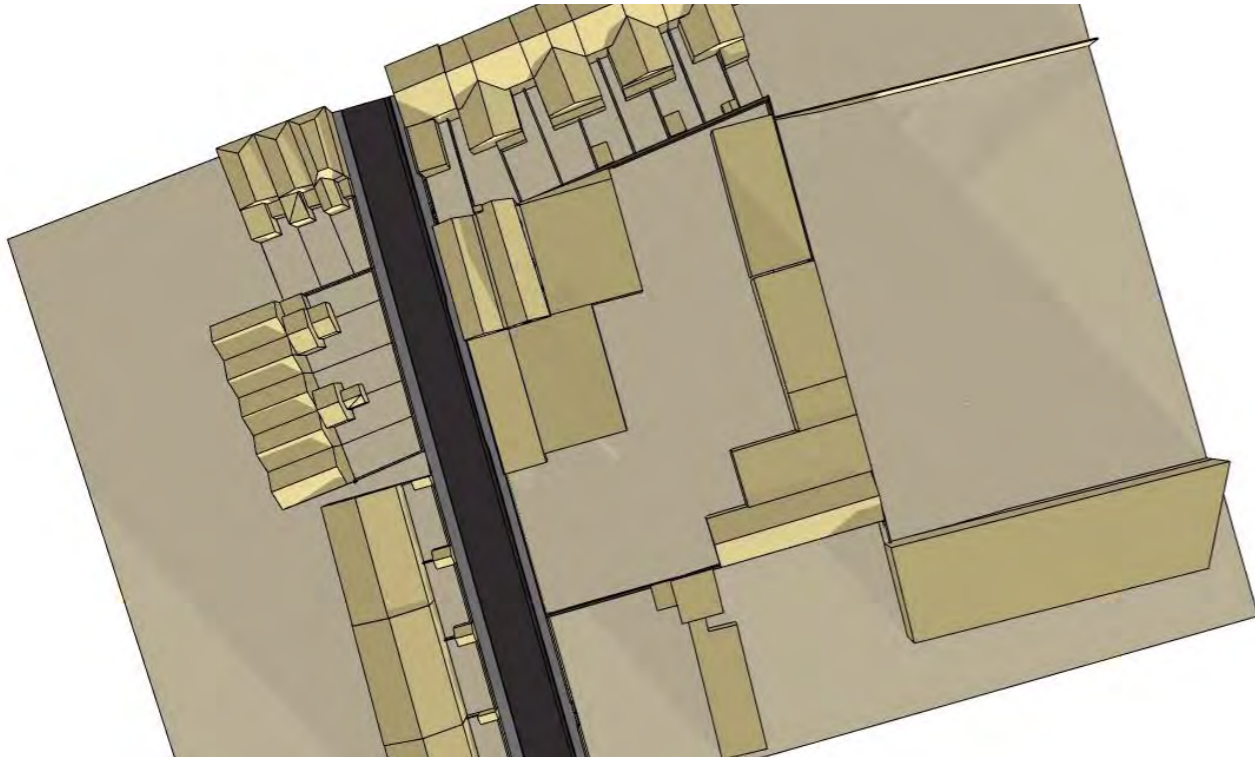


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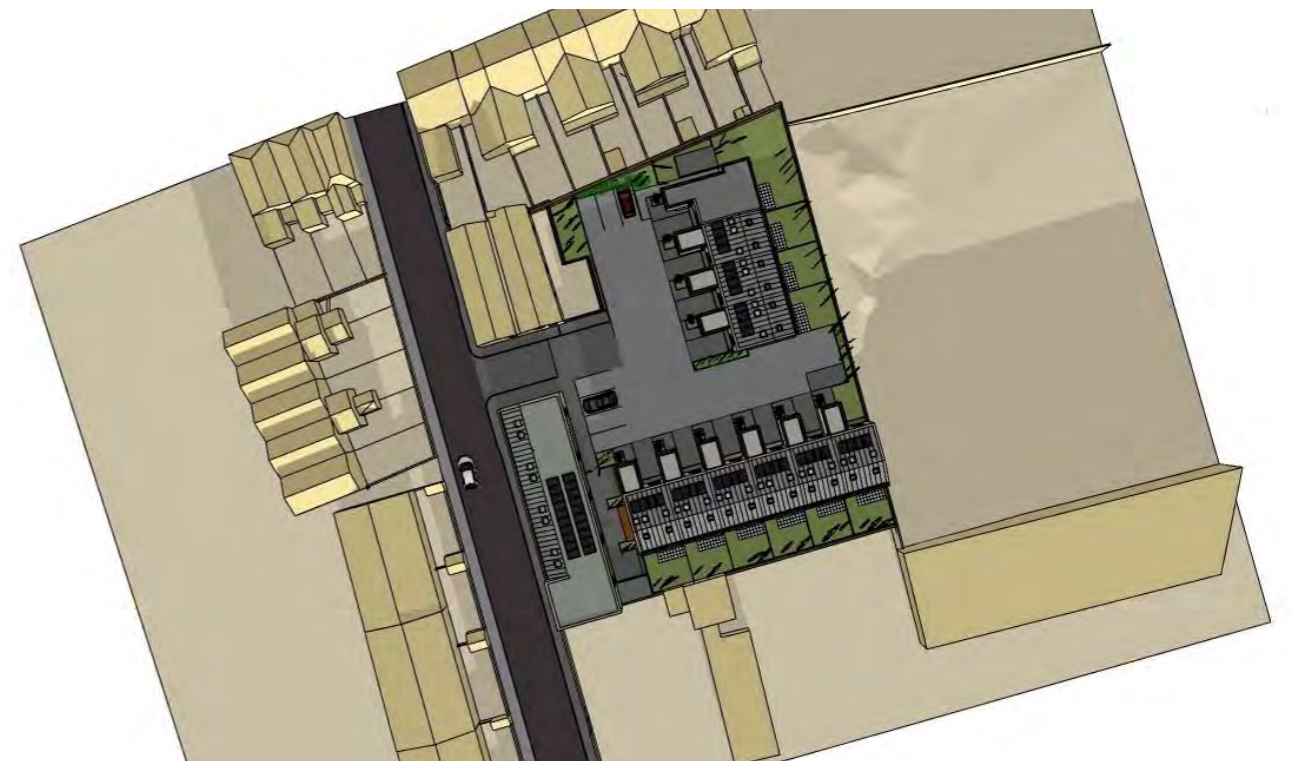
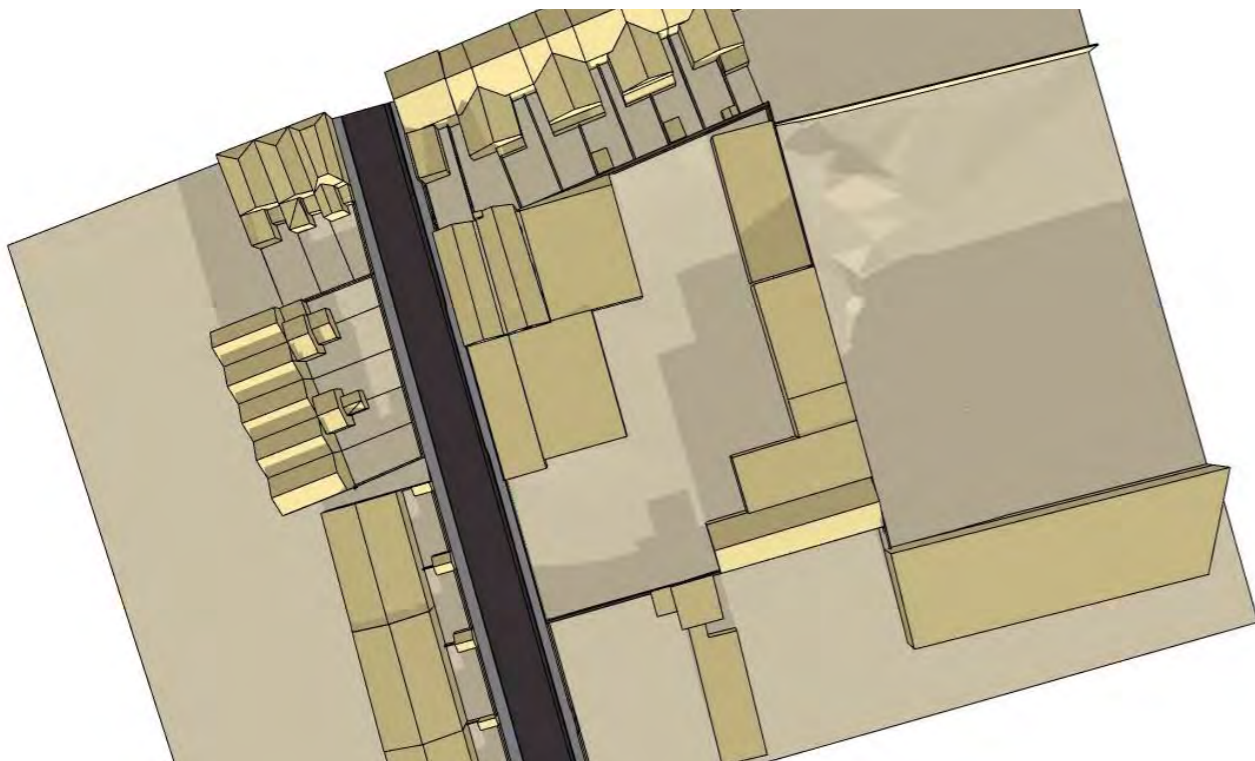
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Page 21

DECEMBER - 11am

21st DECEMBER - 11am

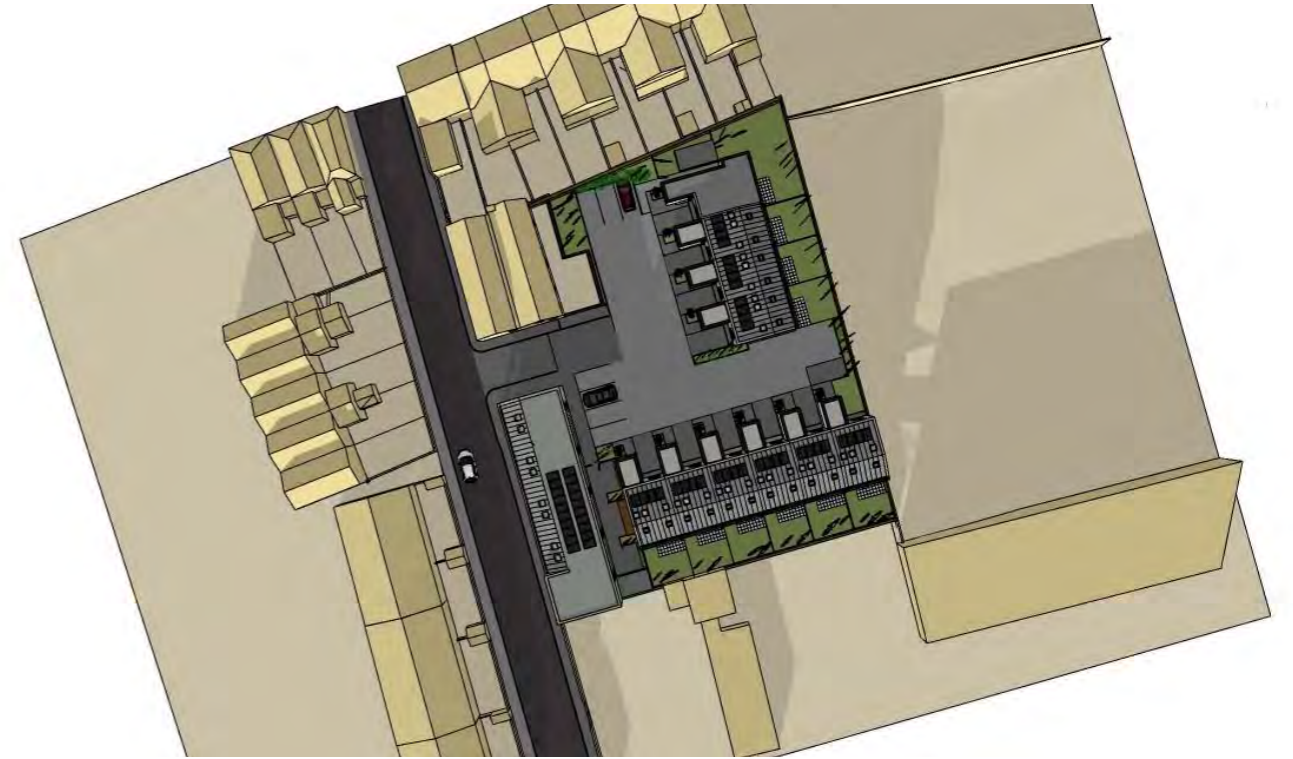
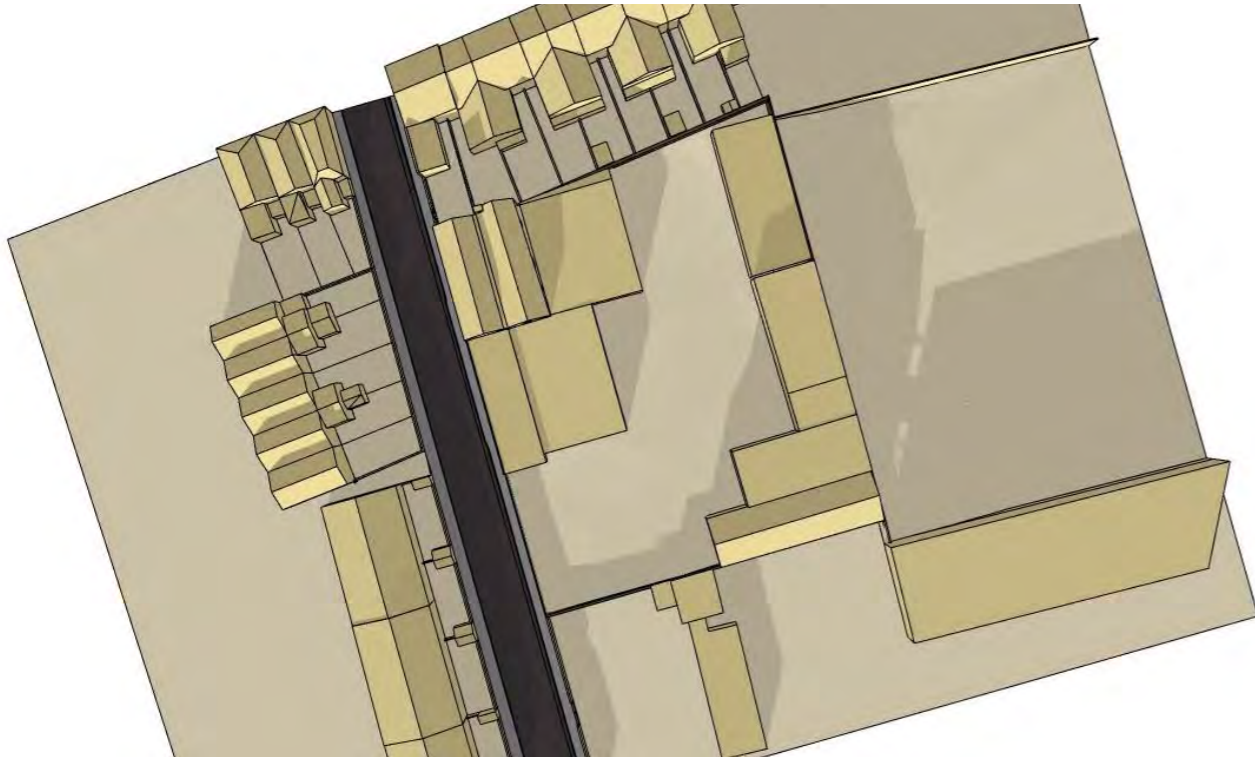


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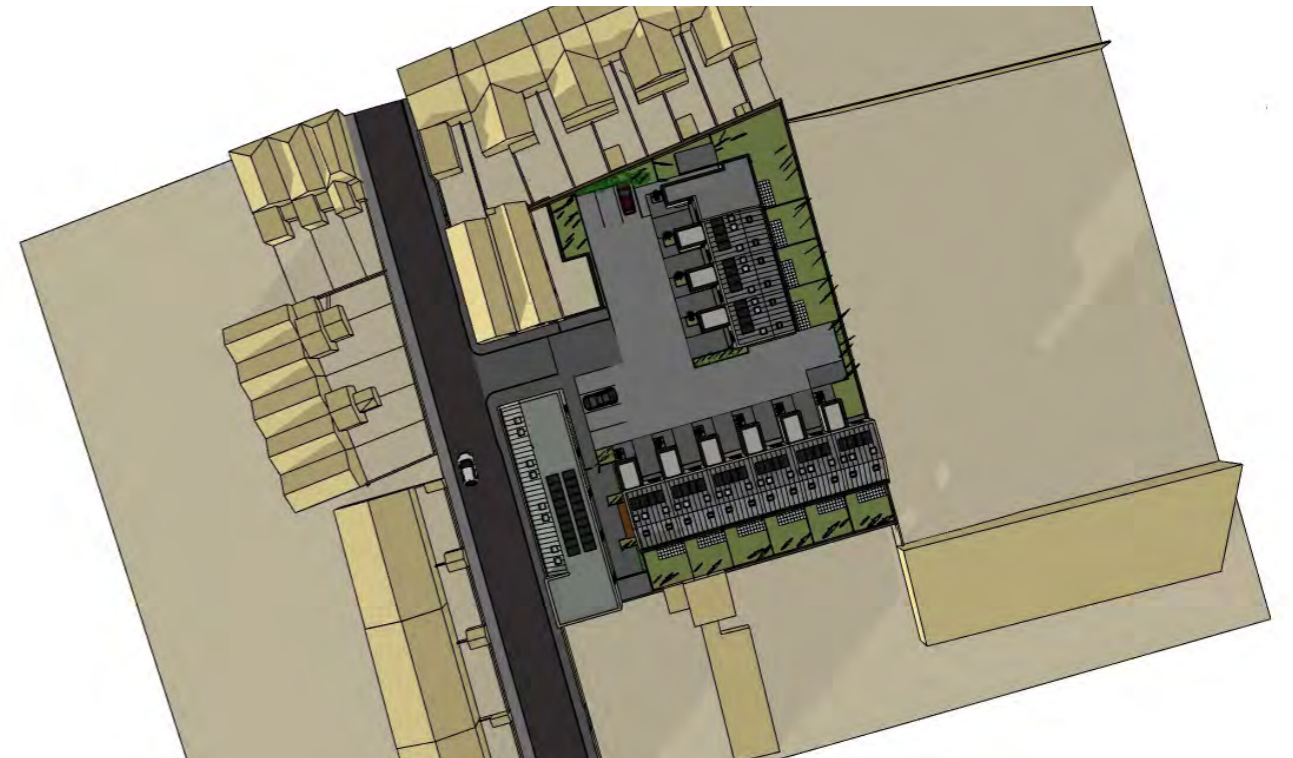
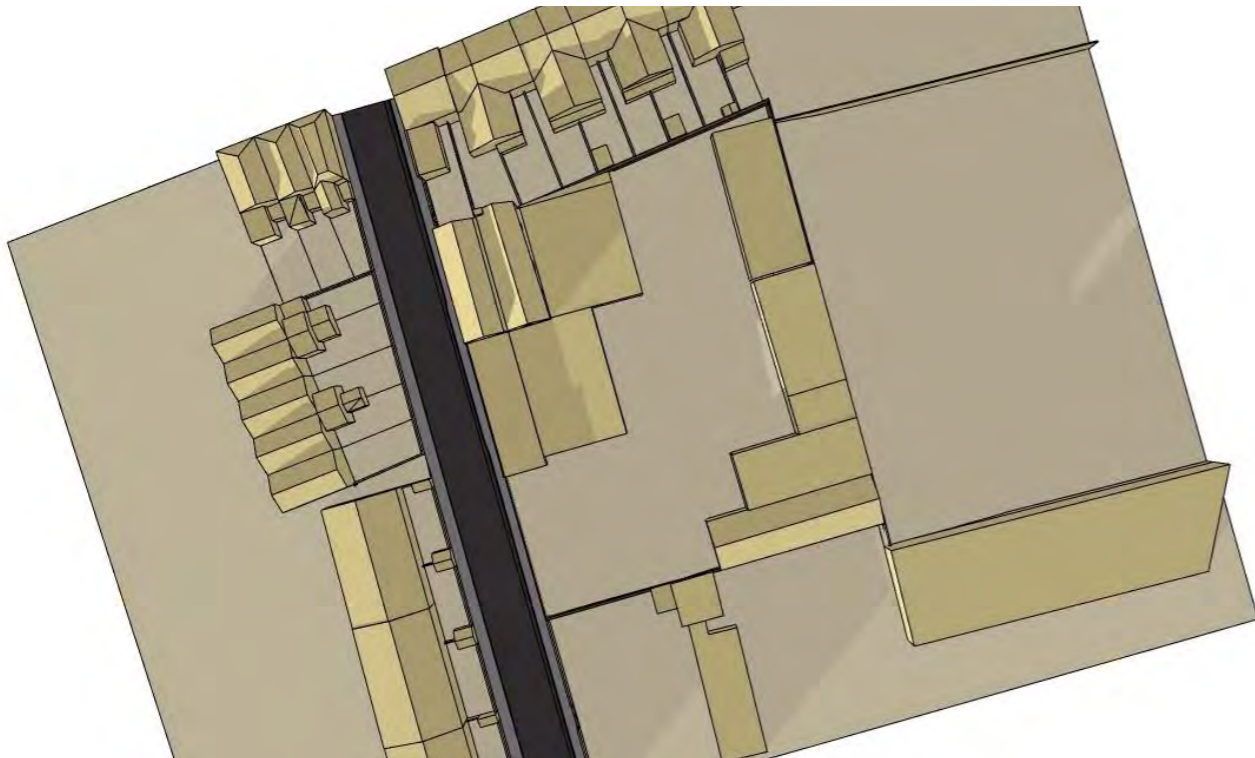
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DECEMBER - 3pm

21st DECEMBER - 3pm

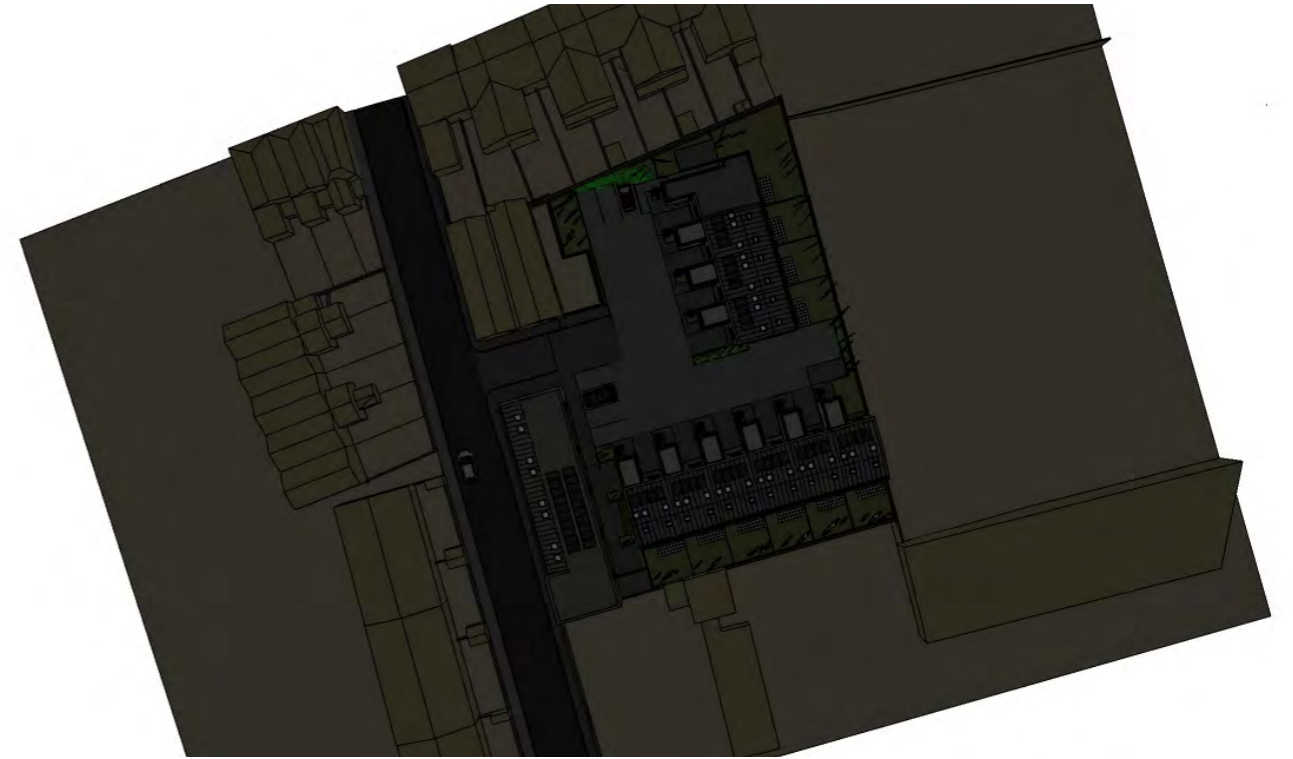
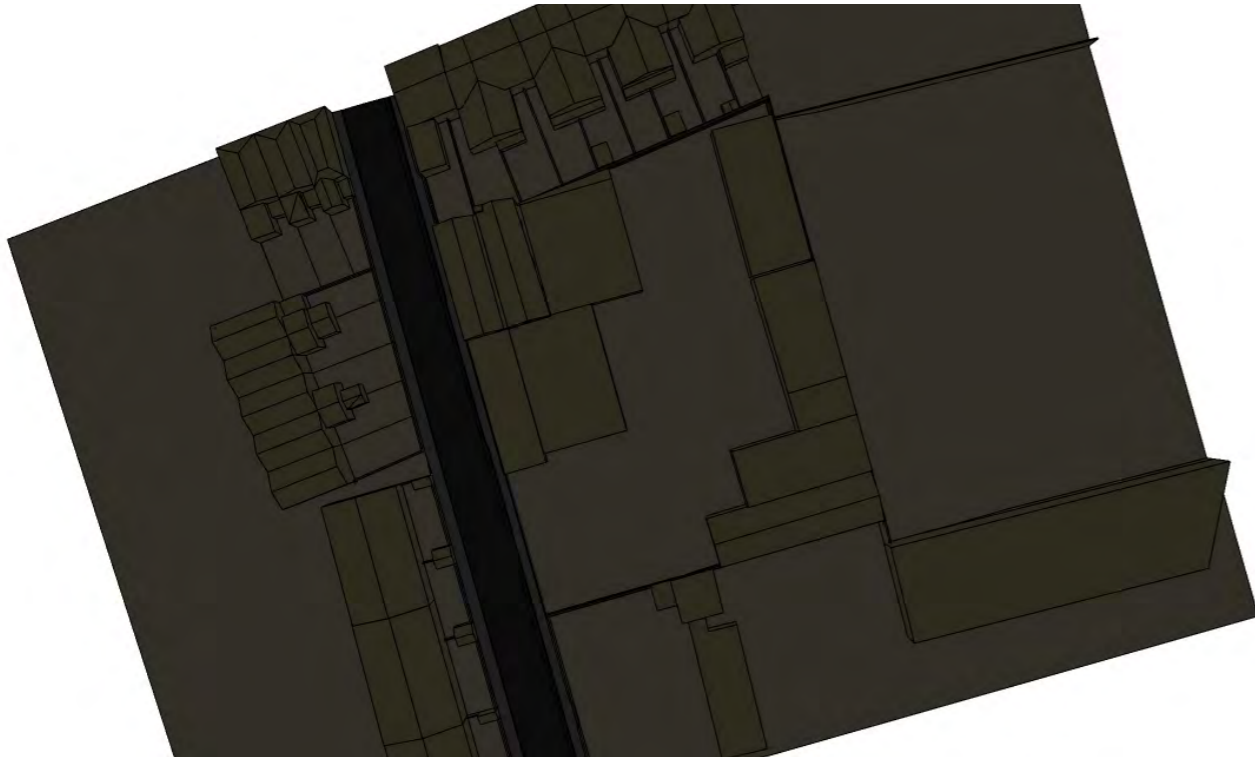


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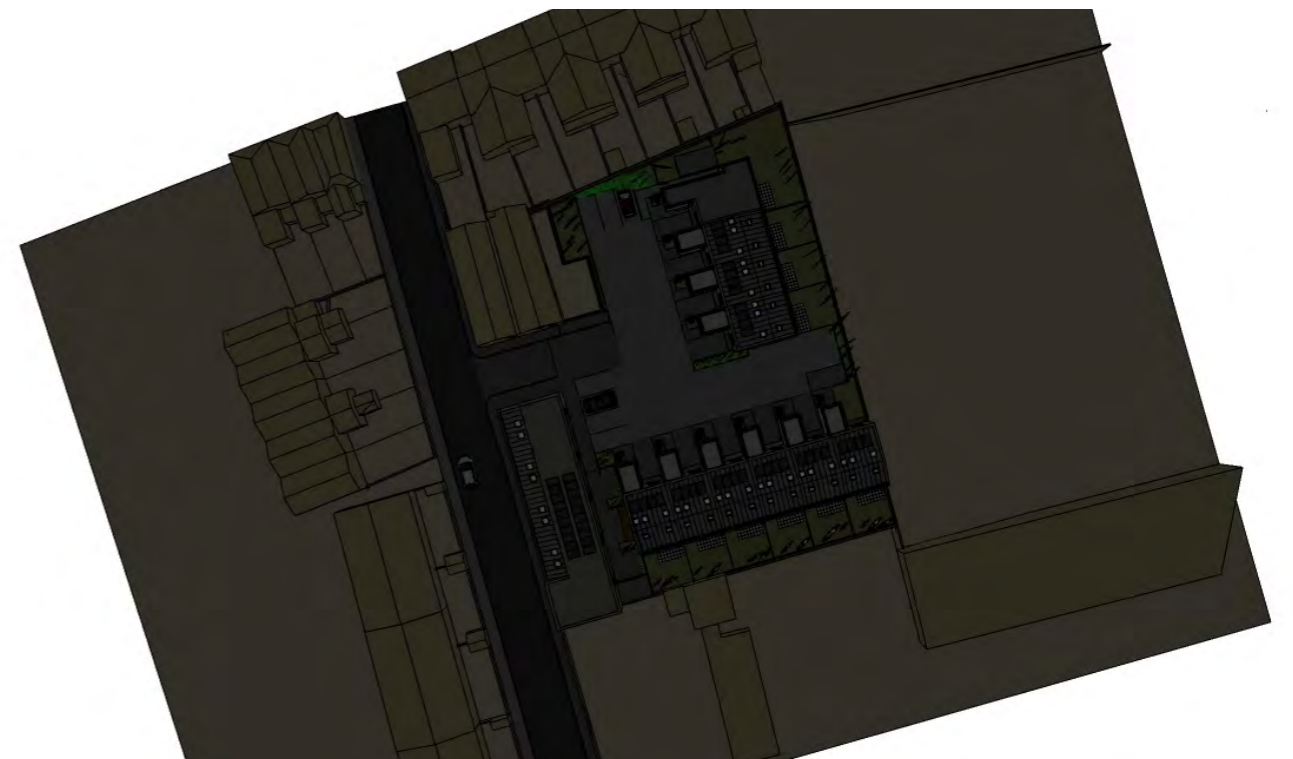
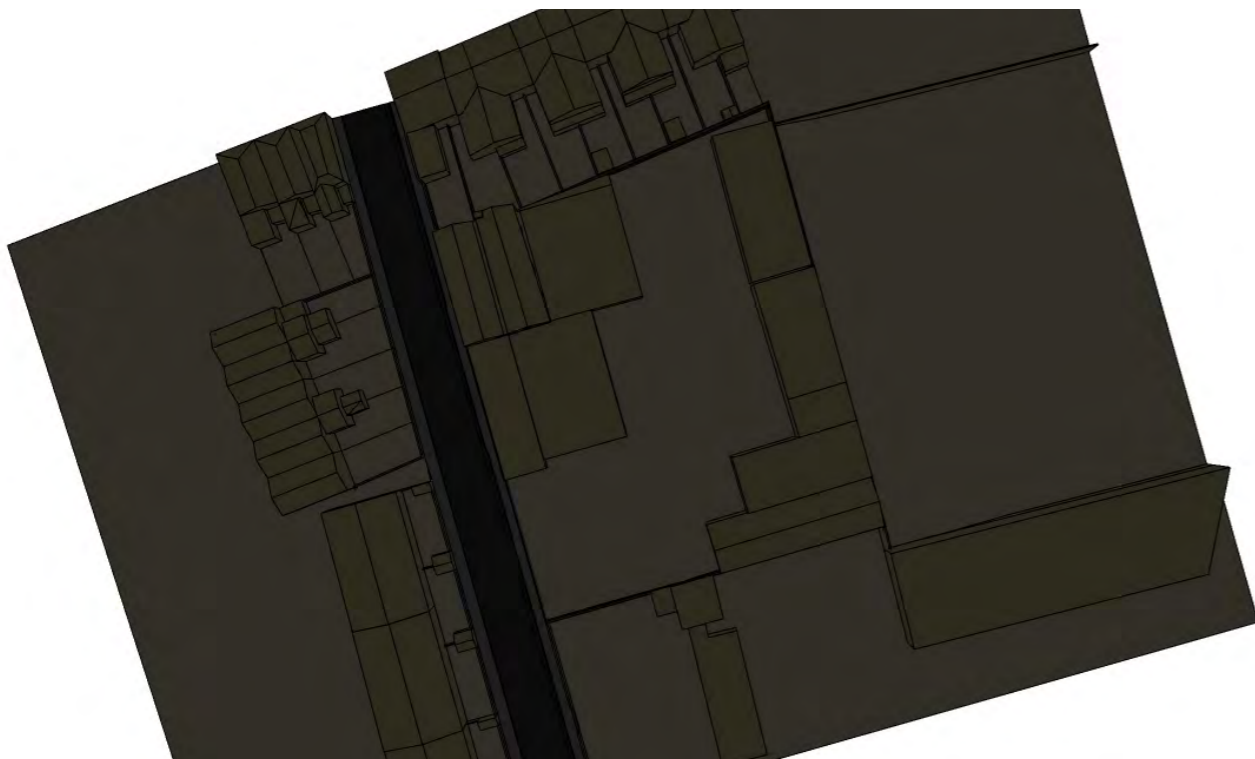
21st DECEMBER - 5pm



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DECEMBER - 7pm

21st DECEMBER - 7pm



Development Control Committee B – 13 March 2019

ITEM NO. 6

WARD: Brislington West CONTACT OFFICER: Jack Jones

SITE ADDRESS: 6 Alford Road Bristol BS4 3HS

APPLICATION NO: 19/00253/H Full Planning (Householders)

DETERMINATION DEADLINE: 14 March 2019

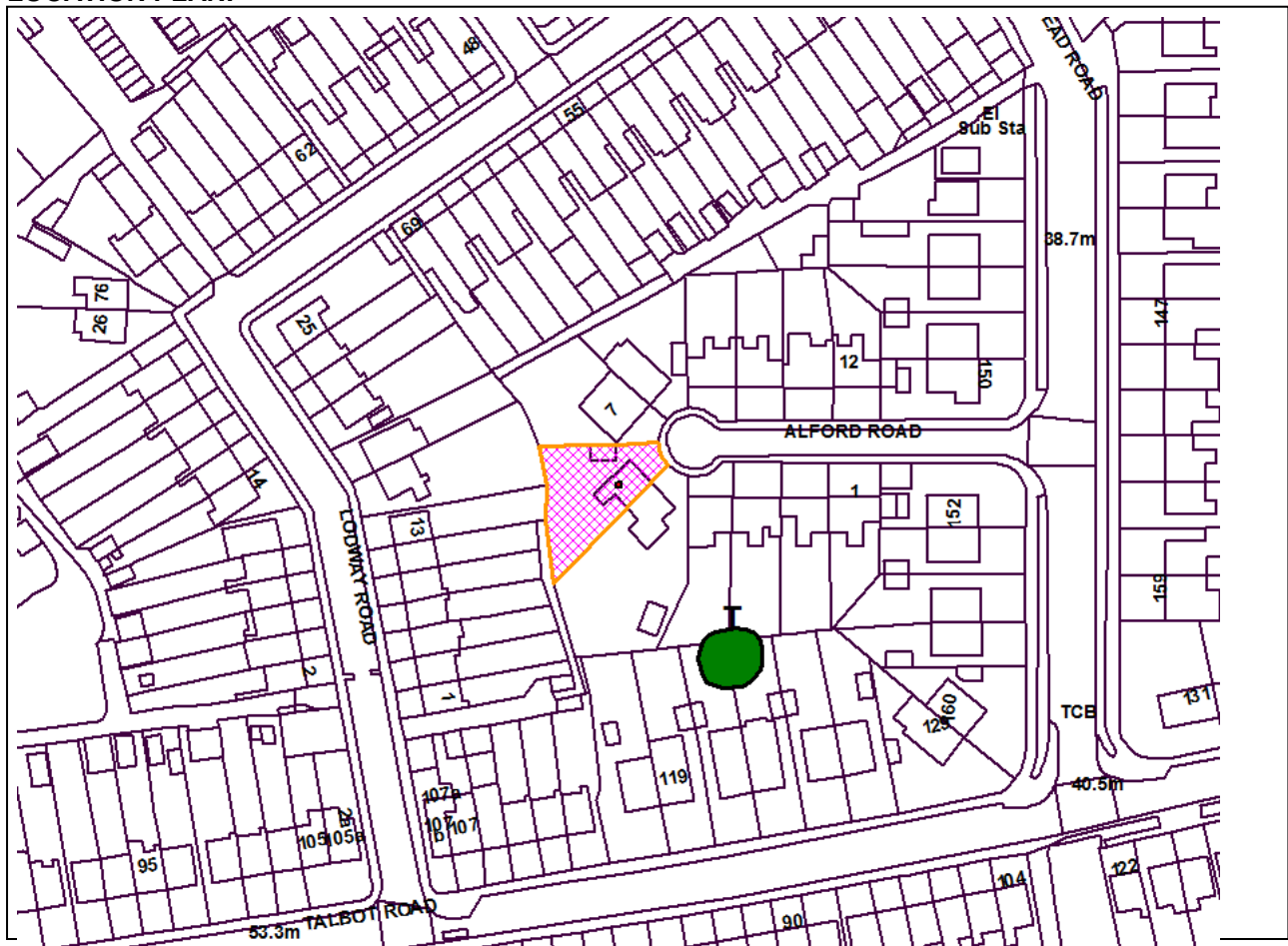
Single storey side/rear extension; replacement garage; and associated works.

RECOMMENDATION: Grant subject to Condition(s)

APPLICANT: Mr & Mrs Cross
6 Alford Road
Bristol
BS4 3HS

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



Development Control Committee B – 13 March 2019
Application No. 19/00253/H : 6 Alford Road Bristol BS4 3HS

BACKGROUND AND SUMMARY

This application is being presented to Development Control Committee due to the applicant being a Bristol City Council Planning Officer. Paragraph 12.1 of the Member and Officer Code of Conduct confirms that:

"Officers must not play any part in the processing of any application where they have, or can be perceived to have, a conflict of an interest. Such interests would include financial or professional interests and the interests of family and friends."

Householder planning permission is sought for the demolition of an existing single storey rear projection and detached garage, and the erection of a single storey rear and side extension and replacement garage.

Key issues in the assessment of the application are design and residential amenity.

The report below assesses the application against the relevant planning policies and guidance. The development is found to be acceptable in design and residential amenity terms. The recommendation of this report is that planning permission should be granted.

SITE DESCRIPTION

The application site is a two-storey semi-detached property set within a wedge-shaped curtilage of varying levels. It is located at the end of Alford Road; a cul-de-sac in the Brislington West Ward in south Bristol.

The property is not listed nor located in a conservation area.

RELEVANT PLANNING HISTORY

None

APPLICATION

This application seeks permission for the demolition of the existing single storey rear projection and detached garage, and the erection of a single storey rear and side extension and replacement garage.

The proposed extension would be situated on the North West and South West facing corner of the host dwelling. It would project 3.8m from the rear elevation and 3m from the side elevation. The extension would have a dual pitched roof; 4.2m in height to the ridge, dropping to 3m and 2.5m at the front and rear eaves respectively. Materials would be similar in appearance to the main dwelling.

The proposed replacement garage would be 1m greater in height than the existing garage; 1.5m greater in depth; and 1m greater in width. However, the overall form and design would remain the same. Materials would be similar in appearance to the existing garage.

Associated works include the excavation of part of the sloping garden to accommodate the extension and the creation of hardstanding and steps.

RESPONSE TO PUBLICITY AND CONSULTATION

Neighbouring properties were consulted on the proposed development with an expiry date for representations of 13th February 2019. No comments were received.

Development Control Committee B – 13 March 2019
Application No. 19/00253/H : 6 Alford Road Bristol BS4 3HS

RELEVANT POLICIES

SPD2 A Guide for Designing House Alterations and Extensions (October 2005)

National Planning Policy Framework – February 2019

Bristol Local Plan comprising Core Strategy (Adopted June 2011), Site Allocations and Development Management Policies (Adopted July 2014) and (as appropriate) the Bristol Central Area Plan (Adopted March 2015) and (as appropriate) the Old Market Quarter Neighbourhood Development Plan 2016 and Lawrence Weston Neighbourhood Development Plan 2017.

In determining this application, the Local Planning Authority has had regard to all relevant policies of the Bristol Local Plan and relevant guidance.

KEY ISSUES

A. IS THE DESIGN OF THE PROPOSED EXTENSION ACCEPTABLE?

Policy BCS21 states that new development should be of high quality, and should contribute positively to an area's character and identity.

Policy DM26 sets out that the layout and form of new development should contribute to the creation of quality urban design. The height, scale and massing of development should be appropriate to the immediate context.

Policy DM30 sets out the design principles for alterations to existing buildings. Extensions to existing buildings are expected to respect the siting, scale, form, proportions, materials and the overall design and character of both the host building and the surrounding area.

SPD2 states that single storey rear extensions should not extend any further than 3.5 metres from the rear of the existing dwelling. It also states that side extensions should be subservient in siting and width terms, and that garages should generally respect the scale, character and materials of the host property and, ideally, be set back from the main building line.

The proposed extension would have no material impact on the visual balance of the pair given the two storey side and rear extension at the attached property (no. 5). The extension would therefore be appropriate to the immediate context.

The siting of the extension at the rear corner of the dwelling means that the extension would not appear prominent from the public realm. Therefore, the 3.8m depth is acceptable in this instance. The width of the extension would be no more than half the width of the existing dwelling and the materials would be similar in appearance to the main dwelling. The extension would therefore be fully subservient to the host dwelling and would respect its' scale, form, proportions, materials and overall design and character.

The replacement garage would include the footprint of the existing garage. Despite the proposed 1m, 1.5m and 1m increases in existing garage height/depth/width respectively, the proposed garage would respect the scale and character of the host property as the building would remain subservient and the materials would match those used currently. It would be set back from the main building line.

The proposed excavation, hardstanding and steps have no detrimental scale, design or appearance implications given the existing context.

In summary, the proposal is considered to accord with policies BCS21, DM26 and DM30 as well as SPD2 guidance and would be acceptable in scale, design and external appearance terms.

Development Control Committee B – 13 March 2019
Application No. 19/00253/H : 6 Alford Road Bristol BS4 3HS

B. WOULD THE PROPOSAL BE ACCEPTABLE IN TERMS OF RESIDENTIAL AMENITY?

Policy BCS21 states that new development should safeguard the amenity of existing development.

Policy DM30 states that alterations to existing development should safeguard the amenity of the host premises and neighbouring occupiers and leave sufficient usable external private space for the occupiers of the building.

SPD2 states that rear extensions should be designed so as not to cross a 45° line (on plan and elevation) projected from the mid-point of an adjoining neighbour's nearest ground floor habitable room window which is perpendicular to the proposed extension.

The proposed rear extension would pass the 45° test on plan and elevation in respect of attached property (no. 5). The siting of the extension, with significant gap to the boundaries on both sides, means that the extension would not have a harmful residential amenity impact on any of the surrounding properties by way of overbearing, loss of light, or loss of outlook. The proposed side facing windows would not have a harmful overlooking impact as they are at ground floor level and the existing boundary treatments would prevent a material impact.

There are no side facing windows on the elevation of no. 7 adjacent to the proposed garage, the area in between not being of any significant amenity value given the size of the gardens in the area. Therefore the proposed garage would not have a harmful overbearing, loss of light, or loss of outlook impact on no. 7.

The proposed excavation, hardstanding and steps would not result in any harm to residential amenity given the existing context.

In summary, it is considered that the proposed development would not be harmful to surrounding residential amenity and would accord with policies BCS21, DM30 and SPD2 guidance.

C. WOULD THE PROPOSAL BE ACCEPTABLE IN TERMS OF THE PRESERVATION OF GREEN INFRASTRUCTURE?

For the avoidance of doubt 'T1' as marked on the proposed site plan (to be retained) is a shrub, not a tree. Therefore, it is not required to be protected or replaced under the terms of the Bristol Tree Replacement Standard. This has been confirmed by BCC Arboriculture Officers.

CONCLUSION

The proposed development is considered to comply with Bristol City Council policy. The design is considered to be acceptable and there would be no detrimental impact on residential amenity. It is recommended that planning permission be approved for the proposed development.

HOW MUCH COMMUNITY INFRASTRUCTURE LEVY (CIL) WILL THIS DEVELOPMENT BE REQUIRED TO PAY?

Development of less than 100 square metres of new build that does not result in the creation of a new dwelling; development of buildings that people do not normally go into, and conversions of buildings in lawful use, are exempt from CIL. This application falls into one of these categories and therefore no CIL is payable.

Development Control Committee B – 13 March 2019
Application No. 19/00253/H : 6 Alford Road Bristol BS4 3HS

RECOMMENDED GRANTED subject to condition(s)

Time limit for commencement of development

1. Full Planning Permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

List of approved plans

2. List of approved plans and drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

P11-CP-015-005 - REV P03 Location and site plans, received 17 January 2019
P11-CP-015-001 - REV P03 Existing ground floor and roof plans, received 17 January 2019
P11-CP-015-002 - REV P03 Proposed floor and roof plans, received 17 January 2019
P11-CP-015-006 - REV P03 Existing and proposed garage plans, received 17 January 2019
P11-CP-015-003 - REV P03 Existing and proposed front and rear elevations, received 17 January 2019
P11-CP-015-004 - REV P03 Existing and proposed side elevations, received 17 January 2019

Reason: For the avoidance of doubt.

Advices

- 1 The proposed development lies within a coal mining area which may contain unrecorded coal mining related hazards. If any coal mining feature is encountered during development, this should be reported immediately to the Coal Authority on 0345 762 6848.

Further information is also available on the Coal Authority website at:
www.gov.uk/government/organisations/the-coal-authority

Supporting Documents

6. 6 Alford Road, Brislington, Bristol, BS4 3HS

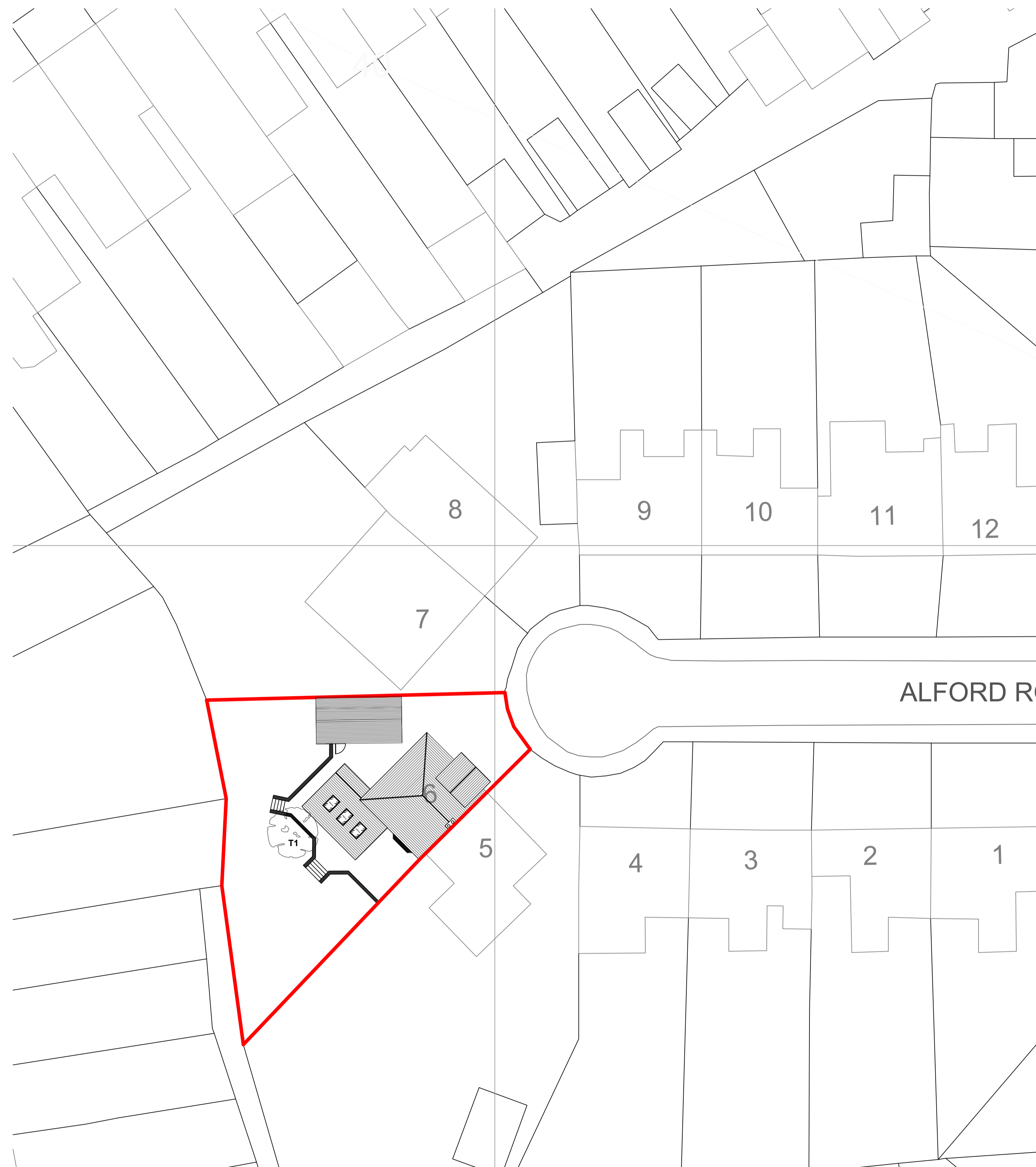
1. Proposed site plan
2. Existing and proposed garage
3. Proposed ground floor plan
4. Proposed front and rear elevations
5. Proposed side elevations



Produced on 02 November 2018 from the Ordnance Survey National Geographic Database and incorporating surveyed revision available at this date.
 This map shows the area bounded by 361254,170761 361254,170861 361354,170861 361354,170761
 Reproduction in whole or part is prohibited without the prior permission of Ordnance Survey.
 Crown copyright 2018. Supplied by copla ltd trading as UKPlanningMaps.com a licensed Ordnance Survey partner (100054135).
 Data licenced for 1 year, expiring 02 November 2019. Unique plan reference: v1b/293697/399612

Location Plan

1:1250



Proposed Site Plan

1:200

P03 Planning Permission (Clients Comments) 04/01/2019 CM CM
 P02 Planning Permission (Clients Comments) 27/11/2018 CM CM
 P01 Planning Permission 28/10/2018 CM CM

Revisions

Project

**6 Alford Road,
 Bristol,
 BS4 3HS**

Client

Mr and Mrs Cross

Title

Location and Site Plans

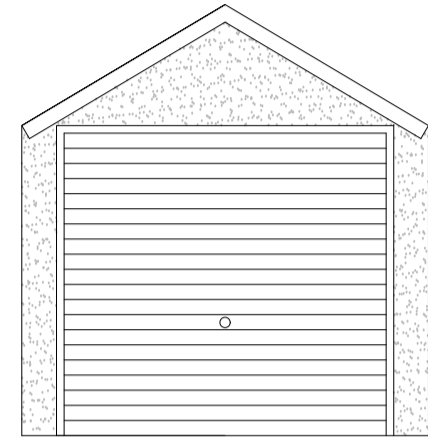
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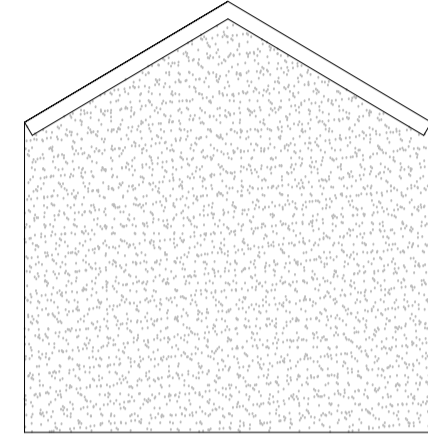
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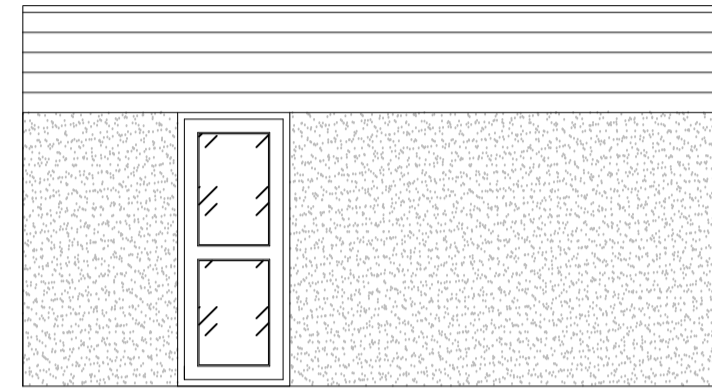
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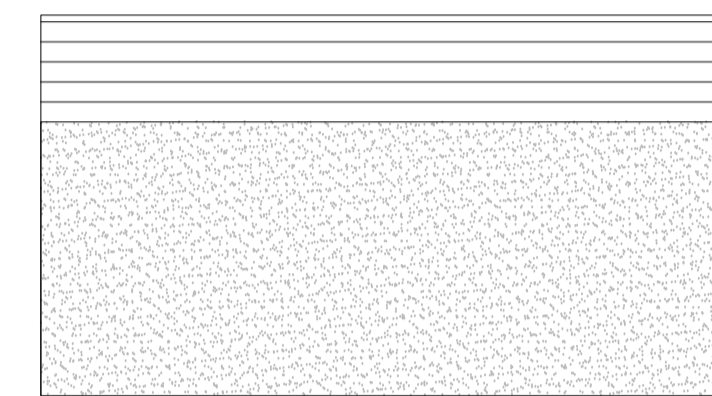
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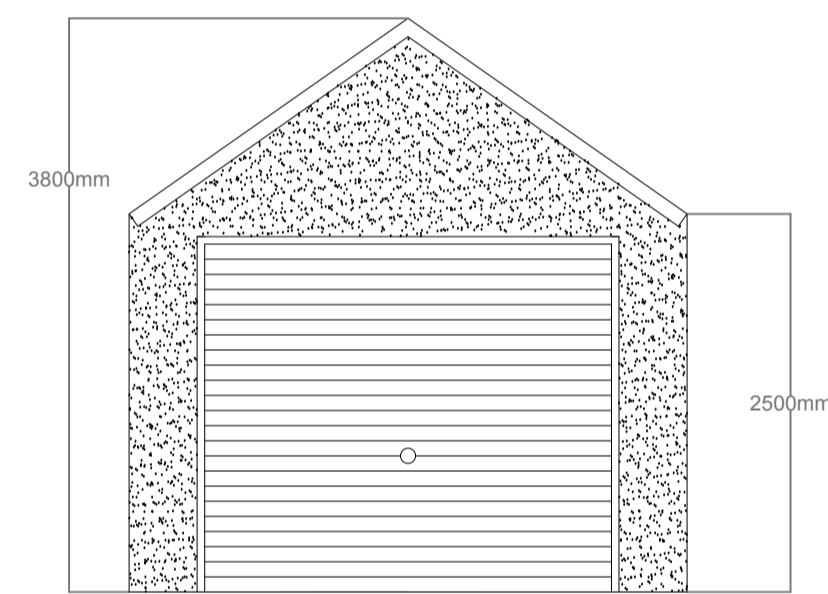
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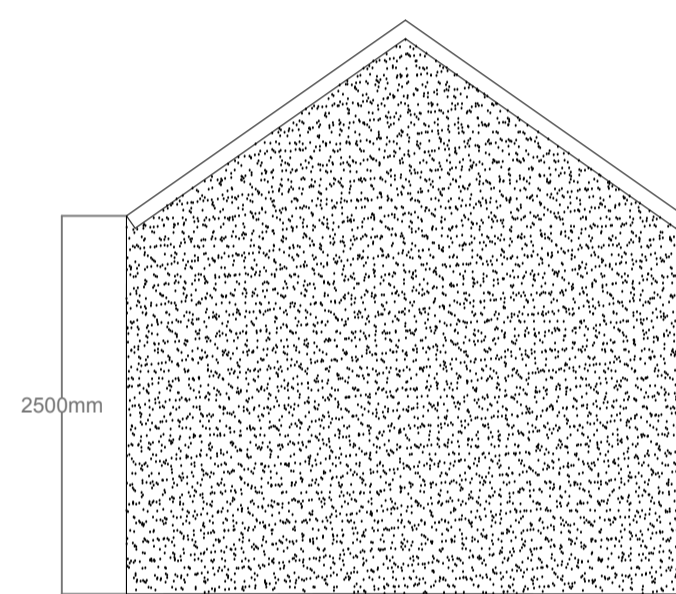
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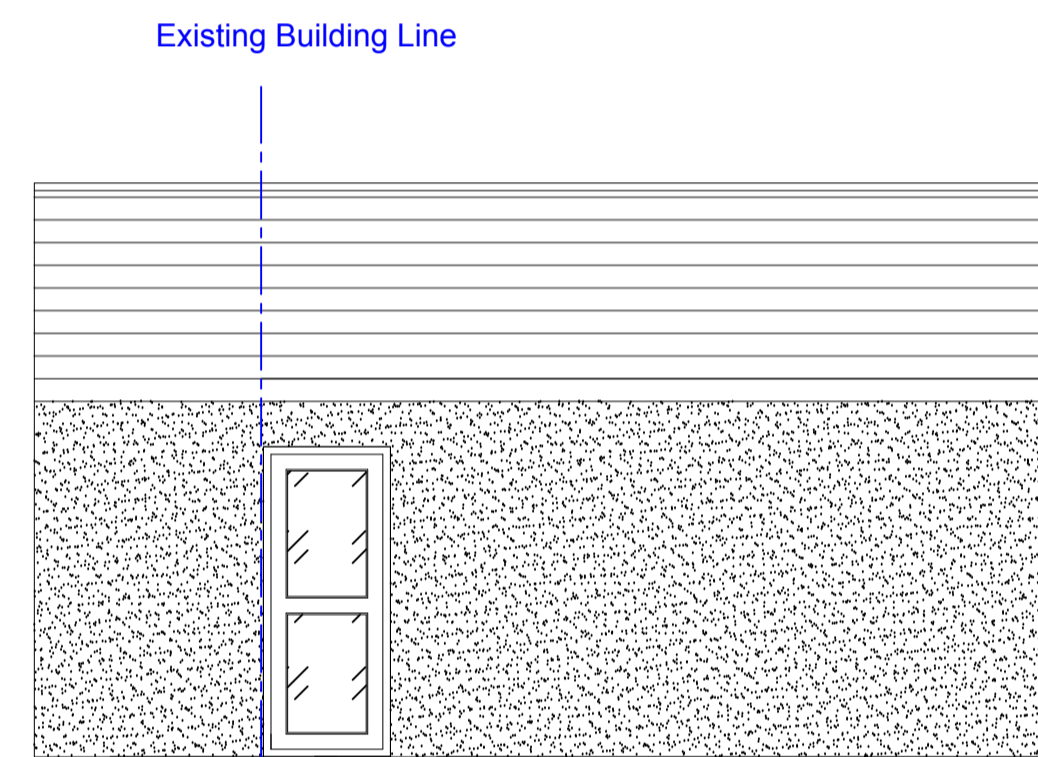
Proposed Front Elevation



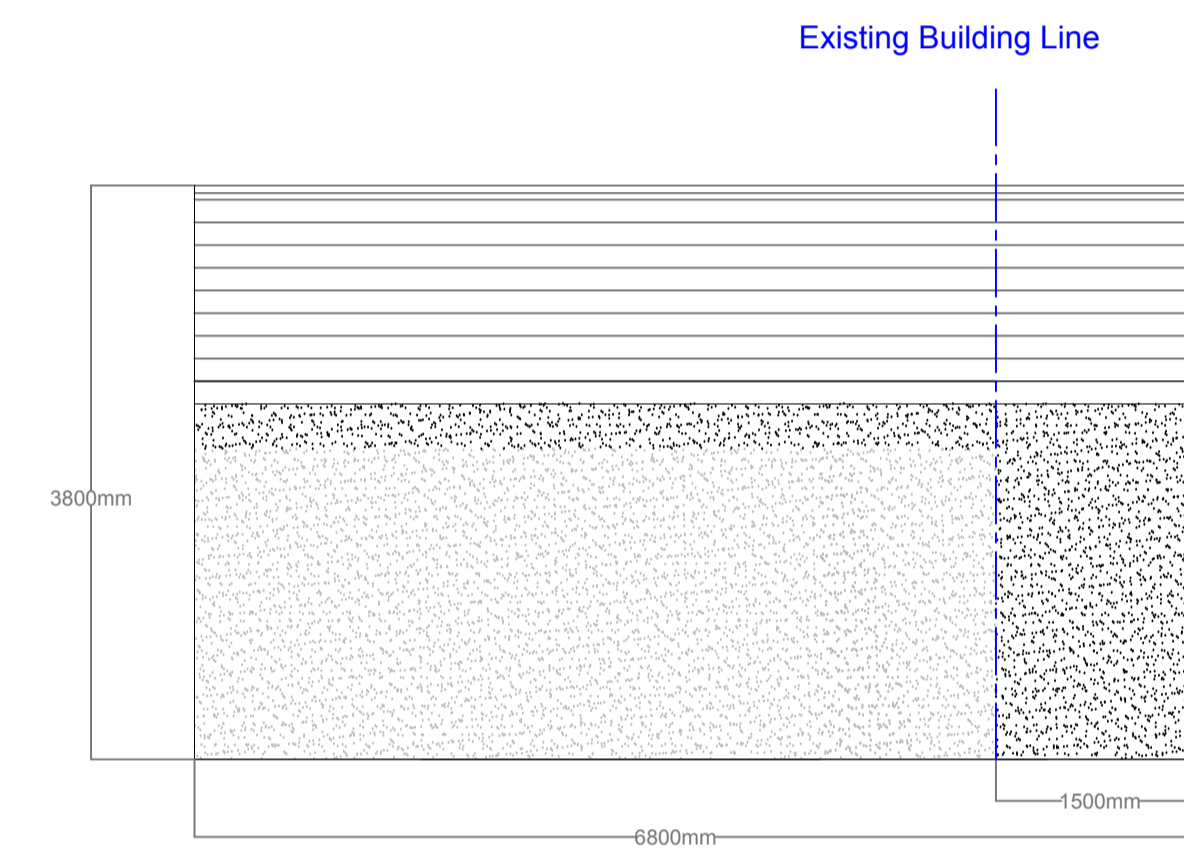
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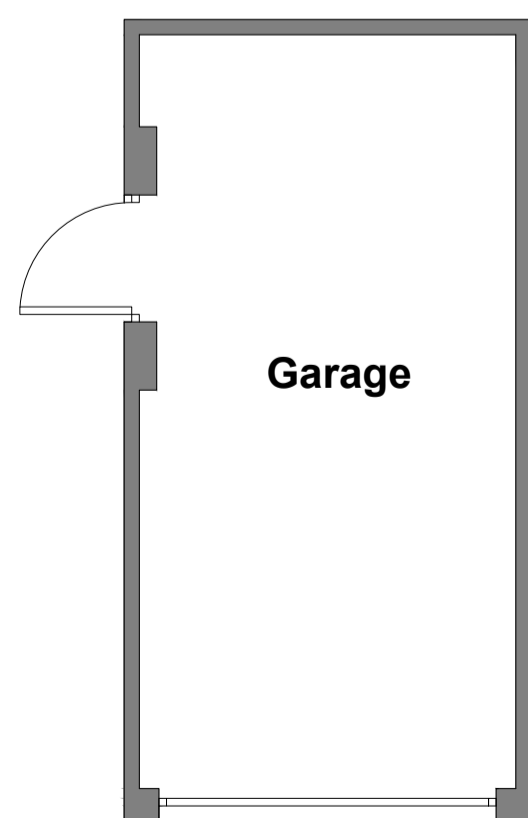
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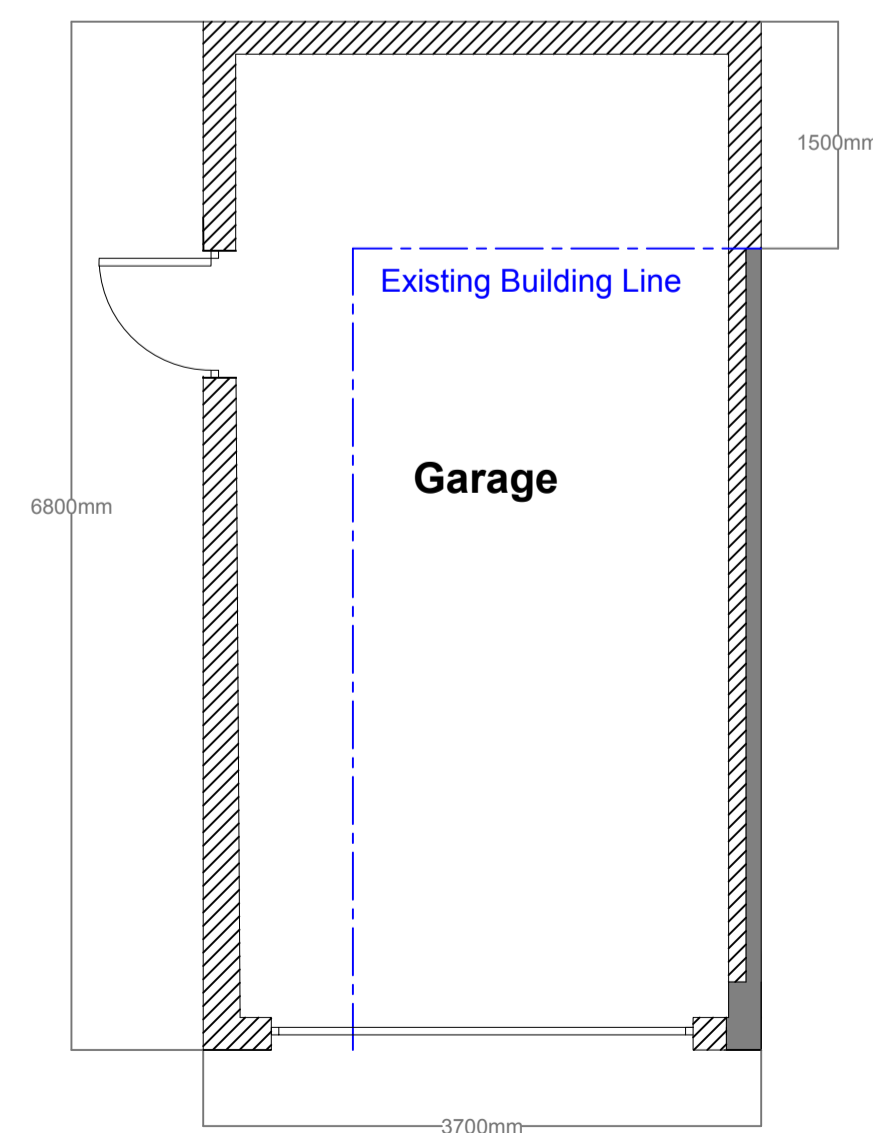
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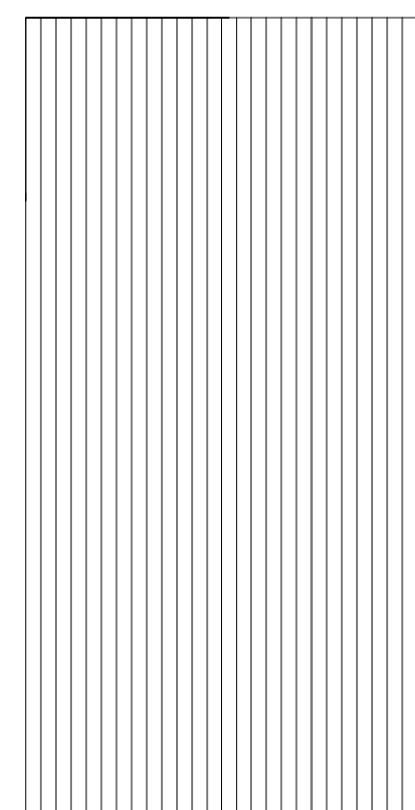
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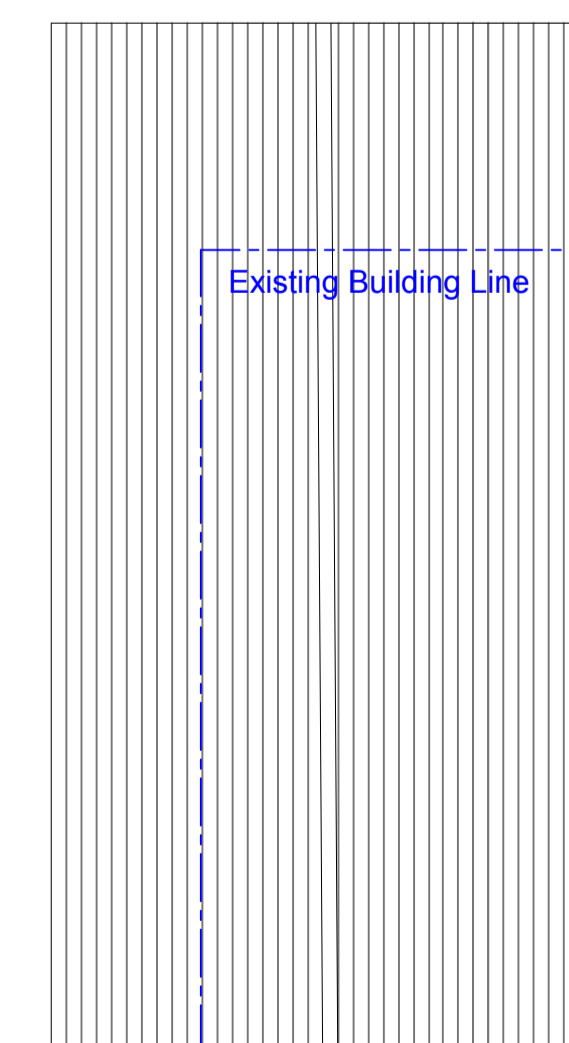
Proposed Floor Plan



Existing Roof Plan



Proposed Roof Plan



P03 Planning Permission (Clients Comments) 04/01/2019 CM
 P02 Planning Permission (Clients Comments) 27/11/2018 CM
 P01 Planning Permission 28/10/2018 CM

Revisions
 Project

**6 Alford Road,
 Bristol,
 BS4 3HS**

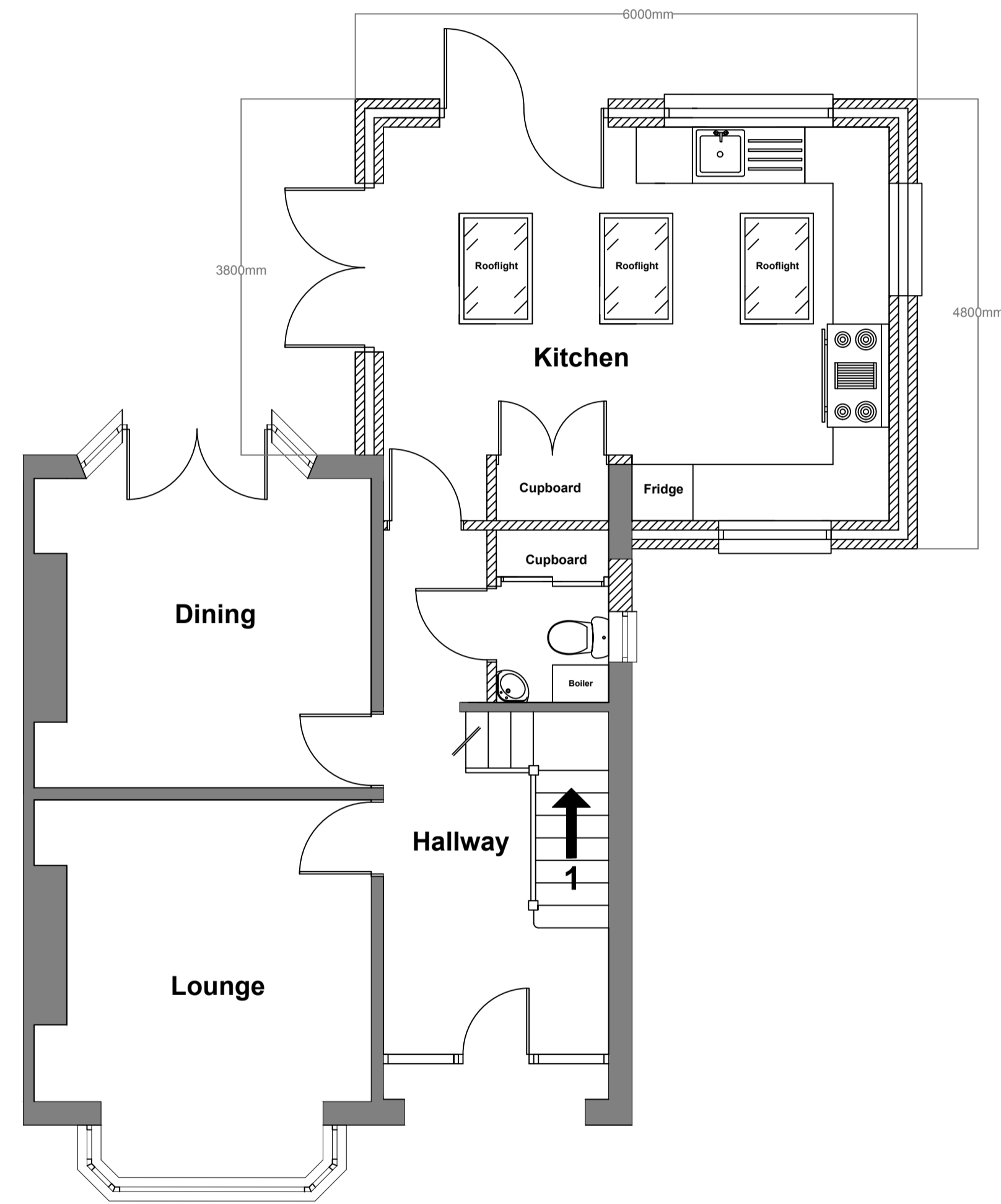
Client
Mr and Mrs Cross

Title
**Existing and Proposed
 Garage Plans**

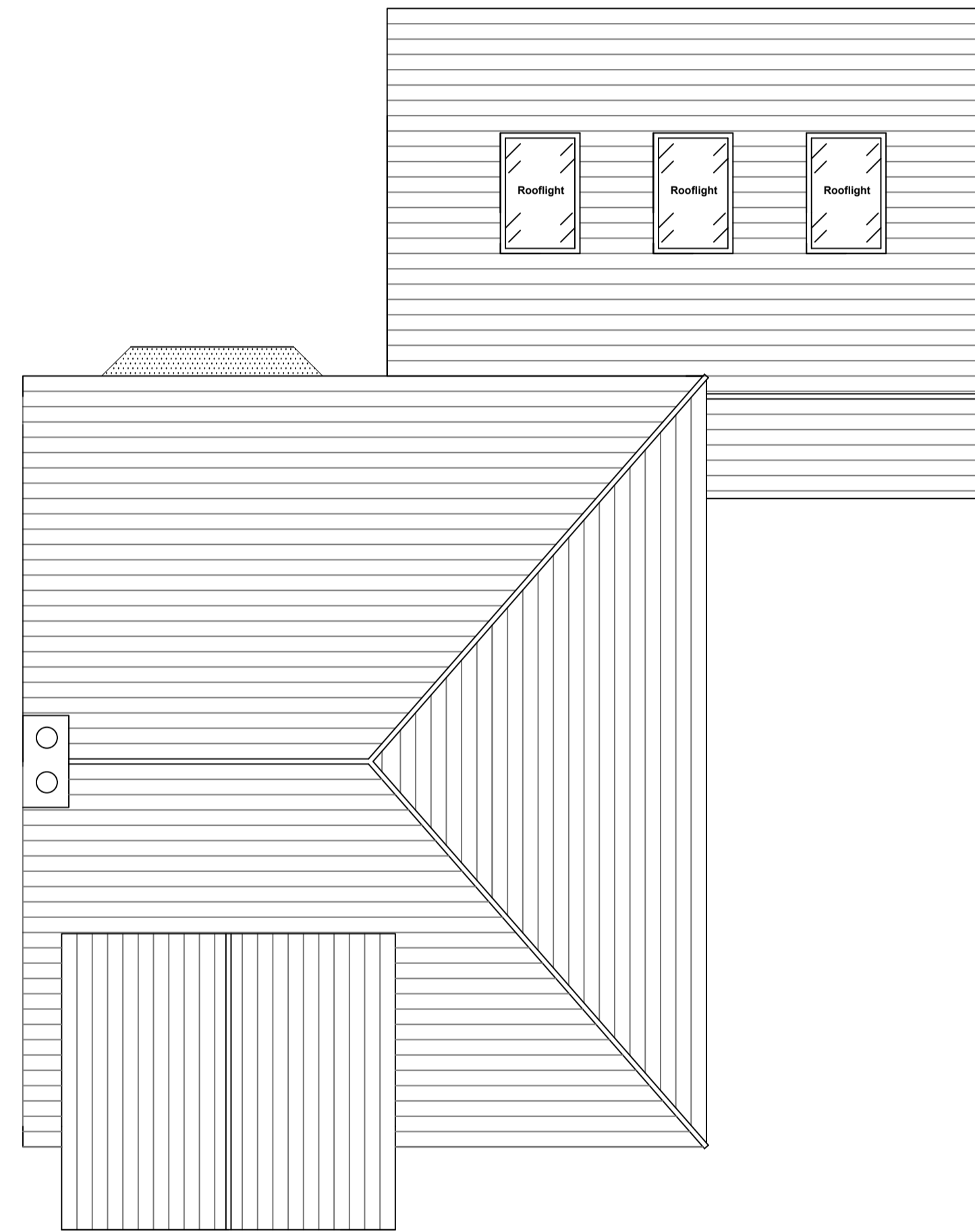
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 Date Checked
04/01/2019 CM





Proposed Ground Floor Plan



Proposed Roof Plan

P03 Planning Permission (Clients Comments) 04/01/2019 CM
 P02 Planning Permission (Clients Comments) 27/11/2018 CM
 P01 Planning Permission 28/10/2018 CM

Revisions
 Project

**6 Alford Road,
 Bristol,
 BS4 3HS**

Client
Mr and Mrs Cross

Title
**Proposed Floor and Roof
 Plans**

Drawing No.	Revision
P11-CP-015-002	P03

Scale @ A1	Drawn
1:50	CM
Date	Checked
04/01/2019	CM

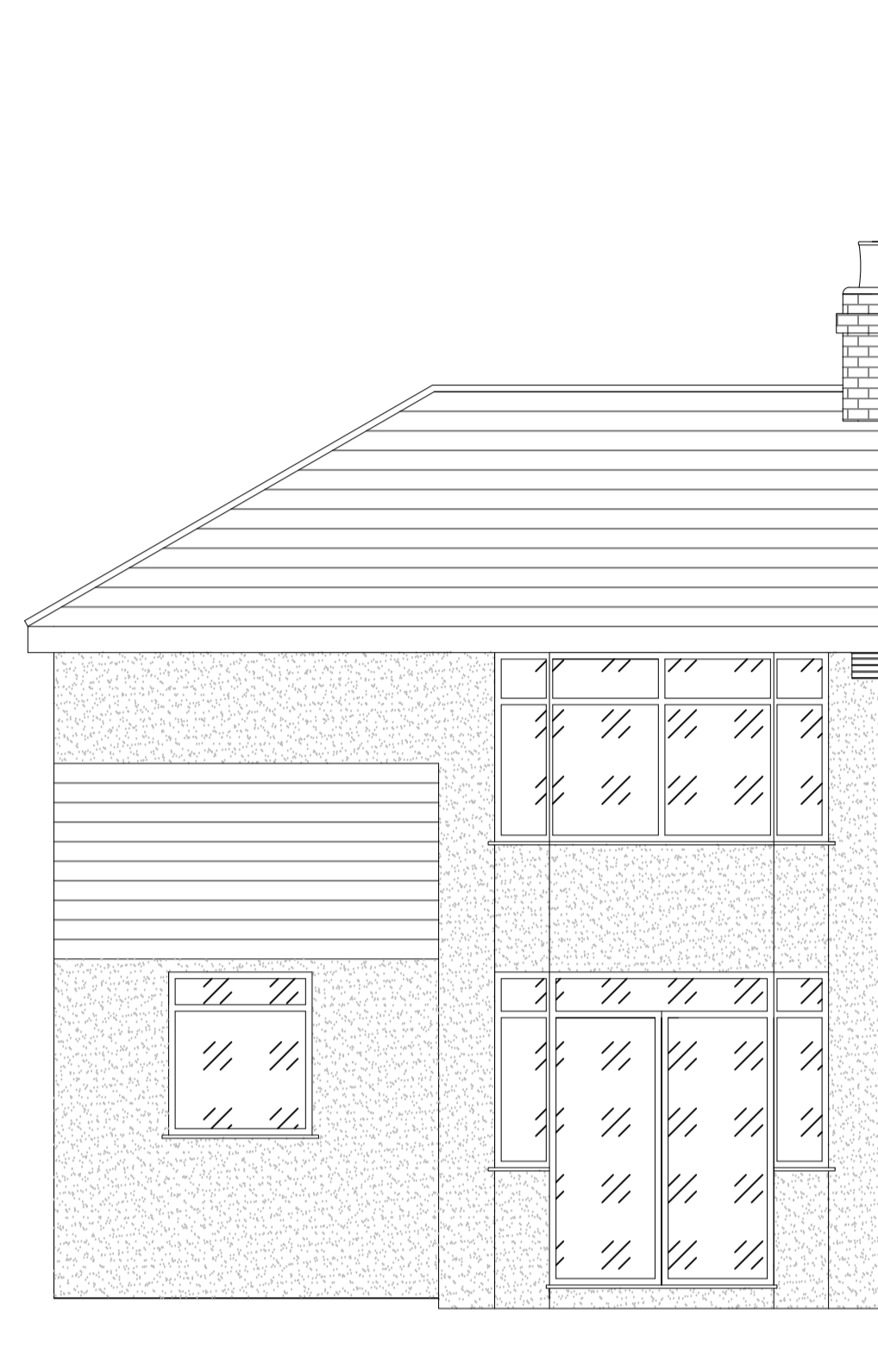


Extent of Property



Existing Front Elevation 1:50

Extent of Property



Existing Rear Elevation 1:50

Extent of Property



Proposed Front Elevation 1:50

Extent of Property



Proposed Rear Elevation 1:50

P03 Planning Permission (Clients Comments) 04/01/2019 CM CM
 P02 Planning Permission (Clients Comments) 27/11/2018 CM CM
 P01 Planning Permission 28/10/2018 CM CM

Revisions
 Project

**6 Alford Road,
 Bristol,
 BS4 3HS**

Client
Mr and Mrs Cross

Title
**Existing and Proposed
 Front & Rear Elevations**

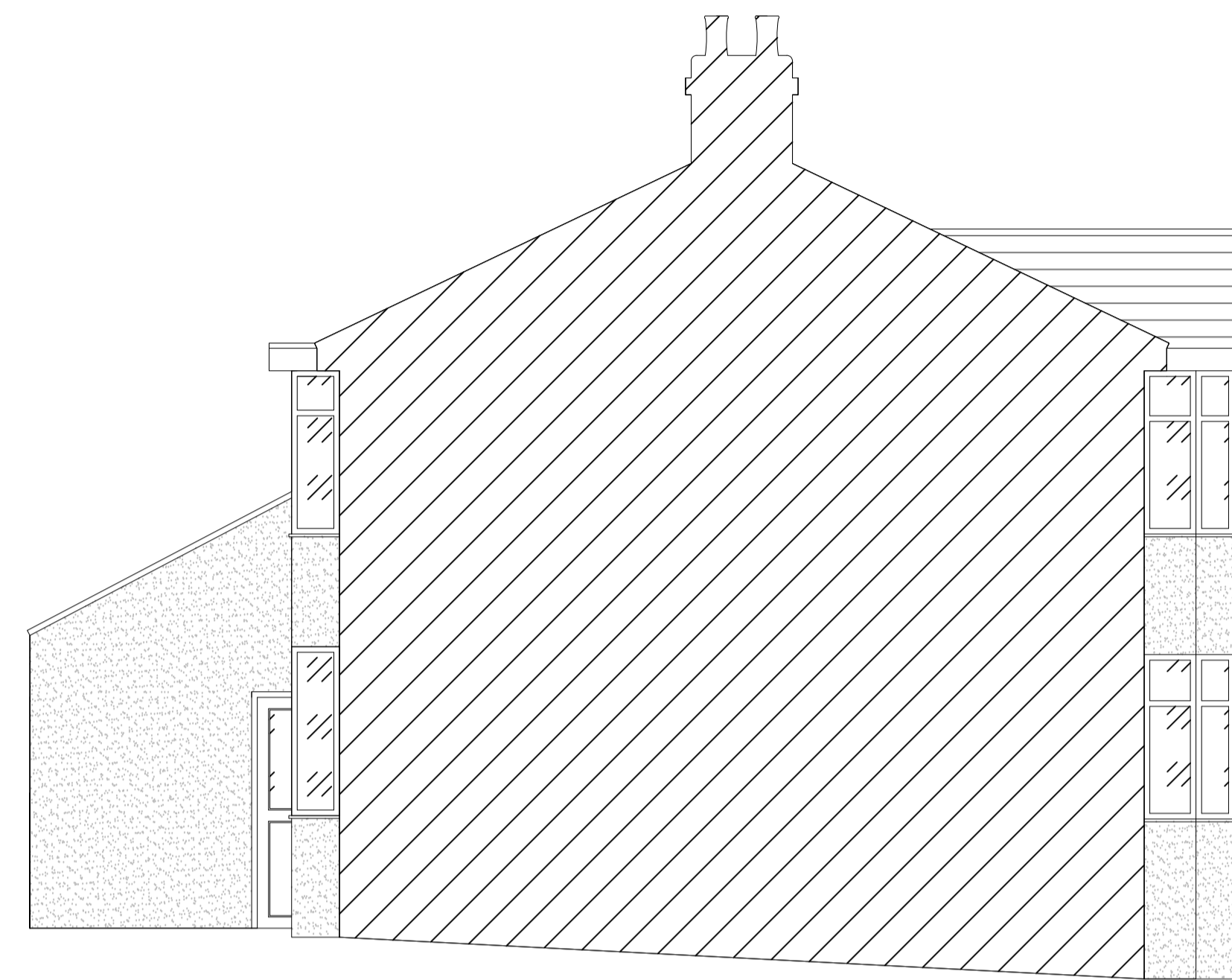
Drawing No. Revision
P11-CP-015-003 P03

Scale @ A1 Drawn
1:50 CM
 Date Checked
04/01/2019 CM





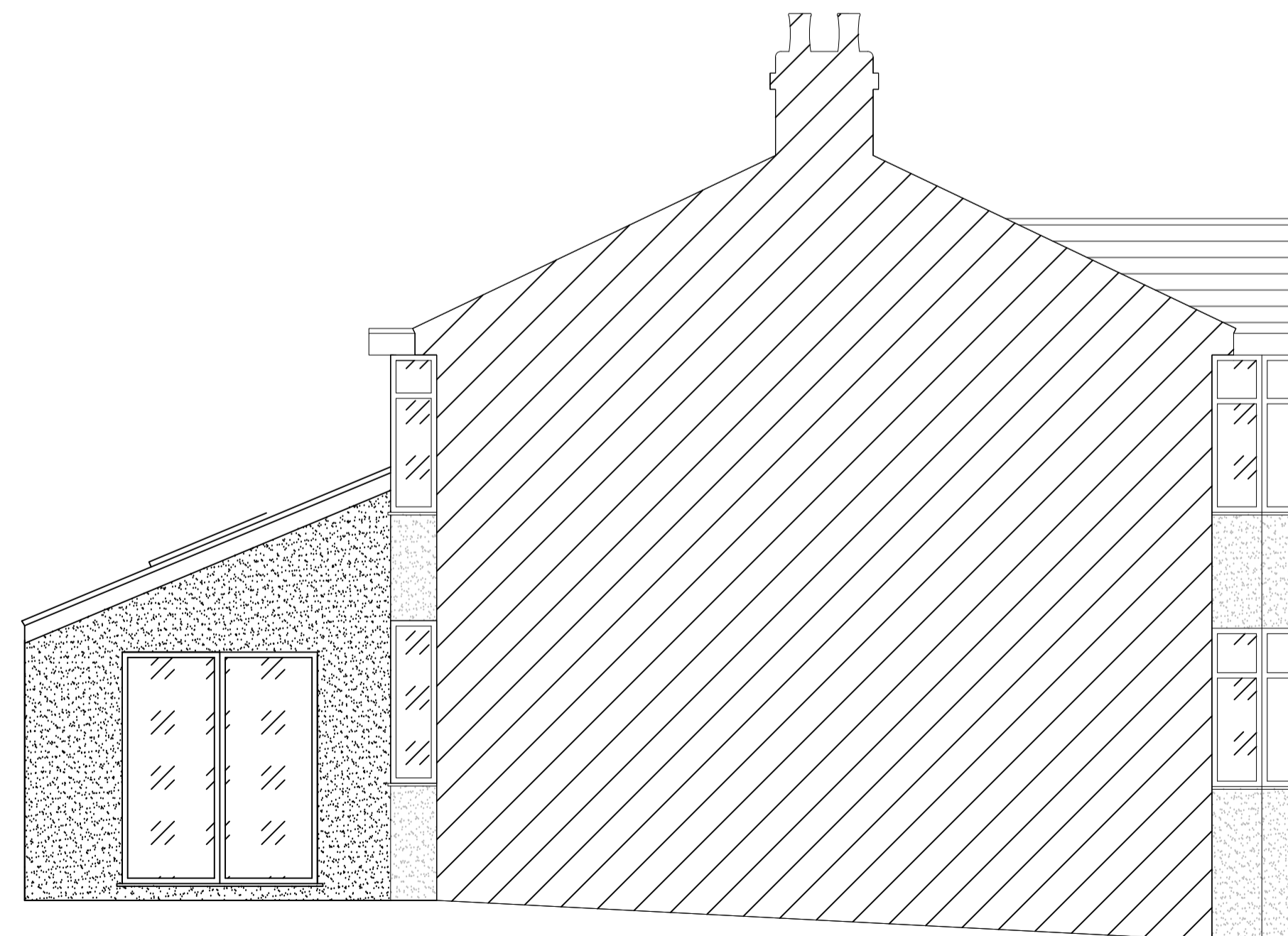
Existing Side Elevation 1:50



Existing Side Elevation 1:50



Proposed Side Elevation 1:50



Proposed Side Elevation 1:50

P03 Planning Permission (Clients Comments) 04/01/2019 CM CM
 P02 Planning Permission (Clients Comments) 27/11/2018 CM CM
 P01 Planning Permission 28/10/2018 CM CM

Revisions
 Project

**6 Alford Road,
 Bristol,
 BS4 3HS**

Client
Mr and Mrs Cross

Title
**Existing and Proposed
 Side Elevations**

Drawing No. Revision
P11-CP-015-004 P03

Scale @ A1 Drawn
1:50 CM
 Date Checked
04/01/2019 CM

